



City of Chicago



SO2018-7759

Office of the City Clerk

Document Tracking Sheet

Meeting Date:	9/20/2018
Sponsor(s):	Misc. Transmittal
Type:	Ordinance
Title:	Zoning Reclassification Map No. 1-E at 197-301 N Harbor Dr and 452-500 E Waterside Dr - App No. 19827
Committee(s) Assignment:	Committee on Zoning, Landmarks and Building Standards

Final for Publication

ORDINANCE

BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF CHICAGO

SECTION 1: Title 17 of the Municipal Code of Chicago, the Chicago Zoning Ordinance is hereby amended by changing all the Residential Business Planned Development No. 70, As Amended symbols and indications as shown on Map No. 1-E in an area bounded by:

The south right-of-way line of East Wacker Drive or the line thereof extended where no street exists; a line 3,095 feet east of the west line of North Michigan Avenue; a line 140 feet south of the north line of East Randolph Street, or the line thereof if extended where no street exists; a line 2,022 feet east of the west line of North Michigan Avenue; the north line of East Randolph Street; the east line of North Stetson Street; the north line of East Lake Street; North Michigan Avenue; the north line of East South Water Street; a line 55.53 feet west of the east line of North Beaubien Court, or the line thereof if extended where no street exists; the south line of the alley next north of and parallel to East South Water Street; the east line of the alley next east of and parallel to North Michigan Avenue; a line 138.74 feet north of the north line of the alley next north of and parallel to East South Water Street, or the line thereof if extended where no alley exists; a line 55.53 feet west of the east line of North Beaubien Court, or the line thereof if extended where no street exists; a line 428.773 feet north of the north line of East South Water Street; a line 19.69 feet west of the east line of north Beaubien Court, or the line thereof if extended where no street exists; a line 468.819 feet north of East South Water Street; and a line 68.43 feet east of the east line of North Beaubien Court, or the line thereof if extended where no street exists,

to the designation of Residential Business Planned Development No. 70, As Amended 2018, which is hereby established in the area above described, subject to such use and bulk regulations as are set forth in the Plan of Development herewith attached and made a part thereof and to no others.

SECTION 2: This Ordinance shall be in force and effect from after its passage.

COMMON ADDRESSES FOR PARCELS I, J, and K/L: 452-500 East Waterside Drive

Final for Publication

COMMON ADDRESSES FOR RESIDENTIAL BUSINESS PLANNED DEVELOPMENT NO. 70, AS

AMENDED 2018: 115-479 East Wacker Drive, 150-244 North Lake Shore Drive, 200-474 East Randolph Street, 119-125 East Lake Street, 201-231 North Michigan Avenue, 200-336 North Stetson Avenue, 151-335 North Stetson Avenue, 150-324 North Columbus Drive, 151-323 North Columbus Drive, 150-250 North Field Boulevard, 151-251 North Field Boulevard

Residential-Business Planned Development Number 70, As Amended 2018

Planned Development Statements

1. The area delineated herein as Residential-Business Planned Development Number 70, as amended ("the Planned Development") consists of approximately one million eight hundred fifty-eight thousand nine hundred twenty-two (1,858,922) square feet (forty-two and sixty-seven hundredths (42.67) acres) of property (the "Property") which is divided into seven (7) subareas as depicted in the attached subarea map. Subareas A, B, C, D, F and G have been substantially developed in a manner consistent with this Planned Development. Subarea E is controlled by Lakeshore East, LLC. IJKL, LLC and Lakeshore East are co-applicants for this planned development amendment.
2. All necessary official reviews, approvals or permits are required to be obtained by the applicants as to Subarea E and by the respective owners of the property to Subareas A, B, C, D, F and G.
3. The requirements, obligations and conditions contained within the Planned Development shall be binding upon the applicant, its successors and assigns and, if different than the applicant, the legal title holder and any ground lessors with respect to Subarea E and to the respective owners, their successors and assigns with respect to Subareas A, B, C, D, F and G. All rights granted hereunder to the applicant shall inure to the benefit of the applicant's successors and assigns and if different than the applicant, then to the owners of record title to all of the Property and any ground lessors with respect to Subarea E and to the respective owners, their successors and assigns with respect to Subareas A, B, C, D, F and G. With respect to Subarea E, Lakeshore

Applicant: Lakeshore East LLC and IJKL, LLC

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Plan Commission: October 18, 2018

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East LLC is hereby designated as the controlling entity for purposes of this Planned Development.

4. This plan of development consists of these eighteen (18) statements; a Bulk Regulations and Data Table; an Existing Zoning Map; a Planned Development Boundary Map; a Subarea Map; a Net Developable Area Map for Subarea E; a Generalized Land-Use Plan for Subarea E; a Pedestrian Walkway System – Pedway Level Plan; a Subarea E Parcelization Plans (Upper Level, Intermediate Level and Lower Level); Subarea E Right-of-Way Adjustment Maps (Upper, Intermediate, Lower); Subarea E Public Park Zone Map; Maximum Building Envelope Site Plan; Parcel C Overall Site Plan; Parcel C Buildings I, J, K/L Enlarged Site Plans; Enlarged Site Plan – Chandler Gates; Site Plan – Phasing; Lower Level Five Plan; Parcel C Overall Landscape Plan; Parcel C Buildings I, J, K/L Enlarged Landscape Plans; Enlarged Valley Park/Dog Park Site Plans; Parcel C Overall Vehicular Access Plan (Lower, Intermediate, Upper); Parcel C Overall Pedestrian Access Plan; Parcel C Lower Harbor Drive Service Road Plan Detail, Roadway Cross-Section, Level Five Building Base Elevation Detail: Lake Shore Drive Underpass and Rendering; Parcel C Buildings I, J, K/L Elevations (Base, Middle and Top, East, West, North and South); dated October 18, 2018, prepared by bKL Architecture. (In addition, the exhibits approved in 2015 and found at 12/9/2015 CJP 15713-15723 are incorporated by reference in this 2018 Amendment and all DPD-issued “Minor Change letters” approved after that date.) The Planned Development is applicable to the area delineated hereto and these and no other zoning controls shall apply to the Property, provided, however, that the development of the Property shall also be subject to an ordinance adopted by the City Council and known as the 2000

