



City of Chicago



SO2017-7021

Office of the City Clerk

Document Tracking Sheet

Meeting Date: 10/11/2017

Sponsor(s): Misc. Transmittal

Type: Ordinance

Title: Zoning Reclassification Map No. 1-G at 156-174 N Peoria St, 906-908 W Randolph St, 151-185 N Sangamon St and 913-925 W Lake St - App No. 19380

Committee(s) Assignment: Committee on Zoning, Landmarks and Building Standards

ORDINANCE

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF CHICAGO:

SECTION 1. That the Chicago Zoning Ordinance be amended by changing all of the C1-1 Neighborhood Commercial District symbols and indications as shown on Map I-G in the area bounded by:

North Sangamon Street; West Lake Street; a line 125.90 feet east of and parallel to North Sangamon Street; a line 99.87 feet south of and parallel to West Lake Street; North Peoria Street; a line 64.93 feet north of and parallel to West Randolph Street; a line 75.57 feet west of and parallel to North Peoria Street; West Randolph Street; a line 100.76 feet west of and parallel to North Peoria Street; a line 64.92 feet north of and parallel to West Randolph Street; a line 125.97 feet east of and parallel to North Sangamon Street; West Randolph Street;

to those of a DX-5 Downtown Mixed-Use District.

SECTION 2. That the Chicago Zoning Ordinance be amended by changing all of the DX-5 Downtown Mixed-Use District symbols and indications as shown on Map I-G in the area bounded by:

North Sangamon Street; West Lake Street; a line 125.90 feet east of and parallel to North Sangamon Street; a line 99.87 feet south of and parallel to West Lake Street; North Peoria Street; a line 64.93 feet north of and parallel to West Randolph Street; a line 75.57 feet west of and parallel to North Peoria Street; West Randolph Street; a line 100.76 feet west of and parallel to North Peoria Street; a line 64.92 feet north of and parallel to West Randolph Street; a line 125.97 feet east of and parallel to North Sangamon Street; West Randolph Street;

to those of a Residential-Business Planned Development which is hereby established in the area above described, subject to such use and bulk regulations as are set forth in the Plan of Development herewith attached and made a part thereof and to no others.

SECTION 3. This Ordinance shall be in force and effect from and after its passage and due publication.

Address: 156-174 North Peoria Street; 906-908 and 912-924 West Randolph Street; 151-185 North Sangamon Street; 913-925 West Lake Street, Chicago, Illinois

RESIDENTIAL-BUSINESS PLANNED DEVELOPMENT NO. _____
PLANNED DEVELOPMENT STATEMENTS

1. The area delineated herein as Planned Development Number TBD (Planned Development) consists of approximately 67,658 square feet of property which is depicted on the attached Planned Development Boundary and Property Line Map (Property). 900 Block II Holdings, LLC is the "Applicant" for this Planned Development with the authorization from the property owner.
2. The requirements, obligations and conditions contained within this Planned Development shall be binding upon the Applicant, its successors and assigns and, if different than the Applicant, the legal title holders and any ground lessors. All rights granted hereunder to the Applicant shall inure to the benefit of the Applicant's successors and assigns and, if different than the Applicant, the legal title holder and any ground lessors. Furthermore, pursuant to the requirements of Section 17-8-0400 of the Chicago Zoning Ordinance, the Property, at the time of application for amendments, modifications or changes (administrative, legislative or otherwise) to this Planned Development are made, shall be under single ownership or designated control. Single designated control is defined in Section 17-8-0400.
3. All applicable official reviews, approvals or permits are required to be obtained by the Applicant or its successors, assignees or grantees. Any dedication or vacation of streets or alleys or grants of easements or any adjustment of the right-of-way shall require a separate submittal to the Department of Transportation on behalf of the Applicant or its successors, assigns or grantees.

Any requests for grants of privilege, or any items encroaching on the public way, shall be in compliance with the Planned Development.

Ingress or egress shall be pursuant to the Planned Development and may be subject to the review and approval of the Departments of Planning and Development and Transportation. Closure of all or any public street or alley during demolition or construction shall be subject to the review and approval of the Department of Transportation.

All work proposed in the public way must be designed and constructed in accordance with the Department of Transportation Construction Standards for Work in the Public Way and in compliance with the Municipal Code of Chicago. Prior to the issuance of any Part II approval, the submitted plans must be approved by the Department of Transportation.

Pursuant to a negotiated and executed Perimeter Restoration Agreement ("Agreement") by and between the Department of Transportation's Division of Infrastructure Management and the Applicant, the Applicant shall provide improvements and restoration of all public way adjacent to the property, which may include, but not be limited to, the following as shall be reviewed and determined by the Department of Transportation's Division of Infrastructure Management:

- Full width of streets
- Full width of alleys
- Curb and gutter
- Pavement markings
- Sidewalks

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Introduced	October 11, 2017
Plan Commission	December 19, 2019

- ADA crosswalk ramps
- Parkway & landscaping

The Perimeter Restoration Agreement must be executed prior to any Department of Transportation and Planned Development Part II review permitting. The Agreement shall reflect that all work must comply with current Rules and Regulations and must be designed and constructed in accordance with the Department of Transportation's Construction Standards for work in the Public Way and in compliance with the Municipal Code of Chicago Chapter 10-20. Design of said improvements should follow the Department of Transportation's Rules and Regulations for Construction in the Public Way as well as The Street and Site Plan Design Guidelines. Any variation in scope or design of public way improvements and restoration must be approved by the Department of Transportation.

As part of this project, the Applicant agrees to contribute \$215,000 to CDOT towards the cost of a proposed Randolph Street streetscape project prior to issuance of a certificate of occupancy for the proposed building.

4. This Plan of Development consists of 17 Statements; a Bulk Regulations Table; an Existing Zoning Map; an Existing Land Use Map; a Planned Development Boundary and Property Line Map; a Sub Area Plan; a Site Plan; a Landscape Plan; Landscape Details; and Building Elevations (North, South, East and West) prepared by Stantec and dated December 19, 2019 submitted herein. In any instance where a provision of this Planned Development conflicts with the Chicago Building Code, the Building Code shall control. This Planned Development conforms to the intent and purpose of the Chicago Zoning Ordinance, and all requirements thereto, and satisfies the established criteria for approval as a Planned Development. In case of a conflict between the terms of this Planned Development Ordinance and the Chicago Zoning Ordinance, this Planned Development shall control.
5. The following uses are permitted in the area delineated herein as a Planned Development: Dwelling Units Above the Ground Floor, Colleges and Universities, Cultural Exhibits and Libraries, Artist Work or Sales Space, Business Equipment Sales and Service, Business Support Services, General and Limited Restaurant, Tavern, Outdoor Patio (rooftop and at grade), Financial Services (except Payday/Title Secured Loan Store and Pawn Shop), Food and Beverage Retail Sales, Liquor Sales, Medical Service, Office, General Retail, Personal Service, Co-located Wireless Communication Facilities, Artisan Manufacturing (maximum 3,500 square feet in Subarea A), non-accessory parking (up to 45 percent pursuant to Section 17-10-0503), incidental and accessory uses and accessory parking.

The following uses shall be permitted in Subareas B and C only: Communication Service Establishments, Entertainment and Spectator Sports (all, except Inter-Track Wagering Facility), Small and Medium Venues, Banquet or Meeting Halls, Entertainment Cabaret, Participant Sports and Recreation, Children's Play Center, Auto Supply/Accessory Sales, Car Wash or Cleaning Service, and Indoor Light Equipment Sales/Rental.

6. On-Premise signs and temporary signs, such as construction and marketing signs, shall be permitted within the Planned Development, subject to the review and approval of the Department of Planning and Development. Off-Premise signs are prohibited within the boundary of the Planned Development.

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7. For purposes of height measurement, the definitions in the Chicago Zoning Ordinance shall apply. The height of any building shall also be subject to height limitations, if any, established by the Federal Aviation Administration.
8. The maximum permitted floor area ratio (FAR) for the Property shall be in accordance with the attached Bulk Regulations and Data Table. For the purpose of FAR calculations and measurements, the definitions in the Zoning Ordinance shall apply. The permitted FAR identified in the Bulk Regulations and Data Table has been determined using a net site area of 67,658 square feet and a base FAR of 5.0.

The Applicant acknowledges that the project has received a bonus FAR of 3.1, pursuant to Sec. 17-4-1000 of the Zoning Ordinance. With this bonus FAR, the total FAR for the Planned Development is 8.1. In exchange for the bonus FAR, the Applicant is required to make a corresponding payment, pursuant to Sections 17-4-1003-B & C, prior to the issuance of the first building permit for any building in Subarea A; provided, however, if the Planned Development is constructed in phases, the bonus payment may be paid on a pro rata basis as the first building permit for each subsequent new building or phase of construction is issued. The bonus payment will be recalculated at the time of payment (including partial payments for phased developments) and may be adjusted based on changes in median land values in accordance with Section 17-4-1003-C.3

The bonus payment will be split between three separate funds, as follows: 80% to the Neighborhoods Opportunity Fund, 10% to the Citywide Adopt-a-Landmark Fund and 10% to the Local Impact Fund. In lieu of paying the City directly, the Department may: (a) direct developers to deposit a portion of the funds with a sister agency to finance specific local improvement projects; (b) direct developers to deposit a portion of the funds with a landmark property owner to finance specific landmark restoration projects; or, (c) approve proposals for in-kind improvements to satisfy the Local Impact portion of the payment.

9. Upon review and determination, Part II Review, pursuant to Section 17-13-0610, a Part II Review Fee shall be assessed by the Department of Planning and Development. The fee, as determined by staff at the time, is final and binding on the Applicant and must be paid to the Department of Revenue prior to the issuance of any Part II approval.
10. The Site and Landscape Plans shall be in substantial conformance with the Landscape Ordinance and any other corresponding regulations and guidelines, including Section 17-13-0800. Final landscape plan review and approval will be by the Department of Planning and Development. Any interim reviews associated with site plan review or Part II reviews, are conditional until final Part II approval.
11. The Applicant shall comply with Rules and Regulations for the Maintenance of Stockpiles promulgated by the Commissioners of the Departments of Streets and Sanitation, Fleet and Facility Management and Buildings, under Section 13-32-085, or any other provision of the Municipal Code of Chicago.
12. The terms and conditions of development under this Planned Development ordinance may be modified administratively, pursuant to Section 17-13-0611-A, by the Zoning Administrator upon the application for such a modification by the Applicant, its successors and assigns and, if different than the Applicant, the legal title holders and any ground lessors.

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13. The Applicant acknowledges that it is in the public interest to design, construct and maintain the project in a manner which promotes, enables and maximizes universal access throughout the Property. Plans for all buildings and improvements on the Property shall be reviewed and approved by the Mayor's Office for People with Disabilities to ensure compliance with all applicable laws and regulations related to access for persons with disabilities and to promote the highest standard of accessibility.
14. The Applicant acknowledges that it is in the public interest to design, construct, renovate and maintain all buildings in a manner that provides healthier indoor environments, reduces operating costs and conserves energy and natural resources. The Applicant shall obtain the number of points necessary to meet the requirements of the Chicago Sustainable Development Policy, in effect at the time the Part II review process is initiated for each improvement that is subject to the aforementioned Policy and must provide documentation verifying compliance.
15. The Applicant acknowledges that it is the policy of the City to maximize opportunities for Minority and Women-owned Business Enterprises ("M/WBEs") and city residents to compete for contracts and jobs on construction projects approved through the planned development process. To assist the city in promoting and tracking such M/WBE and city resident participation, an applicant for planned development approval shall provide information at three points in the city approval process. First, the applicant must submit to DPD, as part of its application for planned development approval, an M/WBE Participation Proposal. The M/WBE Participation Proposal must identify the applicant's goals for participation of certified M/WBE firms in the design, engineering and construction of the project, and of city residents in the construction work. The city encourages goals of 26% MBE and 6% WBE participation (measured against the total construction budget for the project or any phase thereof), and (ii) 50% city resident hiring (measured against the total construction work hours for the project or any phase thereof). The M/WBE Participation Proposal must include a description of the Applicant's proposed outreach plan designed to inform M/WBEs and city residents of job and contracting opportunities. Second, at the time of the Applicant's submission for Part II permit review for the project or any phase thereof, the Applicant must submit to DPD (a) updates (if any) to the Applicant's preliminary outreach plan, (b) a description of the Applicant's outreach efforts and evidence of such outreach, including, without limitation, copies of certified letters to M/WBE contractor associations and the ward office of the alderman in which the project is located and receipts thereof; (c) responses to the Applicant's outreach efforts, and (d) updates (if any) to the applicant's M/WBE and city resident participation goals. Third, prior to issuance of a Certificate of Occupancy for the project or any phase thereof, the Applicant must provide DPD with the actual level of M/WBE and city resident participation in the project or any phase thereof, and evidence of such participation. In addition to the forgoing, DPD may request such additional information as the department determines may be necessary or useful in evaluating the extent to which M/WBEs and city residents are informed of and utilized in planned development projects. All such information will be provided in a form acceptable to the Zoning Administrator. DPD will report the data it collects regarding projected and actual employment of M/WBEs and city residents in planned development projects twice yearly to the Chicago Plan Commission and annually to the Chicago City Council and the Mayor.
16. The Applicant acknowledges and agrees that the rezoning of the Property from the C1-1 Neighborhood Commercial District to the DX-5 Downtown Mixed-Use District and then to this

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Planned Development (PD), for construction of the Project triggers the requirements of Section 2-44-080 of the Municipal Code (the "Affordable Requirements Ordinance" or the "ARO"). The Applicant further acknowledges and agrees that the Property is located in the Near North/Near West Pilot Area, pursuant to Section 2-44-090 of the Municipal Code (the "Near North/Near West ARO Pilot Area Ordinance" or the "Pilot"). The Near North/Near West Pilot Area is divided into two zones: the Near North Zone and the Near West Zone. The Property is located in the Near North Zone. In the Near North Zone, pursuant to the ARO, the percentage of units in a residential housing project required to be affordable for a period of 30 years, whether rental or for-sale, is increased from 10% to 20%. Any developer of a residential housing project in the Near North Zone must provide the first 10% of units required to be affordable (the "First Units") either: (i) in the residential housing project, or (ii) with the approval of the commissioner of the Department of Housing ("DOH"), subject to the transition provisions of Section 2-44-040(c), in consultation with the commissioner of the Department of Planning and Development ("DPD") as appropriate, in an off-site location within two miles of the Property and in the same or a different higher income area or downtown district, or (iii) any combination of (i) and (ii). In addition, the developer must provide the second 10% of units required to be affordable (the "Additional Units") either: (i) in the residential housing project, or (ii) with the approval of the commissioner of DOH (subject to the transition provisions of Section 2-44-040(c)), in consultation with the commissioner of DPD as appropriate, in an off-site location anywhere within the Near North/Near West Pilot Area, regardless of distance from the project or income area, or (iii) any combination of (i) and (ii). The project has a total of 300 housing units. As a result, the Applicant's affordable housing obligation is 60 affordable units (20% of 300), consisting of 30 First Units and 30 Additional Units. Applicant has agreed to satisfy its affordable housing obligation by providing the First Units and the Additional Units in the rental building to be constructed in the Planned Development, to be approved by the DOH in consultation with DPD as appropriate, as set forth in the Affordable Housing Profile Form attached hereto. In accordance with the ARO Pilot, the Applicant is required to lease the First Units to households earning up to 60% of the Chicago Primary Metropolitan Statistical Area median income ("AMI") at prices affordable to households at such income level, and the Additional Units to households earning up to 80% or 100% of AMI, as determined by rule and approved by the Commissioner at prices affordable to households at such income level. If the Applicant subsequently reduces (or increases) the number of housing units in the project, or elects to build a for-sale project instead of a rental project, or (with the Commissioner's approval) elects to construct off-site units instead of on-site units, the Applicant shall update and resubmit the Affordable Housing Profile Form to DOH for review and approval. DOH may adjust the requirements to reflect any such change without amending the Planned Development. Prior to the issuance of any building permits for any residential building in the Planned Development, including, without limitation, excavation or foundation permits, the Applicant must execute and record an affordable housing agreement in accordance with Section 2-44-080(L). The terms of the affordable housing agreement and any amendments thereto are incorporated herein by this reference. The Applicant acknowledges and agrees that the affordable housing agreement will be recorded against the Planned Development, or the applicable portion thereof, and will constitute a lien against such property. The Commissioner may enforce remedies for any breach of this Statement 16, including any breach of any affordable housing agreement, and enter into settlement agreements with respect to any such breach, subject to the approval of the Corporation Counsel, without amending the Planned Development.

17. This Planned Development shall be governed by Section 17-13-0612. Should this Planned Development ordinance lapse, the Commissioner of the Department of Planning and Development

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shall initiate a Zoning Map Amendment to rezone the property to the DX-5 Downtown Mixed-Use District.

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<i>Introduced</i>	October 11, 2017
<i>Plan Commission</i>	December 19, 2019

RESIDENTIAL-BUSINESS PLANNED DEVELOPMENT NO. _____
BULK REGULATIONS AND DATA TABLE

Gross Site Area (sl): 96,368

Area of Public Rights-of-Way (sf): 28,710

Net Site Area (sf): 67,658

Subarea A: 28,946

Subarea B: 37,077

Subarea C: 1,635

Maximum Floor Area Ratio: 8.1

Subarea A: 15.86

Subarea B: 2.31

Subarea C: 2.08

Maximum Permitted Dwelling Units:

Subarea A: 300

Subarea B: 0

Subarea C: 0

Minimum Off-Street Parking Spaces:

Subarea A: 75

Subarea B: 0

Subarea C: 0

Minimum Off-Street Loading Spaces:

Subarea A: 2 (10'x25')

Subarea B: 0

Subarea C: 0

Maximum Building Height:

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Address. 906-908 and 912-924 West Randolph Street, 151-185 North Sangamon Street, 913-925 West Lake Street, 156-174 North Peoria Street
Introduced October 11, 2017
Plan Commission December 29, 2019

EASTM46573967.6

Final for Publication

Subarea A: 495'

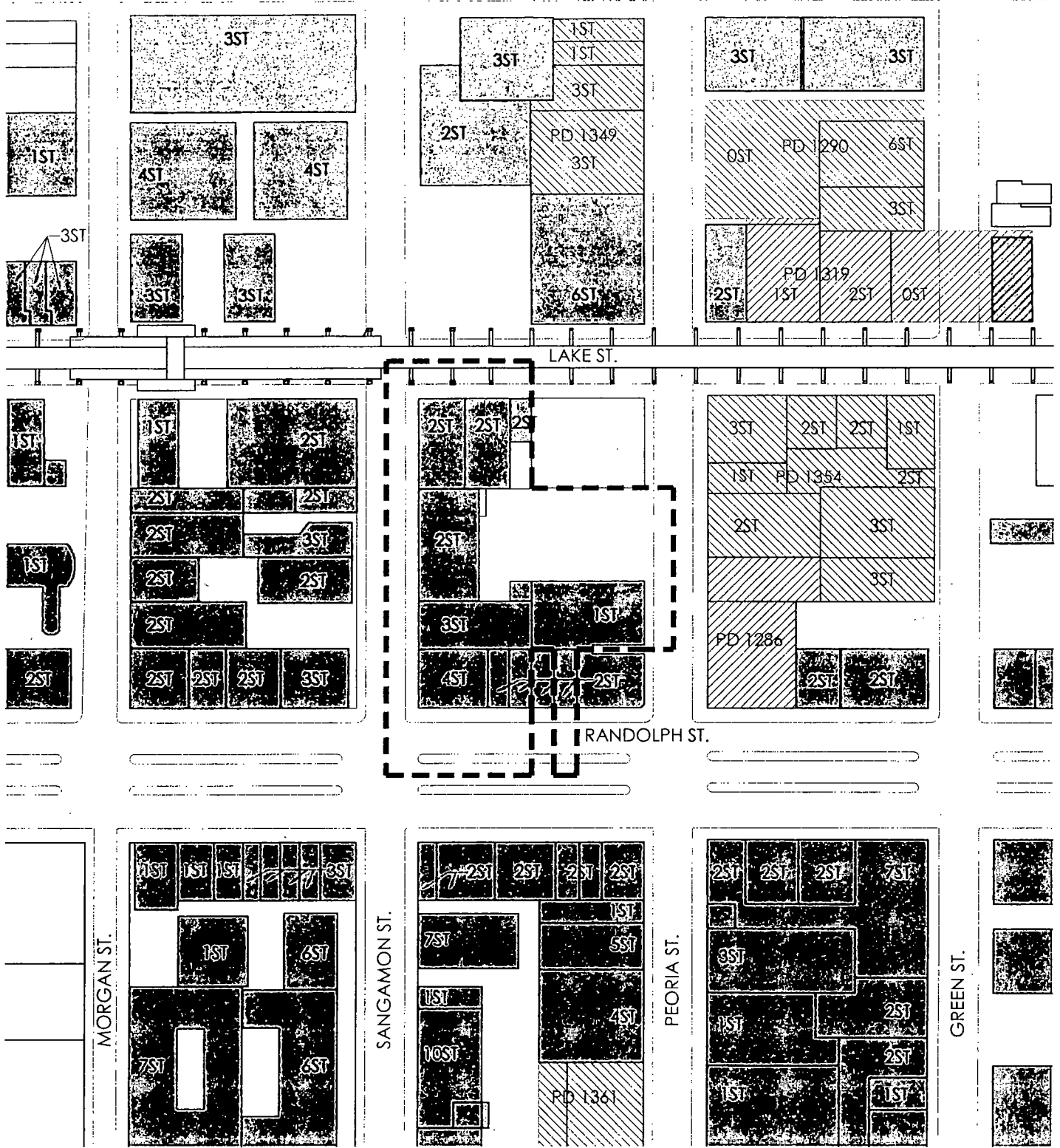
Subarea B: 60'

Subarea C: 30'

Minimum Setbacks: In accordance with plans

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PLAN KEY

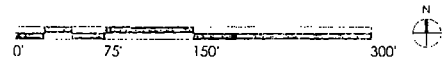
----- PLANNED DEVELOPMENT BOUNDARY

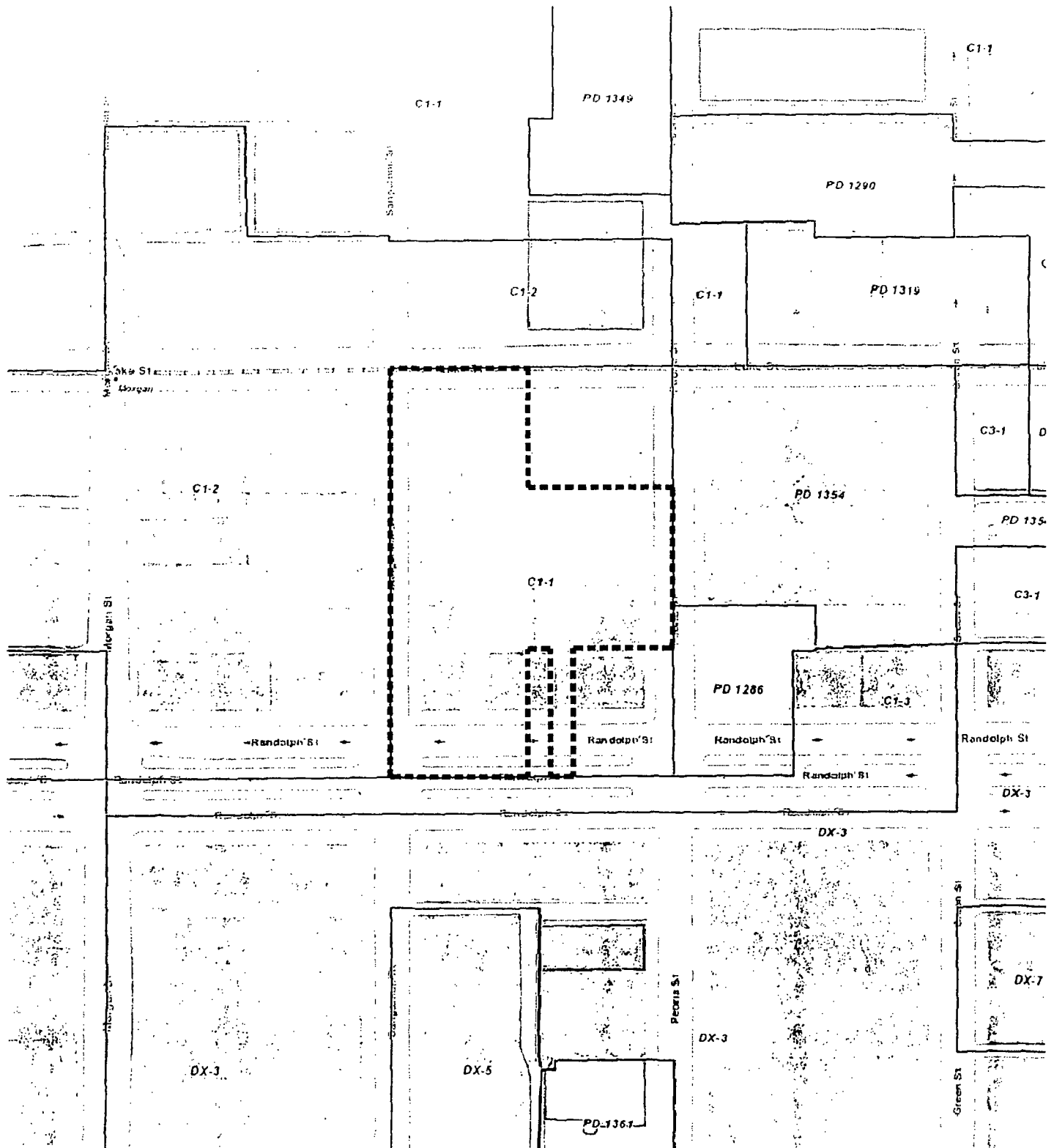
■ COMMERCIAL
□ EXISTING PD

▨ BUSINESS

EXISTING LAND USE PLAN

APPLICANT: 900 BLOCK II HOLDINGS, LLC
 ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
 151-185 NORTH SANGAMON STREET/913-925 WEST LAKE STREET, CHICAGO, ILLINOIS
 INTRODUCTION DATE: OCTOBER 11, 2017
 PLAN COMMISSION: DECEMBER 19, 2019





PLAN KEY

----- PLANNED DEVELOPMENT BOUNDARY

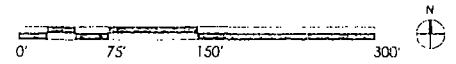
EXISTING ZONING MAP

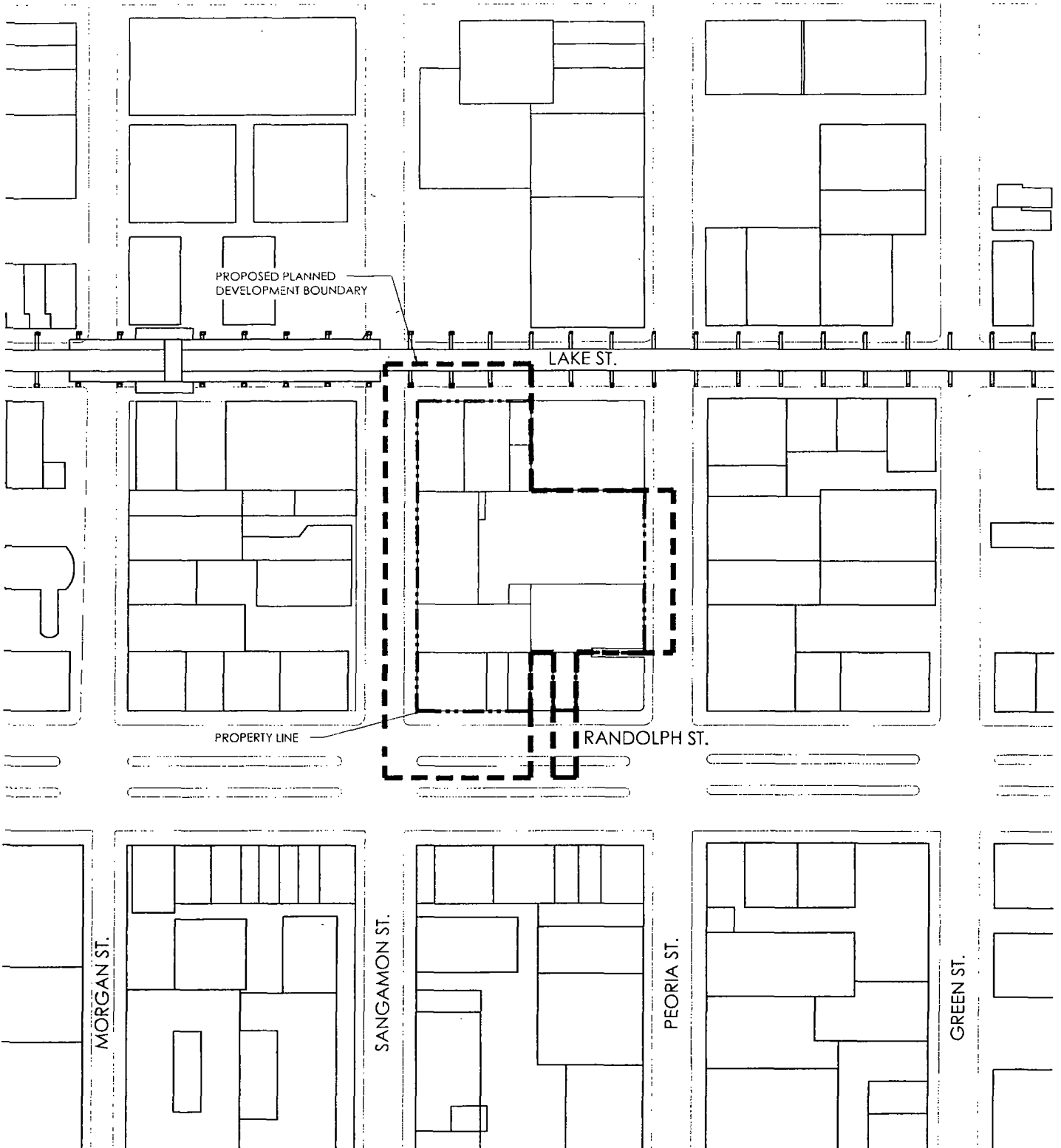
APPLICANT: 900 BLOCK II HOLDINGS, LLC

ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
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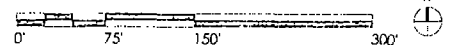