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Amendatory Lakefront Ordinance and also subject to the Lakeshore East Master Plan and Design Standards dated October 18, 2018 prepared by bKL Architecture.

5. (A) Within Subareas A, B, C, D, F and G at the elevations above Chicago City Datum indicated, provided that these elevations may be varied by plus or minus five (+/- 5) feet or as otherwise necessary to meet existing conditions, and subject to the condition in statement 12 and in the Bulk Regulations and Data Table, the following uses are permitted:

Lower Level

(+ 8.0 feet) Illinois Central Gulf tracks and facilities; warehousing and storage; trucking; public esplanade park along the Chicago River; and accessory uses including automobile parking and hotel uses. (Major service access level)

Interim Level Accessory uses including automobile parking and hotel uses.

Intermediate Level

(+ 26.0 feet) Accessory uses including automobile parking and hotel uses. (Vehicular Access Level)

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Interim Level Accessory uses including retail sales, service used
automobile parking and hotel uses.

Arcade Level Pedestrian walkways; public park; and accessory uses
including retail sales, service uses, automobile parking and
hotel uses.

Upper Level

(+ 53.0 feet) and above Apartment, office, mixed use (apartment-office) and
hotel buildings; accessory uses including retail sales
and service uses; related uses; and pedestrian
walkways. (Vehicular Right-of-Way and Access
Level),

(B) Within Subarea E, the following uses are permitted: public parks; public school;
commercial uses, retail uses, hotel uses, residential uses, business uses, office uses, religious and
institutional uses, warehousing and storage and accessory uses.

In addition, the following uses shall be permitted in all subareas and at all levels subject
to the review and approval of the Department of Planning and Development:

(i) broadcast and telecommunication structures, equipment and installations
including parabolic transmitting and receiving antennae;

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(ii) townhouses and any land-use accessory to a principal use at any level and not specifically authorized in statements 5(A) and (B); and

(iii) public utility and public service uses necessary to serve the development including, but not limited to district electrical generation and utility substations under this Planned Development; and

(iv) district cooling and heating.

6. Business identification signs affixed to the face of or recessed into a building or structure shall be permitted within the Planned Development, subject to the review and approval of the Department of Planning and Development. Temporary construction and marketing signs shall be permitted subject to the review and approval of the Department of Planning and Development. No advertising signs shall be permitted within the Planned Development.

7. Any dedication or vacation of streets, alleys or easements or any adjustment of rights-of-way shall require a separate submittal on behalf of the applicant if within Subarea E, and the respective owners of the property within Subareas A, B, C, D, F and G if within those subareas, and approval by the City Council.

8. Off-street parking and loading facilities shall be provided in compliance with this Planned Development subject to the review of the Department of Transportation and the approval of the Department of Planning and Development. A minimum of two percent (2%) of all parking spaces provided within the Planned Development, in parking structures or areas developed after the effective date hereof, shall be designated and designed for parking for the handicapped.

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9. Any service drive or other ingress or egress shall be adequately designed and paved, in accordance with the regulations of the Department of Transportation in effect at the time of construction and in compliance with the Municipal Code of the City of Chicago, to provide ingress and egress for motor vehicles, including emergency vehicles. There shall be no parking within such paved areas except where provided in public street areas as permitted by the Department of Transportation. Ingress and egress shall be subject to the review and approval of the Bureau of Traffic Engineering and Operations and of the Commissioner of Planning and Development.
10. In addition to the maximum height of the buildings and any appurtenances thereto prescribed in this Planned Development, the height of any improvement shall also be subject to height limitations approved by the Federal Aviation Administration.
11. This Planned Development shall be subject to the "Rules, Regulations and Procedures in relation to Planned Development Amendments" as promulgated by the Commissioner of the Department of Planning and Development and in effect on the date hereof.
12. Subareas A, B, C, D, F and G already have been substantially developed. The further development or redevelopment of properties within Subareas A, B, C, D, F and G shall be subject only to the regulations in Subsections A through G below. The development of Subarea E shall be completed in accordance with all of the regulations contained herein and in accordance with the Guidelines of the Lakeshore East Master Plan and Design Standards dated October 18, 2018.

(A) Net Developable Area.