PLAN KEY

- PLANNED DEVELOPMENT BOUNDARY
- PROPERTY LINE

PLANNED DEVELOPMENT BOUNDARY AND PROPERTY LINE MAP



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LAKE ST.

SANGAMON ST.

PEORIA ST.

RANDOLPH ST.

± 126'-0"

± 100'-6"

SUB-AREA B
(~37,077 SF)

± 176'-2"

SUB-AREA A
(~28,946 SF)

± 125'-2"

± 50'-2"

± 179'-11"

± 126'-0"

± 117'-9"

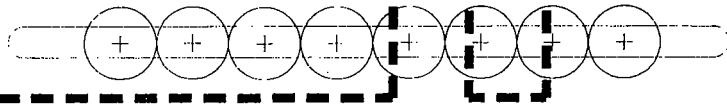
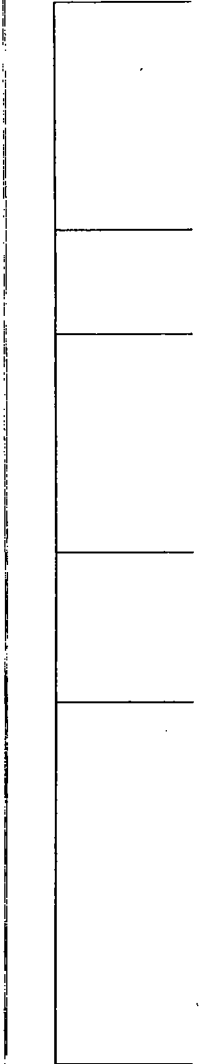
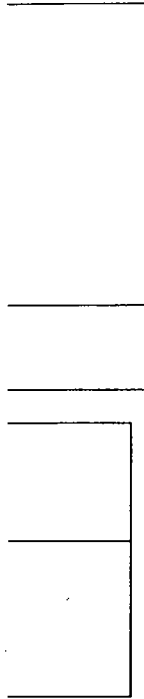
SUB-AREA C
(~1,635 SF)

± 64'-2"

± 25'-6"

± 126'-3"

± 343'-6"



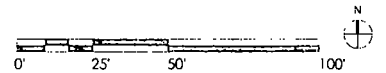
PLAN KEY

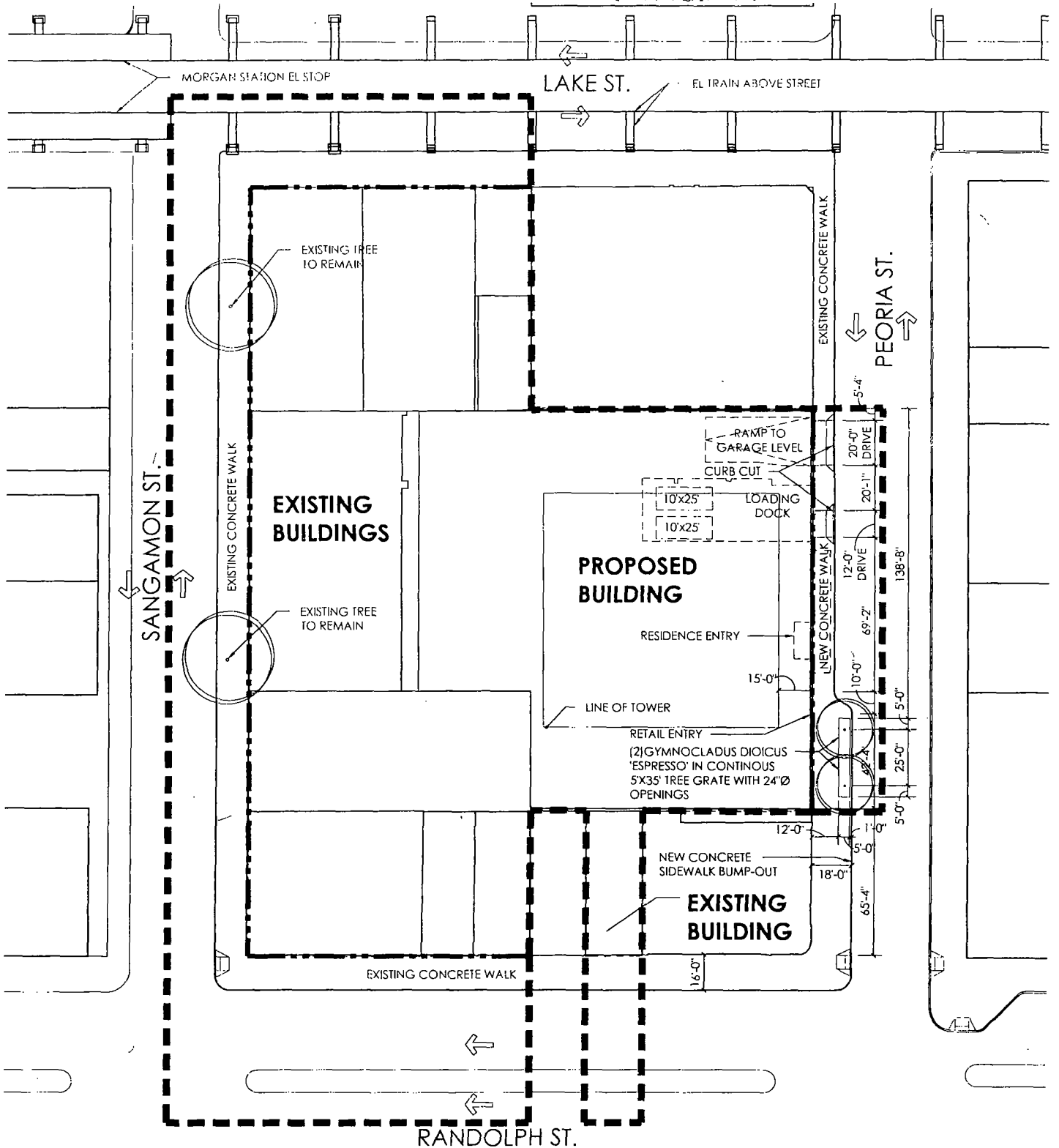
--- SUB-AREA BOUNDARY

SUB AREA PLAN

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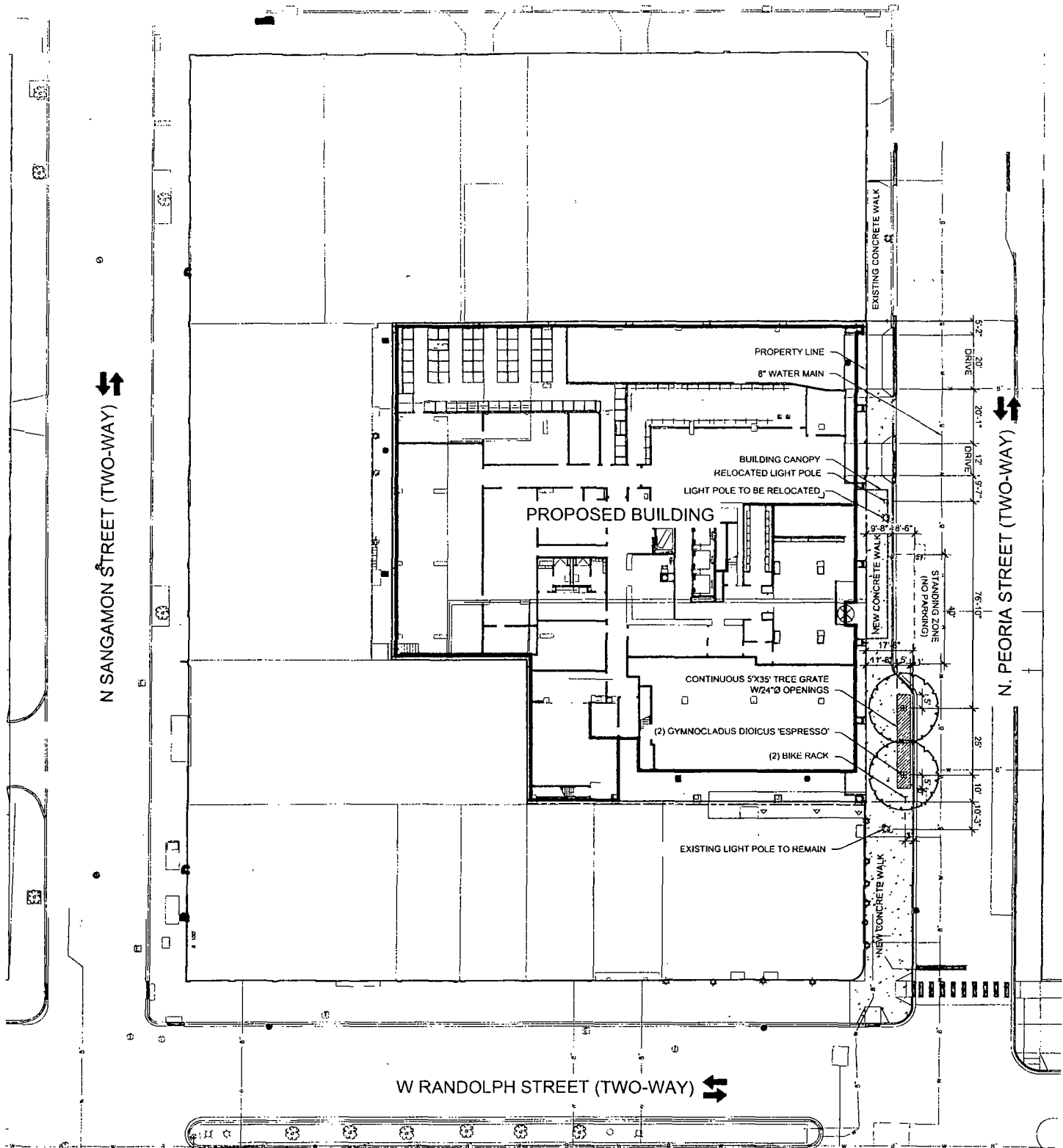
PLAN KEY

- PLANNED DEVELOPMENT BOUNDARY
- PROPERTY LINE

SITE/LANDSCAPE PLAN

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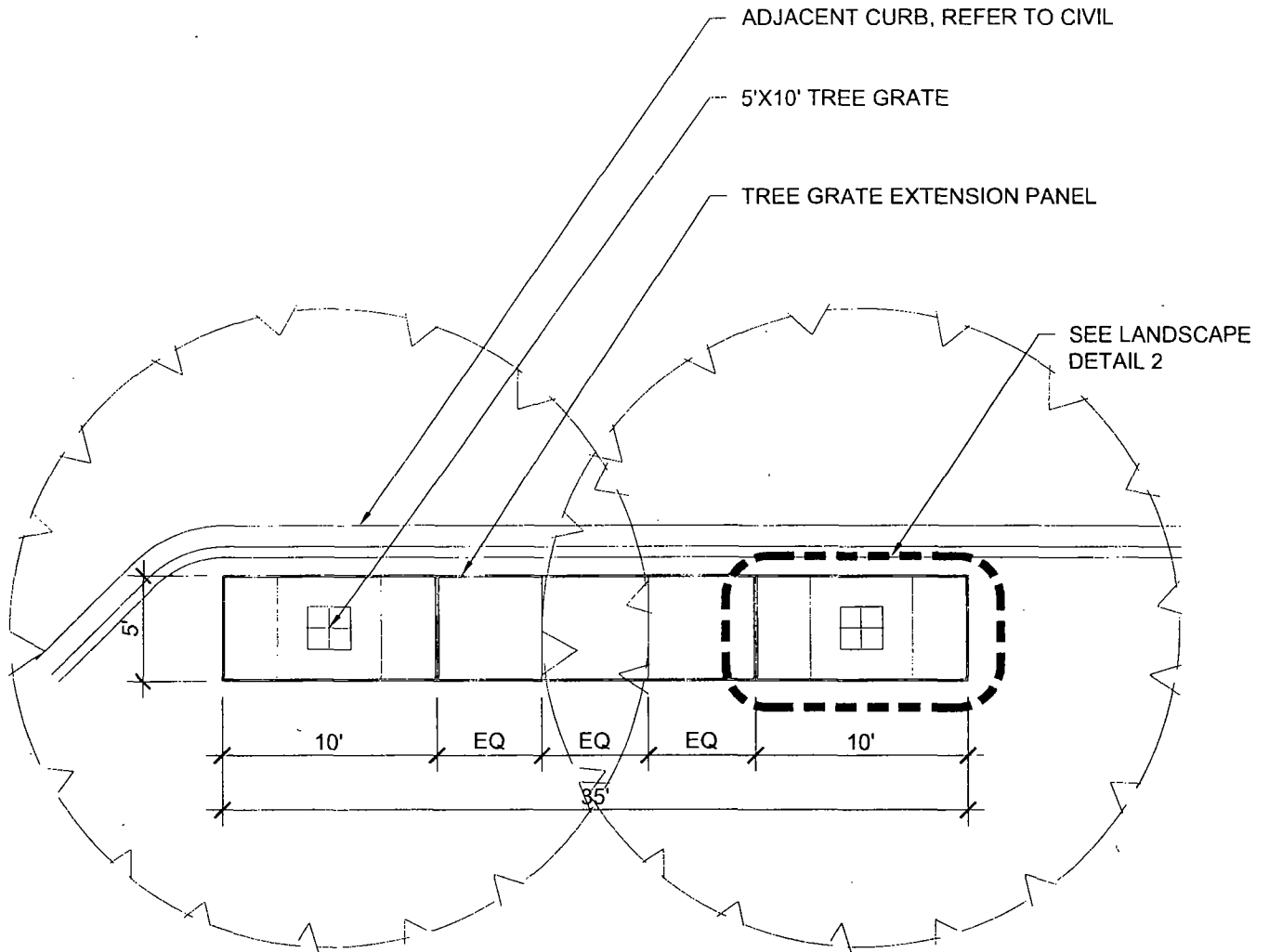


NOTES

- 1 4" LAYER OF 1" BLACK LAVA INSTALLED THROUGHOUT THE GRATE OPENING
LEAVE NO VOIDS BETWEEN SOIL AND GRATE
- 2 ADDITIONAL STREET TREES NOT LOCATED NORTH OF TWO PROPOSED STREET TREES DUE TO BUILDING CANOPY AND DRIVEWAYS

LANDSCAPE PLAN

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NOTE:

1. ALL GRATES MUST BE SECURED FROM BENEATH EACH GRATE PANEL WITH MANUFACTURER'S 1/2" BOLTING HARDWARE. SECURING GRATES TO FRAMES IS PROHIBITED.
2. LAVA ROCK MUST BE 1" BLACK LEAVING NO VOIDS BETWEEN SOIL AND BOTTOM SIDE OF GRATES.
3. TREE GRATE AND EXPANSION PANELS TO BE INSTALLED PER MANUFACTURER'S SPECIFICATIONS.

1

OVERALL CONTINUOUS TREE GRATE LAYOUT

1/8" = 1'-0"

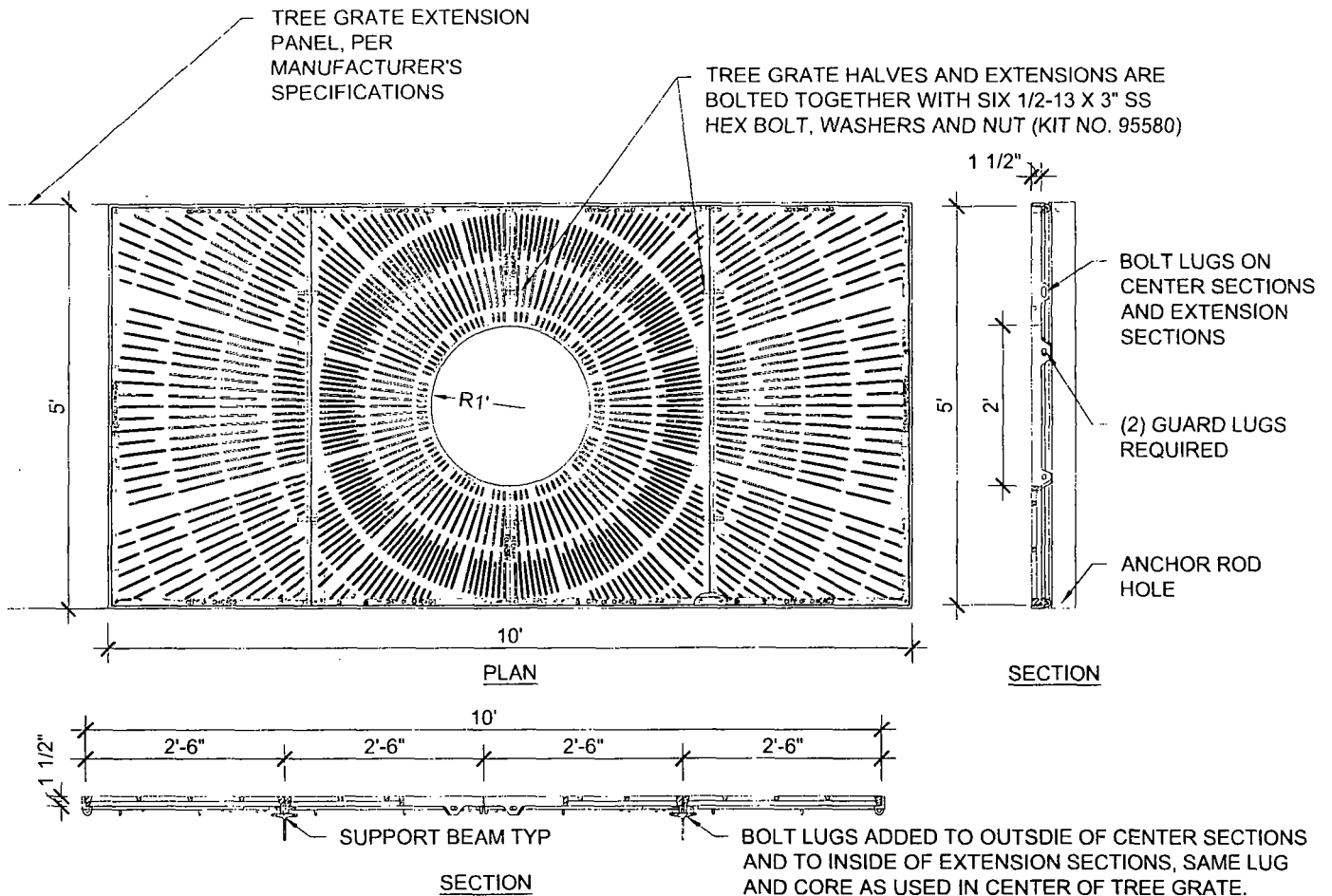
LANDSCAPE DETAILS

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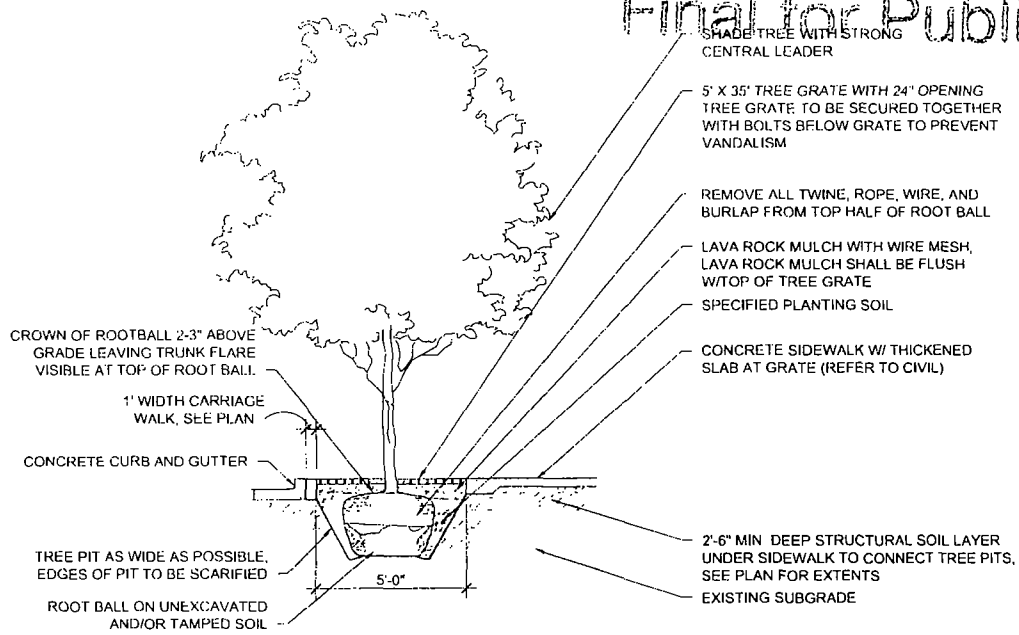
NOTE:

1. ALL GRATES MUST BE SECURED FROM BENEATH EACH GRATE PANEL WITH MANUFACTURER'S 1/2"
2. BOLTING HARDWARE. SECURING GRATES TO FRAMES IS PROHIBITED.
LAVA ROCK MUST BE 1" BLACK LEAVING NO VOIDS BETWEEN SOIL AND BOTTOM SIDE OF GRATES.

2 CONTINUOUS TREE GRATE DETAIL ENLARGEMENT
1/2" = 1'-0"

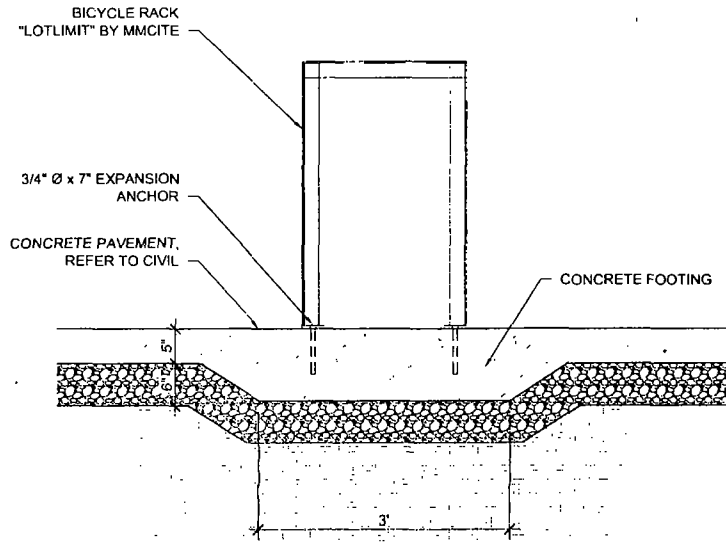
LANDSCAPE DETAILS

APPLICANT: 900 BLOCK II HOLDINGS, LLC
ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
 151-185 NORTH SANGAMON STREET/913-925 WEST LAKE STREET, CHICAGO, ILLINOIS
INTRODUCTION DATE: OCTOBER 11, 2017
PLAN COMMISSION: DECEMBER 19, 2019



- NOTES:
- 1 SEE LANDSCAPE DETAIL 3 FOR TREE GRATE PANEL BASIS OF DESIGN
 - 2 ALL GRATES MUST BE SECURED FROM BENEATH EACH GRATE PANEL WITH MANUFACTURER'S 1/2" BOLTING HARDWARE. SECURING GRATES TO FRAMES IS PROHIBITED.
 - 3 LAVA ROCK MUST BE 1" BLACK LEAVING NO VOIDS BETWEEN SOIL AND BOTTOM SIDE OF GRATES.