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For purposes of Floor Area Ratio calculations, the definitions in the Chicago Zoning Ordinance shall apply, provided, however, that "Net Developable Area" refers to the net site area at the Upper Level. The Upper Level is typically at approximately + 53.0 feet above Chicago City

Datum but may be lower or higher depending on factors such as road levels and the location of building entrances and exits as determined by the applicant. Floor Area below the Upper Level shall not be included in calculating the total number of square feet of development unless the principal entrance to the building is located below the Upper Level and, in that event, the level of the principal entrance to the building shall be considered "curb level" for purposes of the Chicago Zoning Ordinance.

(B) Bulk And Density Regulations.

1) Warehousing and storage uses permitted as principal uses in Statement Number 5 shall be limited to four hundred thousand (400,000) square feet and shall be established in accordance with the DX-16 Downtown Mixed-Use District regulations existing on the effective date of this Planned Development, As Amended 2018.

2) With regard to areas devoted as a principal use to office, hotel and residential uses and retail sales and service uses, the following maximums shall apply:

Office, maximum floor area = 12,000,000 square feet

Hotel, maximum number of rooms/keys = 5,550

Residential maximum number of dwelling units = 9,050 units

Retail sales and service, Maximum floor area = 1,325,000 square feet

Provided, however, that in calculating these maximums, ballrooms, meeting rooms, exhibition space and eating facilities associated with a hotel use and located at or above the

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established Upper Level shall be chargeable against the maximum permitted floor area for principal retail sales and service uses.

3) The applicant may increase the maximum number of dwelling units allowed by up to three thousand one hundred eighty-three (3,183) dwelling units by converting a portion of the maximum allowed office space and/or a portion of the maximum allowed hotel rooms/keys. The applicant may increase the maximum amount of office space by up to two million (2,000,000) square feet by converting a portion of the maximum dwelling units and/or a portion of the maximum number of hotel rooms/keys. The applicant may increase the maximum number of hotel rooms/keys by up to one thousand (1,000) rooms/keys by converting a portion of the maximum allowed office space and/or a portion of the maximum allowed dwelling units. For purposes of this conversion one thousand (1,000) square feet of office space shall be equal to one (1) dwelling unit, two (2) hotel rooms/keys shall be equal to one (1) dwelling unit, and one thousand (1,000) square feet of the office space shall be equal to two (2) hotel rooms/keys.

4) Except for Subarea E, the number of efficiency units within this Planned Development shall not exceed thirty percent (30%) of the number of permitted dwelling units. The number of efficiency units in Subarea E shall not exceed fifteen percent (15%).

5) To the extent this Planned Development does not cover all items required for development, the Chicago Zoning Ordinance shall apply as follows: warehousing and storage shall be in general conformity with the DX-16 Downtown Mixed-Use District regulations; permitted office, hotel and retail uses shall be in general conformity with the DX-16 Downtown Mixed-Use District regulations; and residential uses shall be in general conformity with the DR-10 Downtown Residential District regulations.

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(C) Maximum Permitted Site Coverage.

1) East of North Stetson Avenue: (except for Subarea E) fifty percent (50%) between Upper Level and plus thirty (+30) feet above the Upper Level (sixty percent (60%) on a single parcel provided that a single parcel may contain multiple buildings); and forty percent (40%) above plus thirty (+30) feet above the Upper Level.

2) West of North Stetson Avenue: eighty percent (80%) between the Upper Level and plus seventy-five (+75) feet above the Upper Level; and sixty percent (60%) above plus seventy-five (+75) feet above the Upper Level.

3) Subarea E: Site coverage is controlled by the Parcel Design Criteria as described in the Master Plan and Design Standards dated October 18, 2018.

(D) Periphery Setbacks And Minimum Distance Between Buildings.

1) For Subareas A, B, C, D, F and G:

Minimum Distance Between Building Faces at Upper Level: eighty and zero-tenths (80.0) feet.

Minimum Distance Between Building Corners or Building Face-to-Corner at Upper Level: sixty and zero-tenths (60.0) feet.

Periphery setback and distance provisions may be adjusted where required to allow flexibility of architectural or site design arrangement, subject to the approval of the Department of Planning and Development.

2) For Subarea E: Setbacks are controlled by the Master Plan and Design Standards dated October 18, 2018.

(E) Parking

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1) Minimum Requirements for other uses:

Office building: one (1) space per three thousand five hundred (3,500) square feet.

Hotel: one (1) space per three (3) rooms/keys.

Residential building: spaces equal to fifty-five percent (55%) of the dwelling units including efficiency units, provided that with respect to townhome units a minimum of one (1) parking space per dwelling unit shall be required. Subarea E shall require spaces equal to forty percent (40%) of the dwelling units including efficiency units.

2) Location

All parking spaces required to serve buildings or uses shall be located on the same parcel as the building or use served, or (i) if a residential use, within five hundred (500) feet walking distance measured from the property line; or (ii) if a non-residential use, within one thousand (1,000) feet walking distance measured from the property line.

3) Vehicular entrances and exits to accessory automobile parking areas shall be located in conformance with the Automobile Entrance Zone Maps attached hereto. Provided, however, that temporary driveways shall be permitted within the restricted areas depicted on the Automobile Entrance Zone Maps when necessitated by division of parcels and subject to the review and approval of the Department of Transportation.

4) The location, geometrics and operation of all interior or local ramps adjoining any structure at the Upper Level shall be subject to the review of the Department of Transportation and the approval of the Department of Planning and Development.

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(F) Loading.

Minimum off-street loading shall be provided in accordance with the regulations applicable in the DR-10 Downtown Residential District and DX-12 Downtown Mixed-Use District of the Chicago Zoning Ordinance existing on the effective date hereof. The location of loading berths shall be subject to the review of the Department of Transportation and the approval of the Department of Planning and Development.

(G) Construction Of Public Improvements.

No certificate of occupancy shall be issued for any improvement located within a parcel in the development until such time as the Applicant for the certificate produces evidence that construction of public improvements related to the improvement located within a parcel in said development has been completed, is under construction or is under contract for construction, or that adequate access can be provided, all as certified by the Department of Transportation and approved by the Department of Planning and Development. The vertical connections (handicap accessible) adjacent to the Neighborhood Park shall be installed prior to the issuance of a Certificate of Occupancy for the building containing said vertical connection. In addition, the applicant shall use its best efforts to provide landscaping in the parkway area of North Columbus Drive along the frontage of said street adjacent to Subarea E subject to the review and approval of the Department of Planning and Development and the Department of Transportation.

(H) Public Park.

Applicant has provided the Neighborhood Park depicted on the Public Park Zone map and has dedicated the fee simple title of the Neighborhood Park to the Chicago Park

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District. The City of Chicago acknowledges that this satisfies all open space impact fee requirements of the applicant in the development of Subarea E.

(I) Public Elementary School.

Provided the applicant is instructed to proceed by the City of Chicago, the Chicago Board of Education and the Chicago Park District, the applicant shall be responsible for the construction of a forty-five thousand (45,000) square foot portion of a fifty-three thousand (53,000) square foot structure to contain a public elementary school with space to be shared with the Chicago Park District to be substantially completed on or before June 30, 2025.

(J) Pedestrian Walkways.

The pedestrian walkways depicted on the Pedway Level Pedestrian Walkway System Map shall consist of an enclosed all-weather walkway (as depicted on the Pedway Level Pedestrian Walkway System Map), designed to accommodate pedestrian movement at the Arcade Level and/or other levels as depicted in the Pedway Level Pedestrian Walkway System Map attached hereto. It shall be the responsibility of the applicant to provide continuous pedestrian walkways at the Arcade Level and/or other levels within Subarea E pursuant to the Master Plan and Design Standards dated October 18, 2018 subject to the review of the Department of Transportation and the approval of the Department of Planning and Development.

(K) In addition to other requirements contained within these Statements, the Applicant shall cause the following design principles to be implemented:

1) Improve the connection from E. South Water Street to Wacker Drive, including pedestrian safety measures and traffic calming measures at the intersection of Field Boulevard and Sub Wacker Drive;

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- 2) Create a roadway connection from Waterside Drive (upper level) to Wacker Drive, including traffic calming and pedestrian safety measures;
- 3) Create a pedestrian connection at the upper level from Parcel A2 (Tides) to Parcel B2 (Shoreham), which shall include a terrace overlook with specialty paving, landscaping and accent lighting;
- 4) Create a pedestrian connection at the upper level from the east end of Wacker Drive to Parcel B1 (Regatta), which shall include specialty paving, landscaping and accent lighting;
- 5) Create a pedestrian terrace overlook at the new Wacker Drive extension (upper level), which shall include specialty paving, pedestrian seating, accent lighting and significant landscape planters;
- 6) Create a pedestrian connection at the Pedway level from the east property line of the parcel occupying the western portion of Parcel D1 (GEMS) to the property occupying the eastern portion of Parcel D1;
- 7) Create a Vertical Connection from the Upper Level to the Lower Level, with access to the Pedway level as well;
- 8) The character of the through-building public pedestrian connections to Wacker Drive, at both the upper and lower levels, shall be pedestrian friendly, including adequate lighting for safety, decorative paving and a clear marked pedestrian zone.
- 9) Create a pedestrian connection including a vertical connection/public elevator and park from Harbor Drive sloping down to lower Lake Shore Drive adjacent to Parcel C within

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Sub-Area E. (as depicted on the Enlarged Site Plans). The pedestrian connection shall cross under Lake Shore Drive to connect with the Chicago Park District lakefront park system;

10. Create a Vertical Connection/public elevator from Upper Level Parcel C within Sub-Area E to the lowest level with access across Sub Wacker Drive to connect with the Riverwalk.

11. Applicant agrees, in conjunction with the Master Association, to 24/7, 365 days per year security staffing in the guard booth and patrols in and around Lakeshore East. These patrols would be a combination of automobile, bike and walking patrols. From May through October the patrols would be two (2) people from 4pm through 2am and one (1) person the rest of the day. From November through April the patrol would be one (1) person. This staffing level for the patrols would remain in place until such time as new technology is put into place that can provide off-site 24-hour monitoring by a private security company. Even when this new technology is implemented Applicant would agree to provide 24 hour patrols for special events such as Taste of Chicago and Lollapalooza. This staffing would begin upon completion of the guard station and the valley park. In no event shall staffing of the guard booth be eliminated nor hours decreased.

12. Applicant agrees that there shall not be a hotel in Building I.

13. Applicant agrees that it shall deliver the park in Phase I of the IJKL project, along with Building J and Building K/L. Included in this will be the pedestrian and bike paths to the lakefront as well as the security booth, cameras, fence screening, dog park, buffer zones, and all traffic improvements on the service drive in accordance with the attached site plans.

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14. Applicant shall commit \$115,000 (total budgeted cost) to lighting improvements and cameras on Lower Harbor and Intermediate Randolph in accordance with the attached site plan.