4 **SHADE TREE IN 5'X35' GRATE SECTION**
NOT TO SCALE



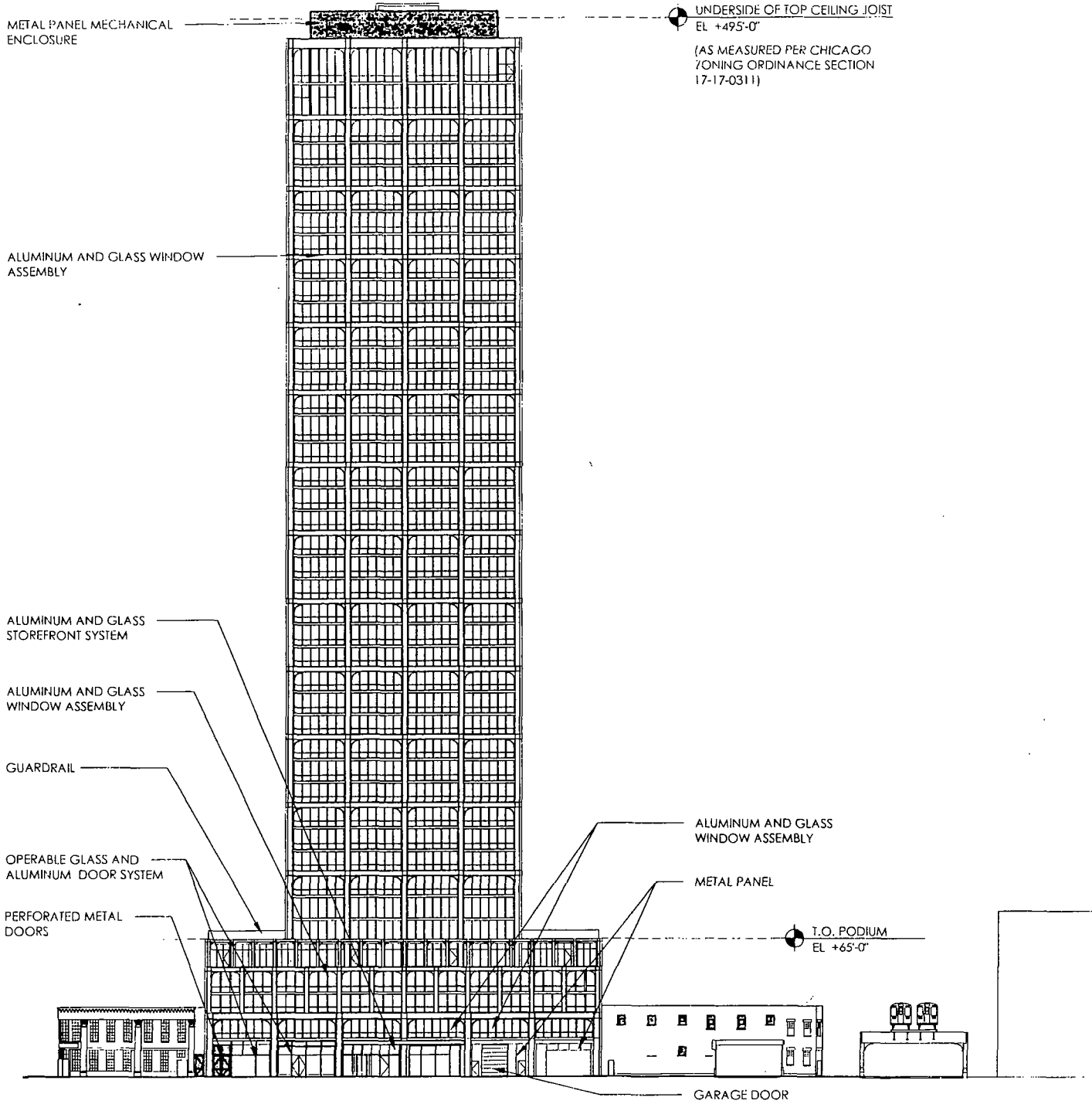
- NOTE
- 1 BASIS OF DESIGN: "LOTLIMIT" BIKE RACK BY MMCITE

5 **BIKE RACK DETAIL**
NOT TO SCALE

LANDSCAPE DETAILS

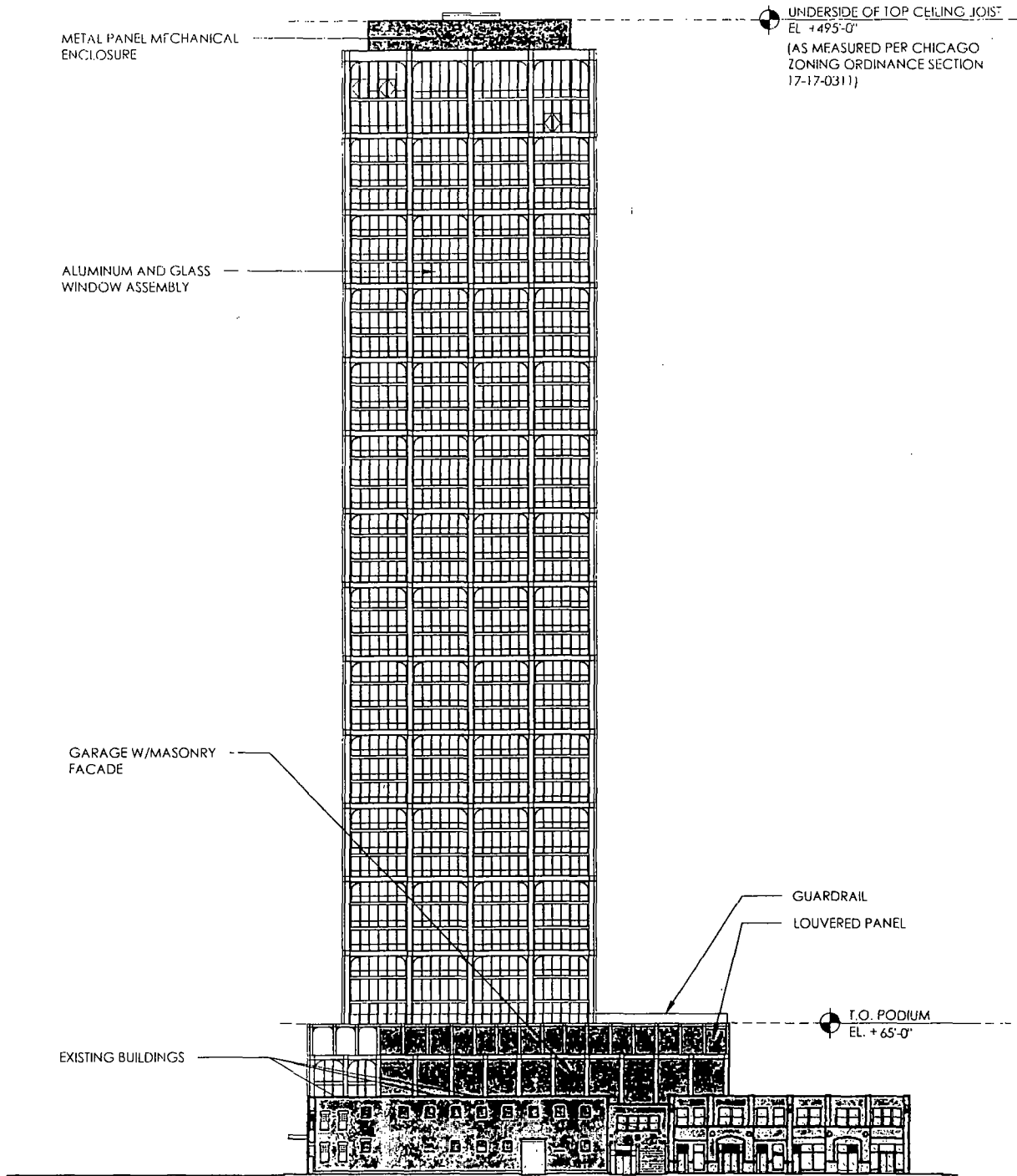
APPLICANT: 900 BLOCK II HOLDINGS, LLC
 ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
 151-185 NORTH SANGAMON STREET/913-925 WEST LAKE STREET, CHICAGO, ILLINOIS
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EAST ELEVATION

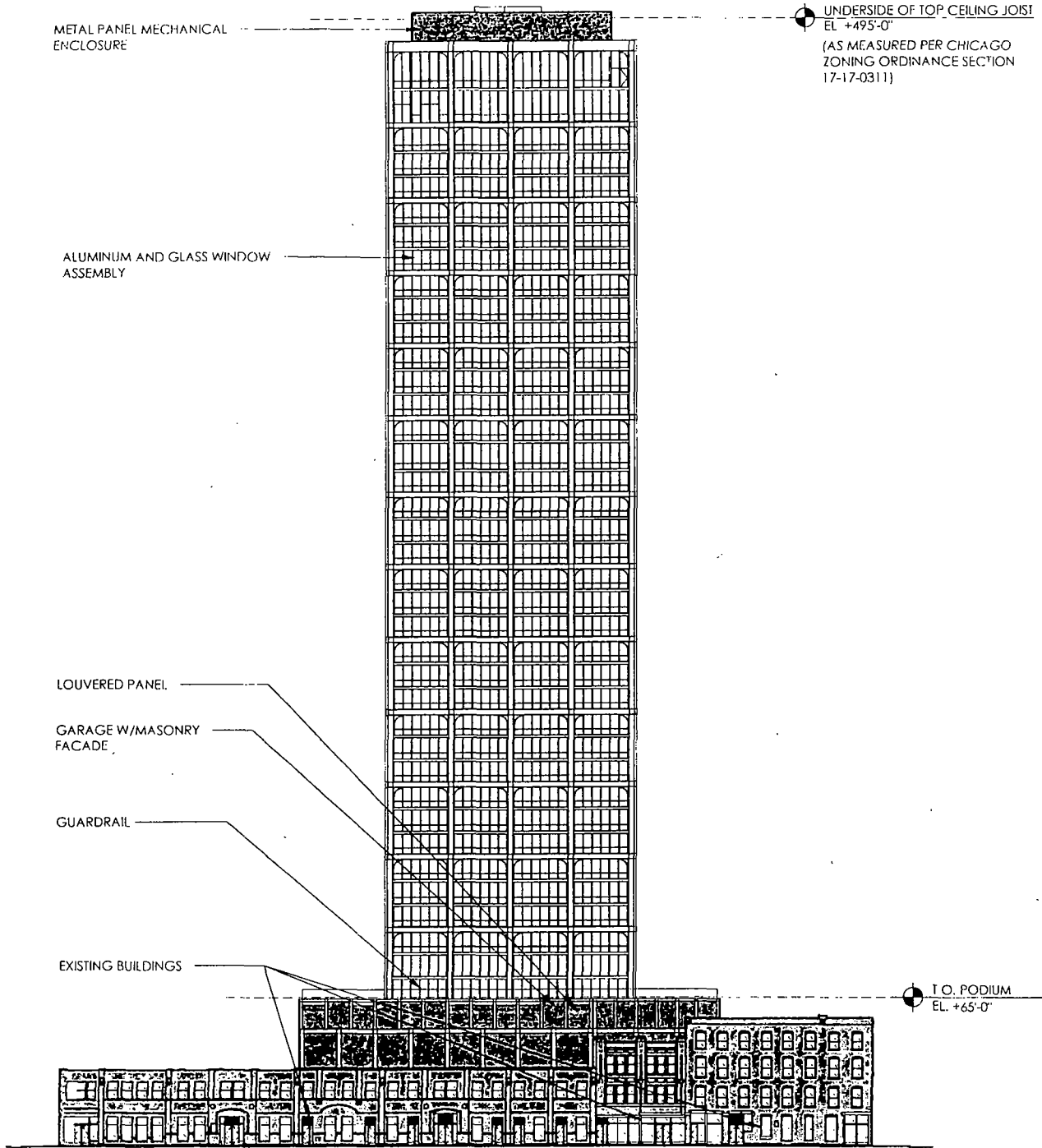
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ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
151-185 NORTH SANGAMON STREET/913-925 WEST LAKE STREET, CHICAGO, ILLINOIS
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NORTH ELEVATION

APPLICANT: 900 BLOCK II HOLDINGS, LLC
ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
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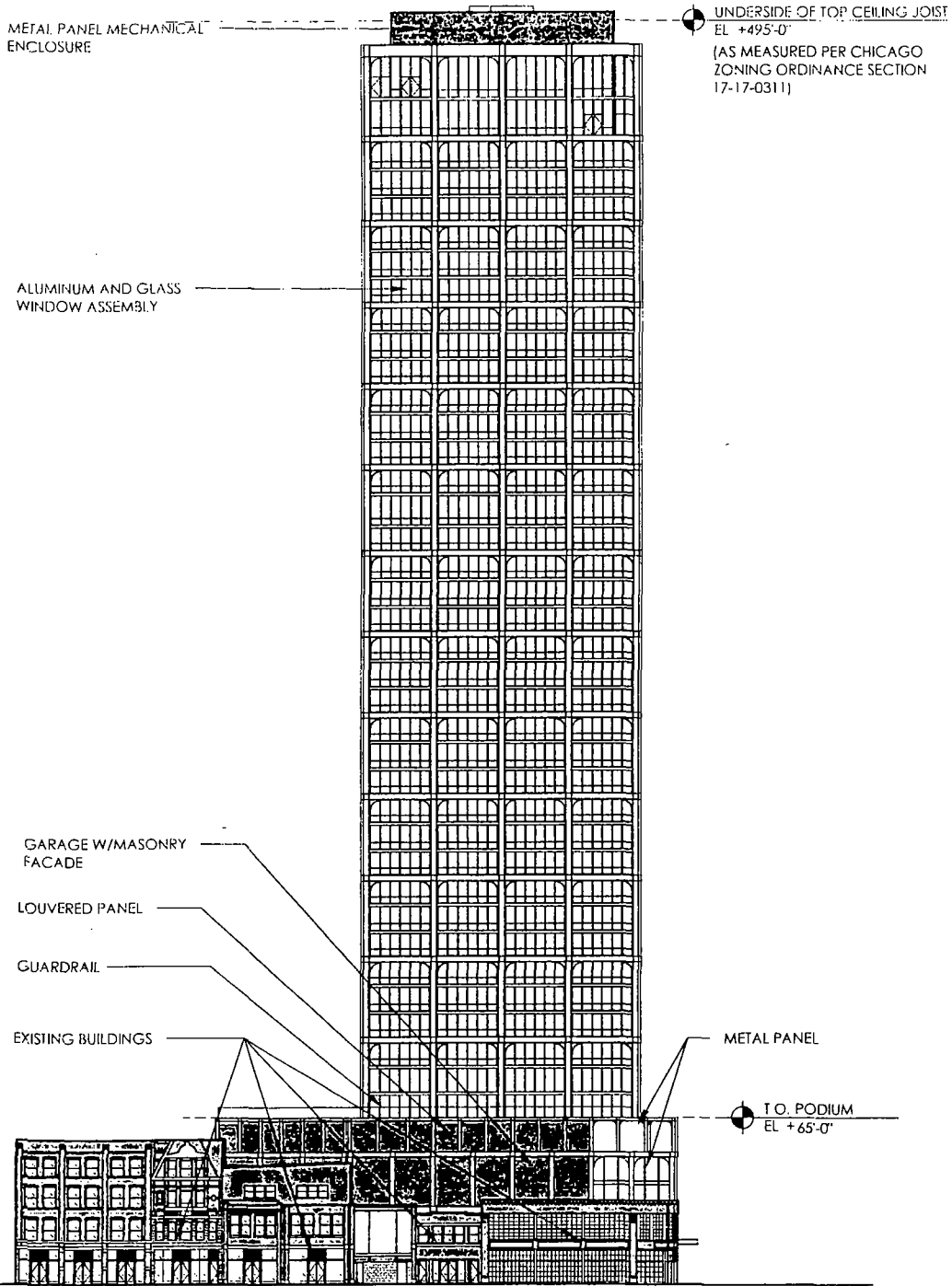
WEST ELEVATION

APPLICANT: 900 BLOCK II HOLDINGS, LLC

ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
151-185 NORTH SANGAMON STREET/913-925 WEST LAKE STREET, CHICAGO, ILLINOIS