15. The IJKL internal road will be one-way northbound and the garage access from this internal road will be ingress only.

16. Applicant has agreed to commit to the following baseline approach to the treatment of Chandler's east blank façade: the painting of the exposed portions of the concrete façade with Modac or some other similar elastomeric coating. Chandler will approve a color that is complimentary to the existing building, pending mock-up. Initial painting will occur around the time of the completion of Building I. Periodic repainting with similar material shall occur when the service life of the coating is visibly or technically compromised, or every ten years, whichever comes first. Maintenance of this façade shall be at the sole expense of the Building I development entity, or assigned to the Building I condominium association (collectively "Building I"). Chandler shall determine whether Chandler or Building I will perform the work. If Building I is to cause the performance of such work, then Chandler will grant access to its window washing equipment as required but in such case all expenses shall be borne by Building I. If Chandler performs the work, the Board will solicit three bids from qualified vendors. The Chandler shall then have their choice bidders so long as the lowest bidder (with a complete scope of work (is within 5% of the nearest bidder and Building I shall promptly reimburse for actual expenses within 60 days. Also note that Chandler and Applicant will consider reasonable alternate treatment for the wall (including landscaping screening of the lower portion) but Building I's financial responsibility will be limited to the cost of the baseline treatment as described above.

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17. Applicant shall be responsible for maintaining the dog park adjacent to Building J.
18. Applicant shall install screening/fence shielding to the area under Lake Shore Drive in accordance with the attached site plans.
19. Applicant shall provide the improvements for the access road, bike and pedestrian paths for the lower level access road at the south of the site in accordance with the attached site plans.
20. Applicant shall continue to work with CDOT and the local Alderman regarding the location of the upper Harbor Drive crosswalk. The local alderman will have final approval on crosswalk location.
21. Applicant shall contribute \$10,000 to the Chandler toward the cost of a gate and shall construct a new gate of mutually agreeable design at the East property line upon completion of Building I in accordance with the attached site plans with reasonable input from DPD, CDOT and the local Alderman.
22. Publicly accessible open space in Parcel C will be open during hours consistent with the Chicago Park District hours for Lakeshore East Park. The Master Association may establish a wayfinding and other appropriate informational signage system, including open space/pedestrian walkway hours, throughout Lakeshore East, subject to approval by DPD and CDOT. In addition, the Master Association may establish a unified system of fencing or other physical additions to restrict access to open space/pedestrian walkways to times consistent with the hours that Chicago Park District facilities are open upon input from DPD and CDOT and the local Alderman.

Applicant: Lakeshore East LLC and IJKL, LLC

Address: 115-479 East Wacker Drive, 150-244 North Lake Shore Drive, 200-474 East Randolph Street, 119-125 East Lake Street, 201-231 North Michigan Avenue, 200-336 North Stetson Avenue, 151-335 North Stetson Avenue, 150-324 North Columbus Drive, 151-323 North Columbus Drive, 150-250 North Field Boulevard, 151-251 North Field Boulevard

Introduced: September 13, 2018

Plan Commission: October 18, 2018

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All the above items shall be subject to further coordination and input with/from the Chicago Department of Transportation and the Department of Planning and Development.

13. Prior to the issuance by the Department of Planning and Development of a determination pursuant to Section 17-13-0610 of the Chicago Zoning Ordinance ("Part II approval") for development or redevelopment of any development parcels within the Planned Development, other than alterations to existing buildings which do not increase their height or alter their footprints, a Site Plan for the proposed development, including parking areas, shall be submitted to the Commissioner of the Department of Planning and Development for approval. Review and approval of the Site Plan by the Commissioner is intended to assure that specific development proposals conform with the general design standards in Statement 12, is consistent with the intent of the Lakeshore East Master Plan and Design Standards dated October 18, 2018 and to ensure coordination of public improvements described in statements 12(G), 12(H), 12(I), 12(J) and 12(K). No Part II approval for work for which a Site Plan must be submitted to the Commissioner shall be granted until the Site Plan has been approved by the Commissioner. Further, all Part II submittals shall be in compliance with the Chicago Landscape Ordinance. Following approval of a Site Plan by the Commissioner, the approved plan shall be kept on permanent file with the Department of Planning and Development and shall be deemed to be an integral part of this Planned Development. The approved Site Plan may be changed by the provisions of Section 17-13-0611 of the Chicago Zoning Ordinance. A Site Plan shall, at a

minimum, provide the following information with respect to the proposed improvements:

Applicant: Lakeshore East LLC and IJKL, LLC

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- (1) the boundaries of the Property;
- (2) the footprint of the improvements;
- (3) location and dimensions of all loading berths;
- (4) preliminary landscaping plan prepared by a landscape architect with final landscaping plan to be approved at Part II stage;
- (5) all pedestrian circulation routes;
- (6) the location of any adjacent public improvements;
- (7) a signage plan for any building where retail or theater uses would be present above the ground level;
- (8) preliminary building sections and elevations of the improvements with a preliminary building materials list; and
- (9) statistical information applicable to the Property limited to the following:
 - (a) floor area and floor area ratio;
 - (b) uses to be established;
 - (c) building heights; and
 - (d) all setbacks, required and provided.

A Site Plan shall include such other information as may be necessary to illustrate conformance with the applicable provisions of this Planned Development.

14. The terms, conditions and exhibits of this Planned Development ordinance may be modified administratively by the Commissioner of the Department of Planning and Development upon the application for such a modification by the Applicant and after a determination by the

Commissioner of the Department of Planning and Development that such a modification is

Applicant: Lakeshore East LLC and IJKL, LLC

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minor, appropriate and consistent with the nature of the improvements contemplated in this Planned Development and the purposes underlying the provisions hereof. Any such modification of the requirements of this statement by the Commissioner of the Department of Planning shall be deemed to be a minor change in the Planned Development as contemplated by Section 17-13-0611 of the Chicago Zoning Ordinance.

15. The applicant acknowledges that it is in the public interest to design, construct and maintain all buildings in a manner which provides healthier indoor environments, reduces operating costs and conserves energy and natural resources. At the time of a hearing before the Chicago Plan Commission all developments must be in substantial compliance with the current City of Chicago Sustainable Development Policy set forth by the Department of Planning and Development. The applicant shall use best and reasonable efforts to design, construct and maintain all buildings located within Subarea E of this Planned Development in an energy efficient manner, generally consistent with LEED Certification or its equivalent. Applicant shall provide a green roof to cover at least 25% of the net roof area of a building. "Net roof area" is defined as a total roof area minus any required perimeter setbacks, roof top structures, and roof-mounted equipment. The Co-Applicants shall comply with the City of Chicago Sustainability Policy in effect at the time of submission for Part II approval for the IJKL Project.

16. The applicant acknowledges that it is in the public interest to design, construct and maintain the project in a manner which promotes, enables, and maximizes universal access throughout the property. Plans for all buildings and improvements on the property shall be reviewed and approved by the Mayor's Office for People with Disabilities ("M.O.P.D.") to

ensure compliance with all applicable laws and regulations related to access for persons with

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Introduced: September 13, 2018

Plan Commission: October 18, 2018

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disabilities and to promote the highest standard of accessibility. No approvals shall be granted pursuant to Section 17-13-0610 of the Chicago Zoning Ordinance until the Director of M.O.P.D. has approved detailed construction drawings for each building or improvement.

17. With respect to the proposed project that is the subject of this 2018 Amendment to Planned Development 70 (The IJKL Project) The Applicant acknowledges that it is the policy of the City to maximize opportunities for Minority and Women-owned Business Enterprises (“M/WBEs”) and city residents to compete for contracts and jobs on construction projects approved through the planned development process. To assist the city in promoting and tracking such M/WBE and city resident participation, an applicant for planned development approval shall provide information at three points in the city approval process. First, the applicant must submit to DPD, as part of its application for planned development approval, an M/WBE Participation Proposal. The M/WBE Participation Proposal must identify the applicant’s goals for participation of certified M/WBE firms in the design, engineering and construction of the project, and of city residents in the construction work. The city encourages goals of (i) 26% MBE and 6% WBE participation (measured against the total construction budget for the project or any phase thereof), and (ii) 50% city resident hiring (measured against the total construction work hours for the project or any phase thereof). The M/WBE Participation Proposal must include a description of the Applicant’s proposed outreach plan designed to inform M/WBEs and city residents of job and contracting opportunities. Second, at the time of the Applicant’s submission for Part II permit review for the project or any phase thereof, the Applicant must

submit to DPD (a) updates (if any) to the Applicant’s preliminary outreach plan, (b) a description

Applicant: Lakeshore East LLC and IJKL, LLC

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of the Applicant's outreach efforts and evidence of such outreach, including, without limitation, copies of certified letters to M/WBE contractor associations and the ward office of the alderman in which the project is located and receipts thereof; (c) responses to the Applicant's outreach efforts, and (d) updates (if any) to the applicant's M/WBE and city resident participation goals.

Third, prior to issuance of a Certificate of Occupancy for the project or any phase thereof, the Applicant must provide DPD with the actual level of M/WBE and city resident participation in the project or any phase thereof, and evidence of such participation. In addition to the forgoing, DPD may request such additional information as the department reasonably determines may be necessary or useful in evaluating the extent to which M/WBEs and city residents are informed of and utilized in planned development projects. All such information will be provided in a form acceptable to the Zoning Administrator. DPD will report the data it collects regarding projected and actual employment of M/WBEs and city residents in planned development projects twice yearly to the Chicago Plan Commission and annually to the Chicago City Council and the Mayor.

18. All minimum construction within Subarea E set forth in Statement No. 17 of the 2002 version of Residential Business Planned Development No. 70 has been completed.

Applicant: Lakeshore East LLC and IJKL, LLC

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Bulk Regulation + Data Table