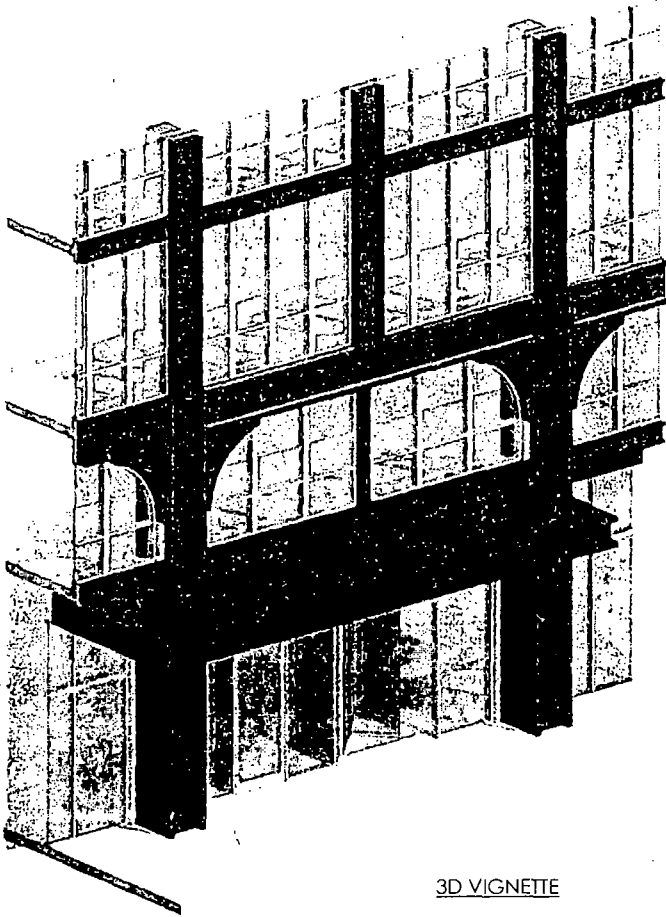
INTRODUCTION DATE: OCTOBER 11, 2017

PLAN COMMISSION: DECEMBER 19, 2019

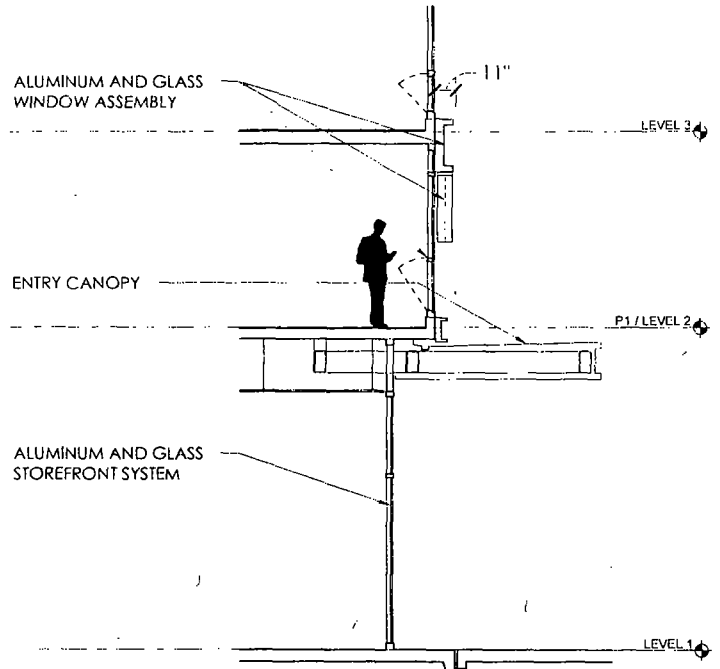


SOUTH ELEVATION

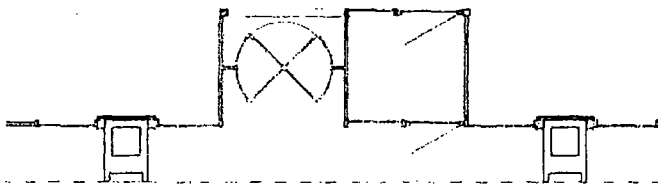
APPLICANT: 900 BLOCK II HOLDINGS, LLC
ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
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INTRODUCTION DATE: OCTOBER 11, 2017
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3D VIGNETTE



PARTIAL SECTION



PARTIAL PLAN

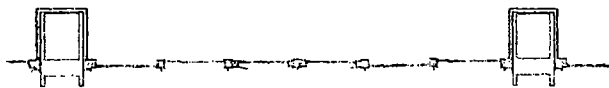
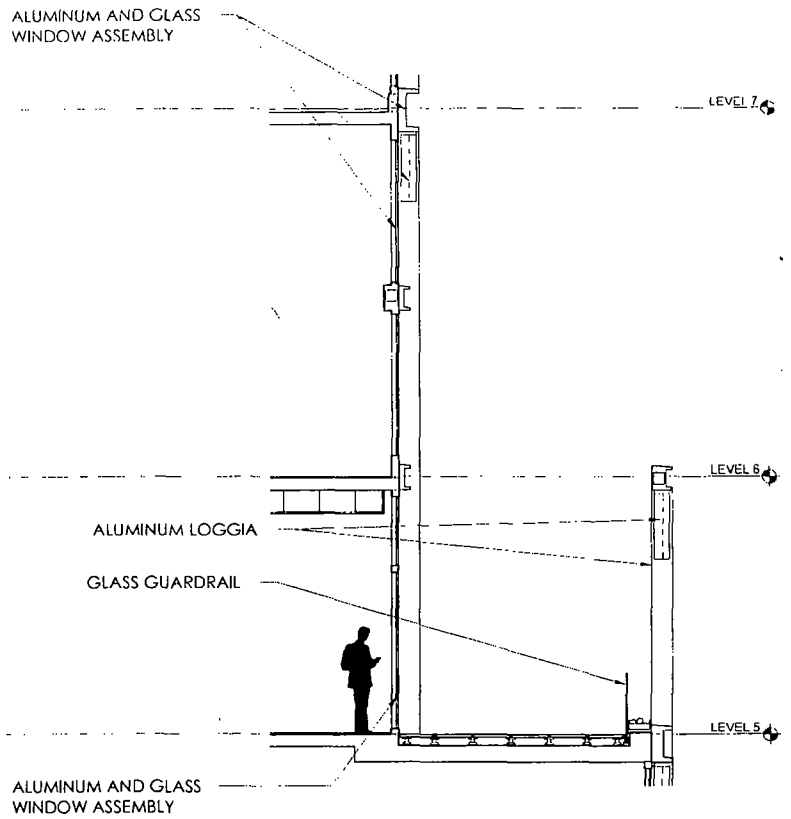
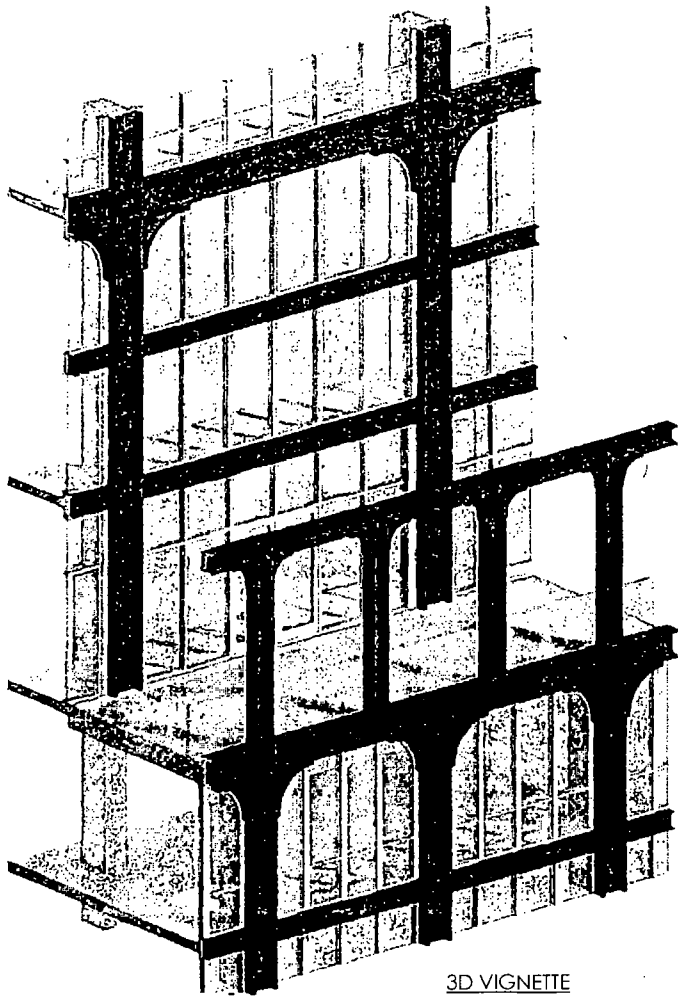
TYPICAL BASE SECTION AND ELEVATION

APPLICANT: 900 BLOCK II HOLDINGS, LLC

ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
151-185 NORTH SANGAMON STREET/913-925 WEST LAKE STREET, CHICAGO, ILLINOIS

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PARTIAL PLAN

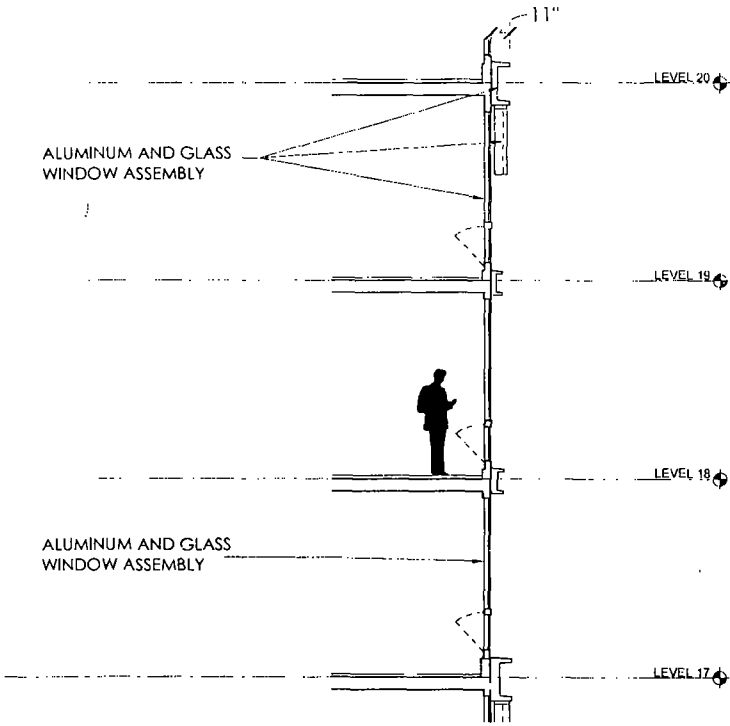
TYPICAL PODIUM SECTION AND ELEVATION

APPLICANT: 900 BLOCK II HOLDINGS, LLC
ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
151-185 NORTH SANGAMON STREET/913-925 WEST LAKE STREET, CHICAGO, ILLINOIS
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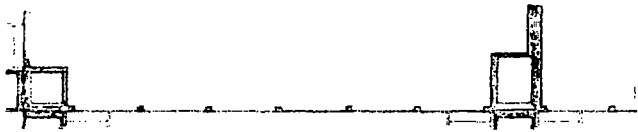
Final for Publication



3D VIGNETTE



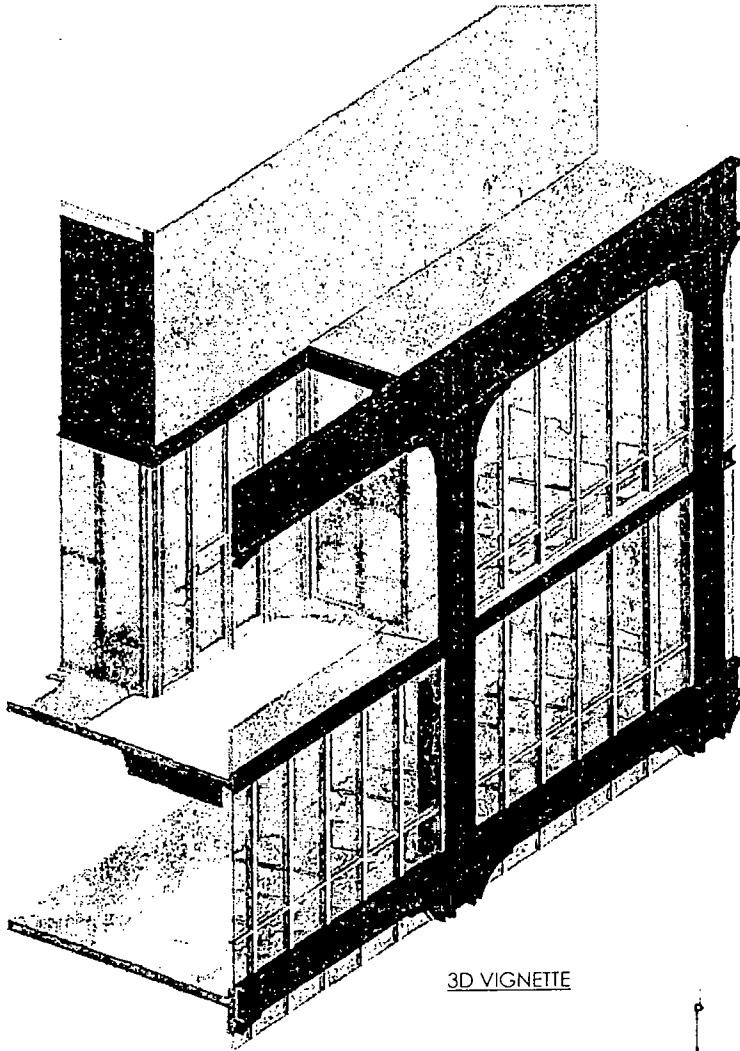
PARTIAL SECTION



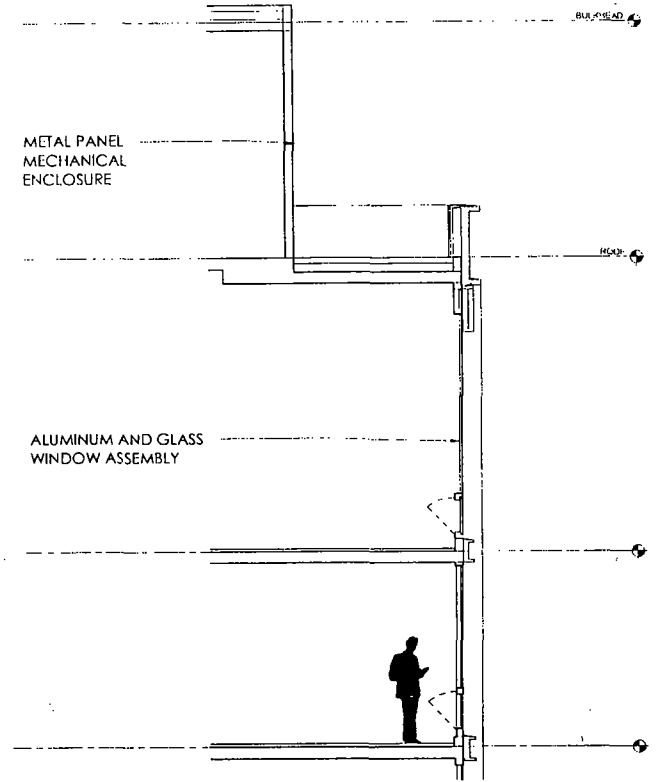
PARTIAL PLAN

TYPICAL TOWER SECTION AND ELEVATION

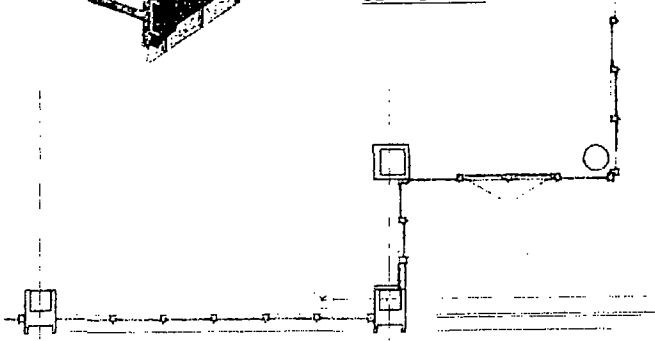
APPLICANT: 900 BLOCK II HOLDINGS, LLC
ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
151-185 NORTH SANGAMON STREET/913-925 WEST LAKE STREET, CHICAGO, ILLINOIS
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3D VIGNETTE



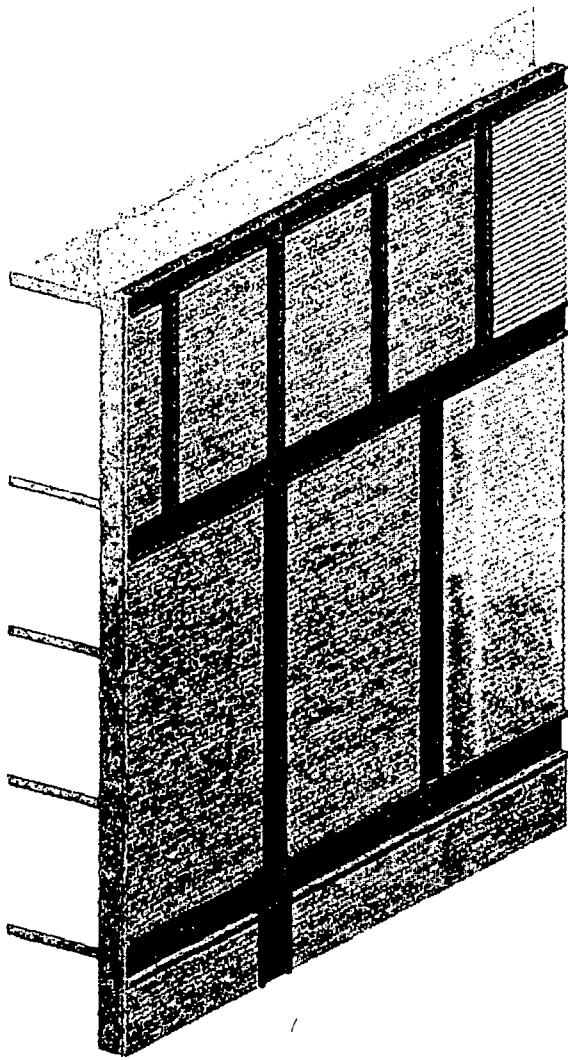
PARTIAL SECTION



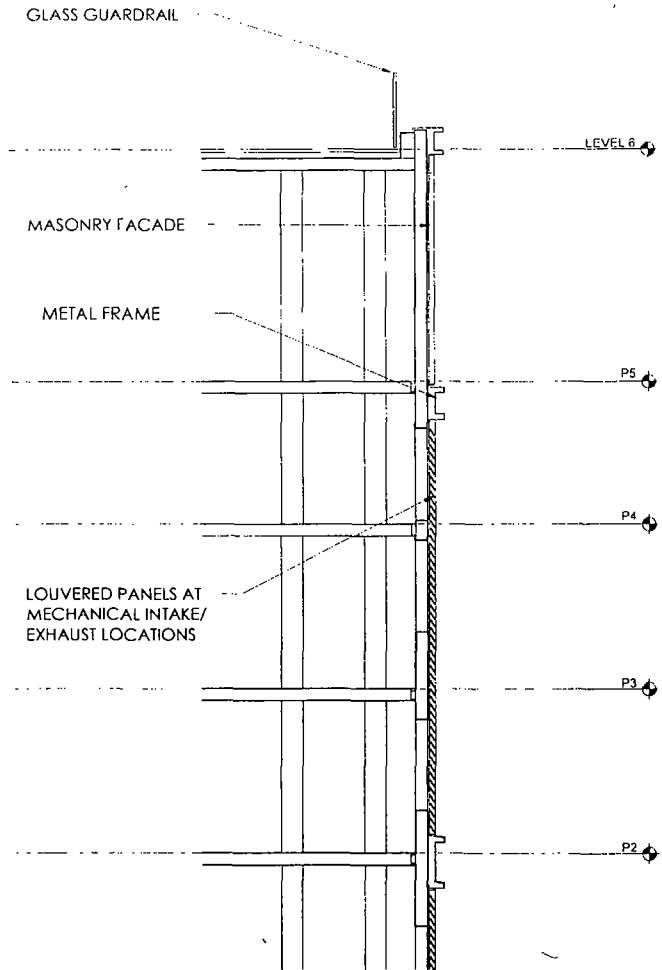
PARTIAL PLAN

TYPICAL PENTHOUSE SECTION AND ELEVATION

APPLICANT: 900 BLOCK II HOLDINGS, LLC
ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
151-185 NORTH SANGAMON STREET/913-925 WEST LAKE STREET, CHICAGO, ILLINOIS
INTRODUCTION DATE: OCTOBER 11, 2017
PLAN COMMISSION: DECEMBER 19, 2019



3D VIGNETTE



PARTIAL SECTION



PARTIAL PLAN

TYPICAL LOUVERED/ MASONRY PODIUM BAY

APPLICANT: 900 BLOCK II HOLDINGS, LLC

ADDRESS: 156-174 NORTH PEORIA STREET/906-908 AND 912-924 WEST RANDOLPH STREET/
151-185 NORTH SANGAMON STREET/913-925 WEST LAKE STREET, CHICAGO, ILLINOIS

INTRODUCTION DATE: OCTOBER 11, 2017

PLAN COMMISSION: DECEMBER 19, 2019

ARO Affordable Housing Profile Form (AHP)

Submit this form for projects that are subject to the 2015 ARO, Near North/Near West Pilot, Milwaukee Pilot or Pilsen/Little Village Pilot Ordinances (all projects submitted to City Council after October 13, 2015). More information is online at www.cityofchicago.gov/ARO. Submit the completed to the Department of Housing (DOH), 121 N LaSalle Street, 10th Floor, Chicago, IL 60602. E-mail: denise.roman@cityofchicago.org or justin.root@cityofchicago.org. Applications that include off-site units should submit documentation listed on page two.

Date: 9/23/2019

DEVELOPMENT INFORMATION

Development Name: 900 W. Randolph

Development Address: 156-174 N. Peoria St, Chicago, IL 60607

Zoning Application Number, if applicable:

Ward: 27th

If you are working with a Planner at the City, what is his/her name?

Type of City Involvement
check all that apply

- City Land
- Financial Assistance
- Zoning increase

- Planned Development (PD)
- Transit Served Location (TSL) project

REQUIRED ATTACHMENTS: the AHP will not be reviewed until all required docs are received

- ARO Web Form completed and attached - or submitted online on
- ARO "Affordable Unit Details and Square Footage" worksheet completed and attached (Excel)
- If ARO units proposed, Dimensioned Floor Plans with affordable units highlighted are attached (pdf)
- If ARO units proposed are off-site, required attachments are included (see next page)
- If ARO units are CHA/Authorized Agency units, signed acceptance letter is attached (pdf)

DEVELOPER INFORMATION

Developer Name Related Midwest

Developer Contact Dani Sassower

Developer Address 350 W Hubbard St; Ste 300, Chicago, IL 60654

Email dsassower@relatedmidwest.com

Developer Phone 312-595-7400

Attorney Name Richard F. Klawiter

Attorney Phone 312-368-7243

TIMING

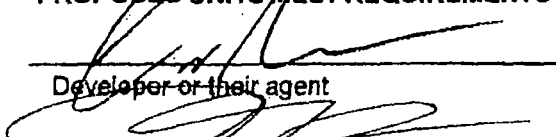
Estimated date marketing will begin November, 2021

Estimated date of building permit* July, 2020

Estimated date ARO units will be complete August, 2022

*The in-lieu fee, recorded covenant and \$5,000 per unit administration fee (for off-site units) are required prior to the issuance of any building permits, including the foundation permit.

PROPOSED UNITS MEET REQUIREMENTS (to be executed by Developer & ARO Project Manager)



 Developer or their agent

 Justin Roof or Denise Roman, DOH

10-2-2019

 Date
10/24/19

 Date



AFFORDABLE REQUIREMENTS ORDINANCE

Applicant Contact Information

Name: Dani Sassower
Email: dsassower@relatedmidwest.com

Development Information

Address

Submitted Date: 09/25/2019

Number From: 156 Number To: 174 Direction: N
Street Name: Peoria Postal Code: 60607

Development Name

900 W. Randolph

Are you rezoning to downtown?: No
Is your project subject to the ARO Pilots?: PILOTS APPLY

Information

Ward: 27 ARO Zone: Higher Income Pilot Area: Near North

Details

ARO trigger: Downtown Planned Development
Total units: 272
Development type: Rent
Date submitted: 09/25/2019

Requirements

First ARO Units: 27 Additional ARO Units: 27

How do you intend to meet your ARO obligation for the First ARO Units?

On-Site: 27 Off-Site: 0
On-Site to CHA or Authorized agency: 0 Off-Site to CHA or Authorized agency: 0
Total Units: 27

How do you intend to meet your ARO obligation for the Additional ARO Units?

Will the Units be 80% AMI or 100% AMI: 80% AMI

On-Site: 27

Off-Site: 0

On-Site to CHA or Authorized agency: 0

Off-Site to CHA or Authorized agency: 0

Total Units: 27

The ARO PM is aware that several floors of the project exceed 50% ARO. No one floor is 100% ARO. The reason for this allowance is due to the fact that 1) the building's design and shape only allows for a limited amount of units per floor, 2) all 20% of the ARO units for this project will be on site, and 3) the unit sizes/mixes meet ARO Rules, including many 2 and 3 bedroom units.

All projects with proposed ARO units must complete this tab

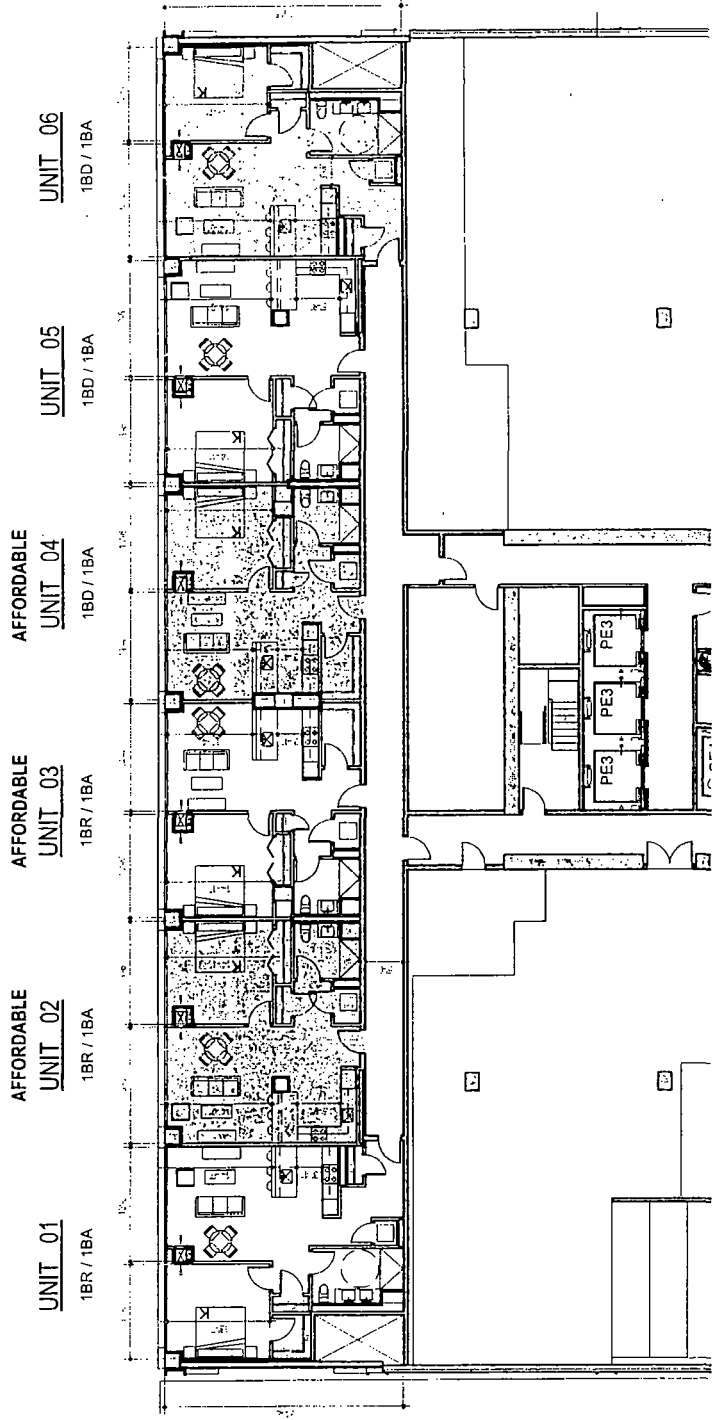
Project Name	900 W. Randolph
Zoning Application number, if applicable	
Address	156-174 N. Peoria
Is this a For Sale or Rental Project?	Rental
Anticipated average psf rent/price?*	\$4 25