Residential - Business Planned Development Number 70

Amended Planned Development Use and Bulk Regulations and Data

October 16, 2018

Subarea	Net Development Area (1)		Building Area		Max Net F.A.R.	Residential Dwelling Units		Office (t.f.)		Hotel Rooms (# of Keys)		Retail Sales/Service Area	
	sf	acres	Existing/Under Construction	Future Allowed		Existing/Under Construction	Future Allowed	Existing/Under Construction	Future Allowed	Existing/Under Construction	Future Allowed	Existing/Under Construction	Future Allowed
Subarea A	One Illinois Center	66,104	1.52										
	Hyatt West	52,256	1.20										
	Two Illinois Center	80,085	1.84										
	Hyatt East	73,000	1.68										
	Columbus Plaza	38,154	0.88										
Subarea B	Three Illinois Center	52,560	1.21										
	Subtotal	362,159	8.33	4,419,463		552		1,761,906		2,041		153,958	
Subarea B	ACN Building (Amoco)	136,819	3.14	2,698,308				2,698,308				84,000	
Subarea C	Outer Drive East	88,463	2.03										
	Harbor Point	79,950	1.84										
	Subtotal	168,413	3.87	2,604,235		1,682						42,668	
Subarea D	Buckingham	34,825	0.80										
	175 Harbor Dr.	50,249	1.15										
	Park Shore	41,672	0.96										
	Swissotel	34,000	0.78										
	Farmont	43,169	0.99										
	Athletic Club	21,943	0.50										
	BTE	44,091	1.01										
	BTN	63,660	1.46										
	BTS	53,778	1.23										
	Subtotal	387,387	8.88	5,707,385		1,390		2,699,000		1,337		221,369	
Subarea E	Public School	31,583	0.73										
	Private Development	825,455	18.95	7,126,103		4,029 (4)		286,567 (4)		544 (4)		185,162	
	Subtotal	857,038	19.67	7,126,103		4,029 (4)		286,567 (4)		544 (4)		185,162	
Subarea F	BCBS	100,905	2.32	1,813,819									
Subarea G	Park Millennium	40,641	0.93	582,903									
SUBTOTAL		2,053,362	47.14	24,352,206		8,133		9,260,000		3,922		50,000	
ORDINANCE MAXIMUMS (existing and future, prior to conversion)													
				Building Area		Max Net F.A.R.	Residential Dwelling Units	Office (t.f.)		Hotel Rooms (# of Keys)		Retail Sales/Service Area	
				27,880,245		13.58	9,050	12,000,000		5,500		1,325,000	

(1) Net developable areas for individual parcel components (A, B, C, or D) are taken from original survey as prepared by Dale Weaver

(2) Areas outside of Subarea E provided by the City of Chicago & LE Consultants, Surveyors

(3) Future Allowed figures are subject to potential modifications/transfers provided for in PD 70 and include L, J, K, L, Building C and other future development in Subarea E

(4) Future Allowed figures have been adjusted to reflect an allowed PD conversion of keys and office space to dwelling units (1,000 s.f. office space = 1 dwelling unit and 2 hotel keys = 1 dwelling unit)