Total Units in Project	272
Total Affordable units	54

ALL ON SITE

	Market Rate Units	Affordable Units
	Not Included in Rent; Extra Fee	Not Included in Rent; Extra Fee
	In-Unit	In-Unit
Parking	TBD	TBD
Laundry	TBD	TBD
Appliances	TBD	TBD
Refrigerator	TBD	TBD
age/EnergyStar/make/model/color	TBD	TBD
Dishwasher	TBD	TBD
age/EnergyStar/make/model/color	TBD	TBD
Stove/Oven	TBD	TBD
age/EnergyStar/make/model/color	TBD	TBD
Microwave	TBD	TBD
age/EnergyStar/make/model/color	TBD	TBD
Bathroom(s)	TBD	TBD
how many?	TBD	TBD
Half bath? Full bath?	TBD	TBD
Kitchen countertops	TBD	TBD
material	TBD	TBD
Flooring	TBD	TBD
material	TBD	TBD
HVAC	TBD	TBD
Other	TBD	TBD

unit type	how many?	% of total	avg. square footage	ARO			affordable v. market square footage*
				how many?*	% of total	avg. square footage	
studio	15	7%	615	6	11%	615	100%
one-bed	58	27%	942	15	28%	804	85%
two-bed	111	51%	1,426	28	52%	1,242	87%
three-bed	28	13%	1,955	5	9%	1,663	85%
PH Units	6	3%	2,729	0	0%	#DIV/O!	#DIV/O!
Summary.							
market rate							



L2-L4 Typical Podium Unit Layouts

10/11/2019

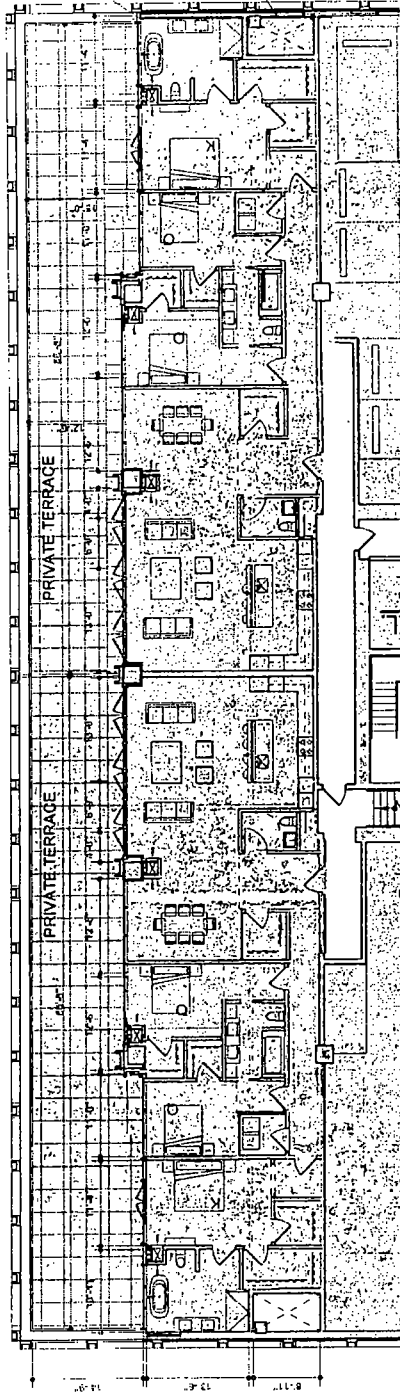
170 North Peoria St.
Chicago, Illinois, 60607

Morris Adjmi Architects
www.ma.com



UNIT 02
3BR / 2.5BA

UNIT 01
3BR / 2.5BA

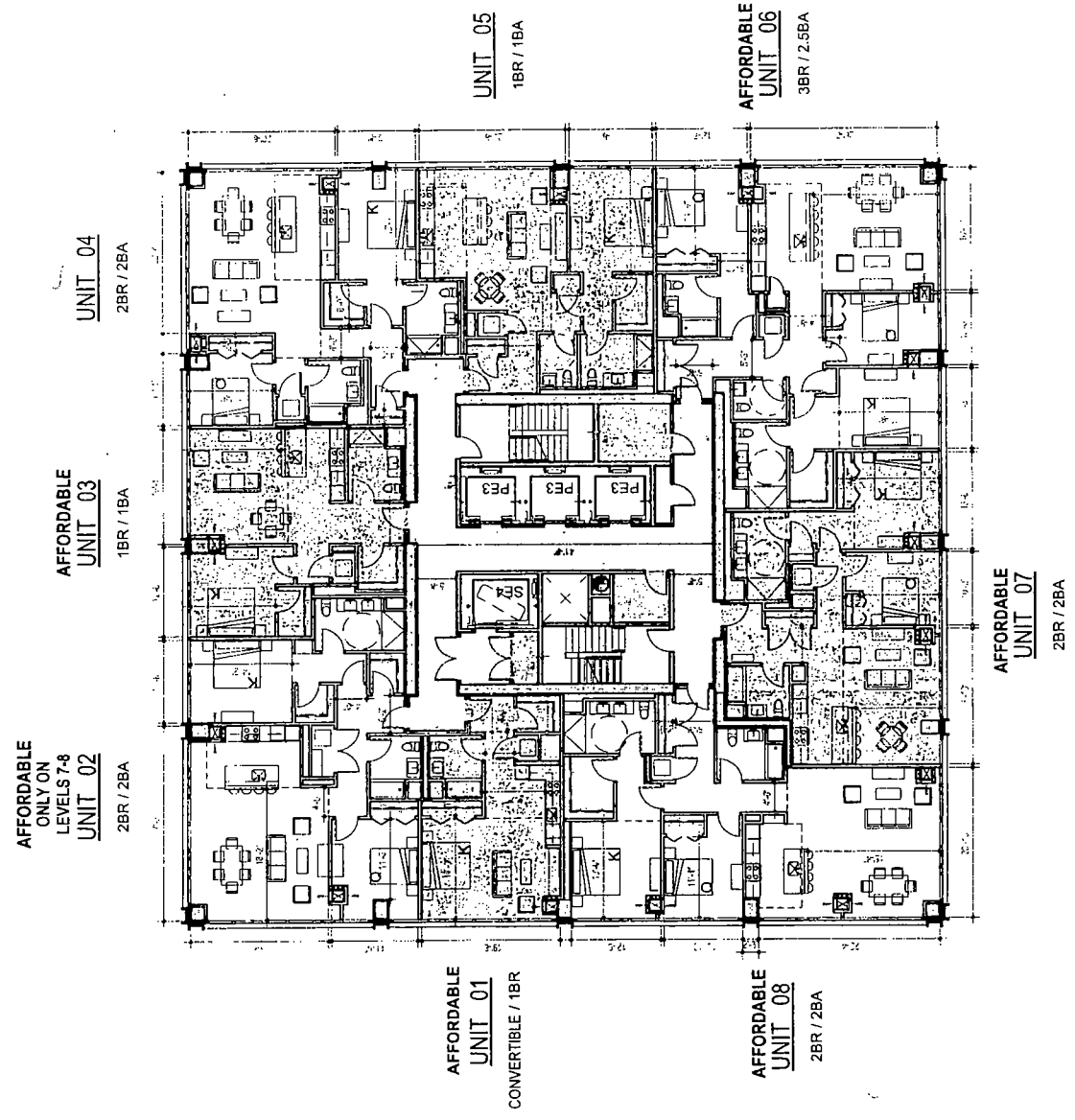


L5 Podium Unit Layouts
10/11/2019

170 North Peoria St.
Chicago, Illinois, 60607

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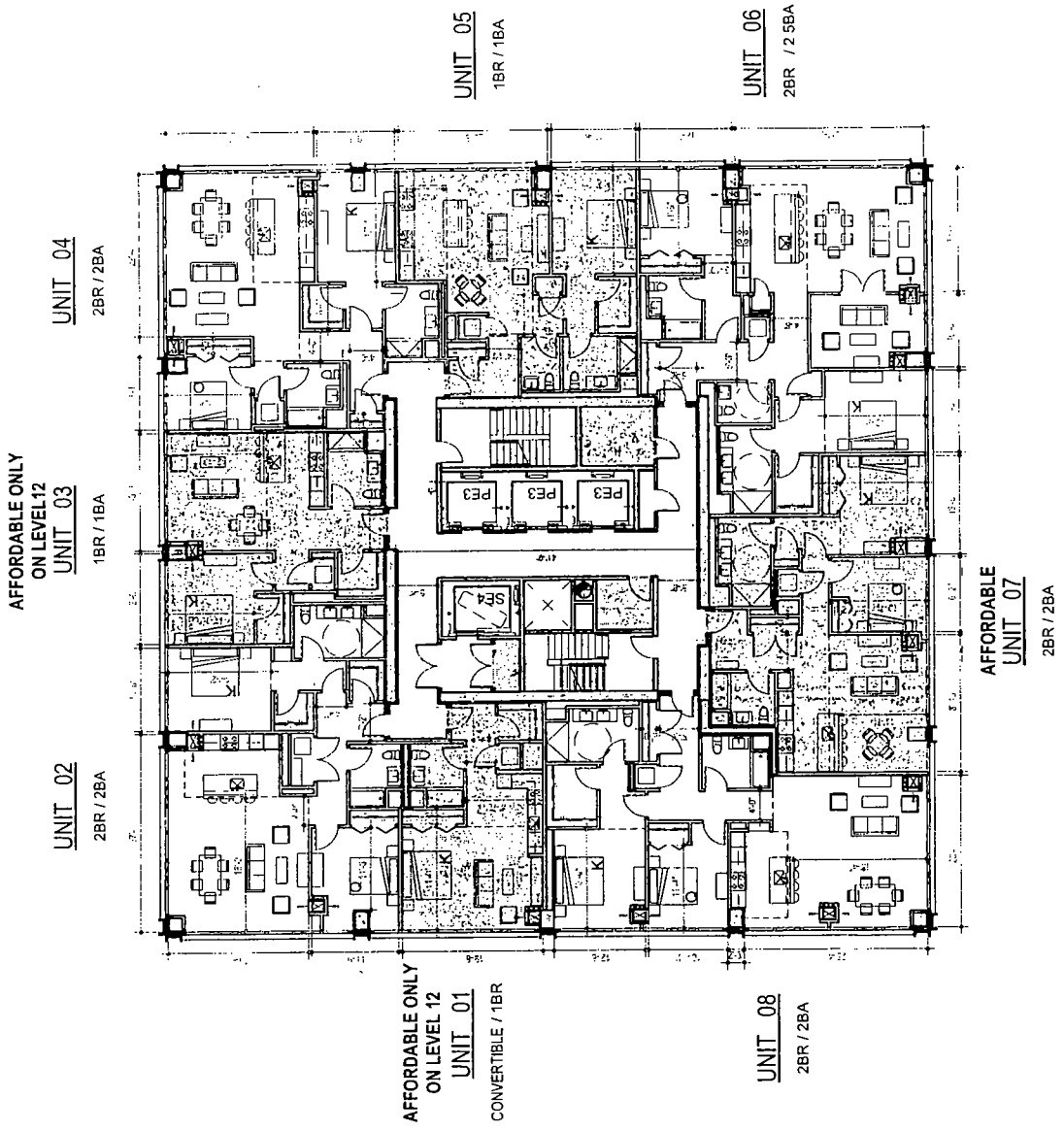


L7-L11 FLOOR PLAN (TYP TIER 1A)
10/11/2019

170 North Peoria St.
Chicago, Illinois, 60607

Morris Adimi Architects
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AFFORDABLE ONLY
ON LEVEL 12

UNIT 04
2BR / 2BA

UNIT 03
1BR / 1BA

UNIT 02
2BR / 2BA

AFFORDABLE ONLY
ON LEVEL 12
UNIT 01
CONVERTIBLE / 1BR

UNIT 05
1BR / 1BA

UNIT 06
2BR / 2.5BA

AFFORDABLE
UNIT 07
2BR / 2BA

UNIT 08
2BR / 2BA

L12-28 FLOOR PLAN (TYP TIER 1B)

10/11/2019

170 North, Peoria St.
Chicago, Illinois, 60607

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UNIT_02
2 BD / 2.5BA

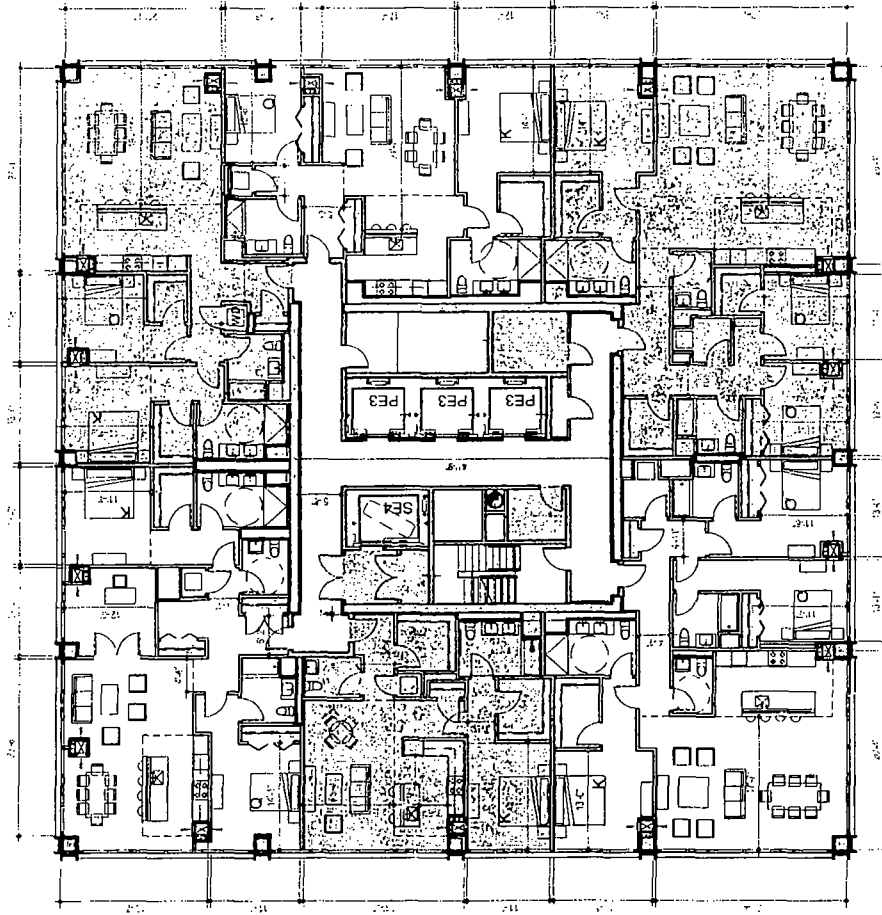
UNIT_03
2BD / 2BA

UNIT_04
2BD / 2BA

UNIT_05
3BD / 2.5BA

UNIT_06
3BD / 3.5BA

UNIT_01
1BD / 1.5BA



L29-L41 FLOOR PLAN (TYP TIER 2)
10/11/2019

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Chicago, Illinois, 60607

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www.ma.com