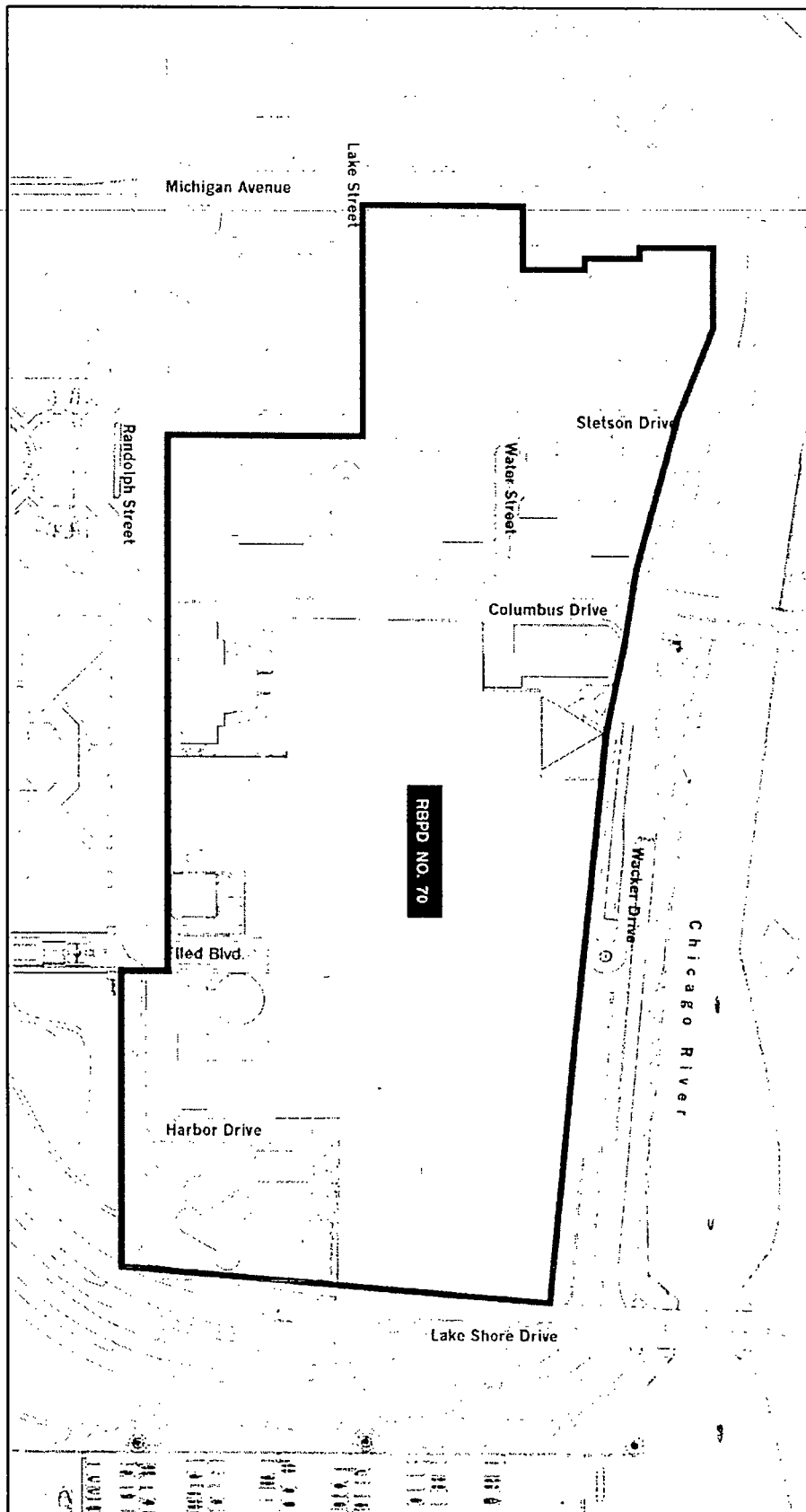
1,760,099 s.f. office space converted

981 hotel keys converted

* Private developable area include public spaces not dedicated. This PD eliminates 10,469 s.f. of dedicated ROW. Note that 17,898 s.f. of public streets not dedicated has been added to the program (included in the Net Developable Area).

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Planned Development Boundaries



Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive

Applicant • IJKL, LLC and Lakeshore East LLC

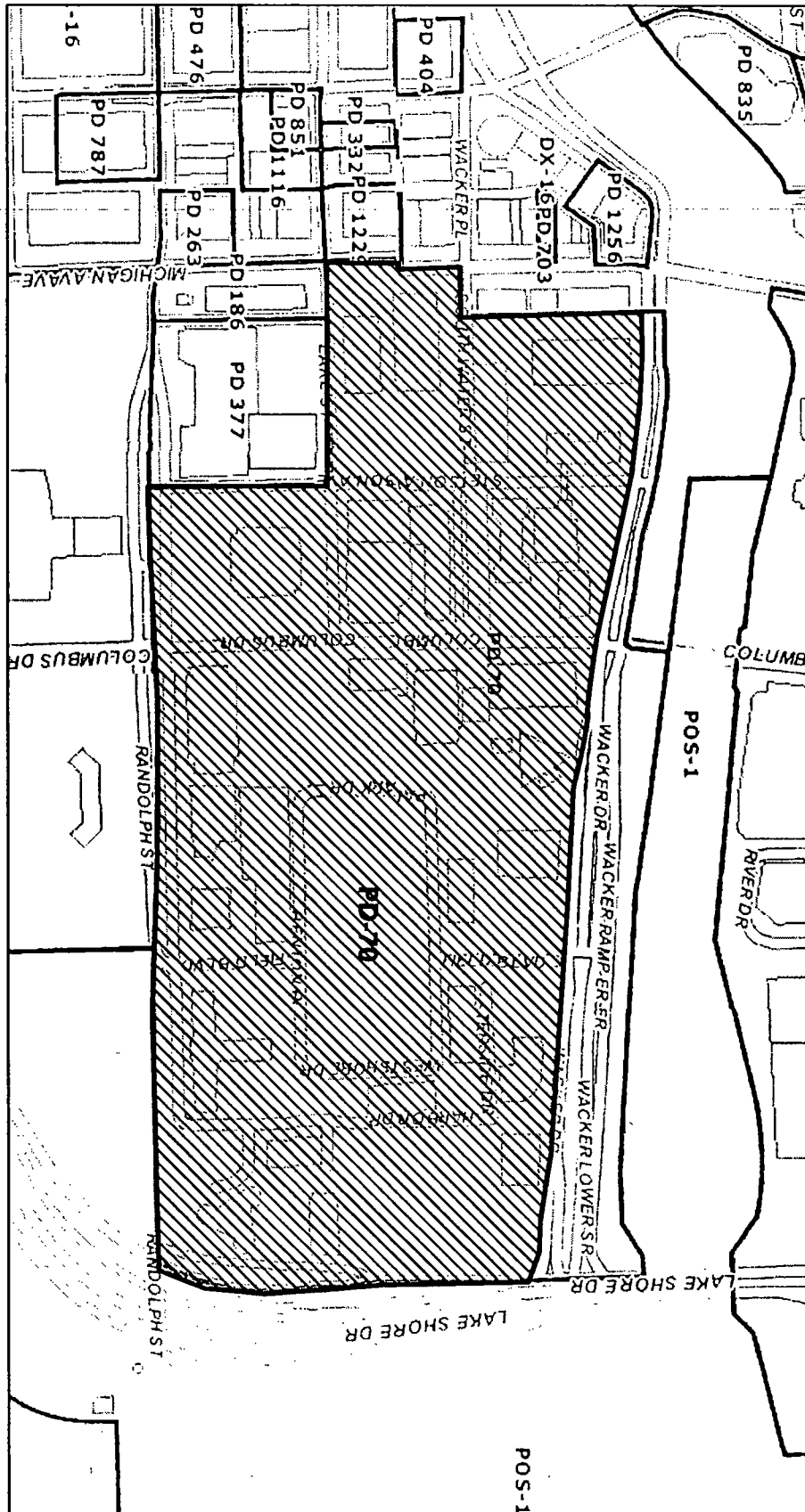
Introduced • September 13, 2018

Plan Commission • October 18, 2018

Existing Zoning Map



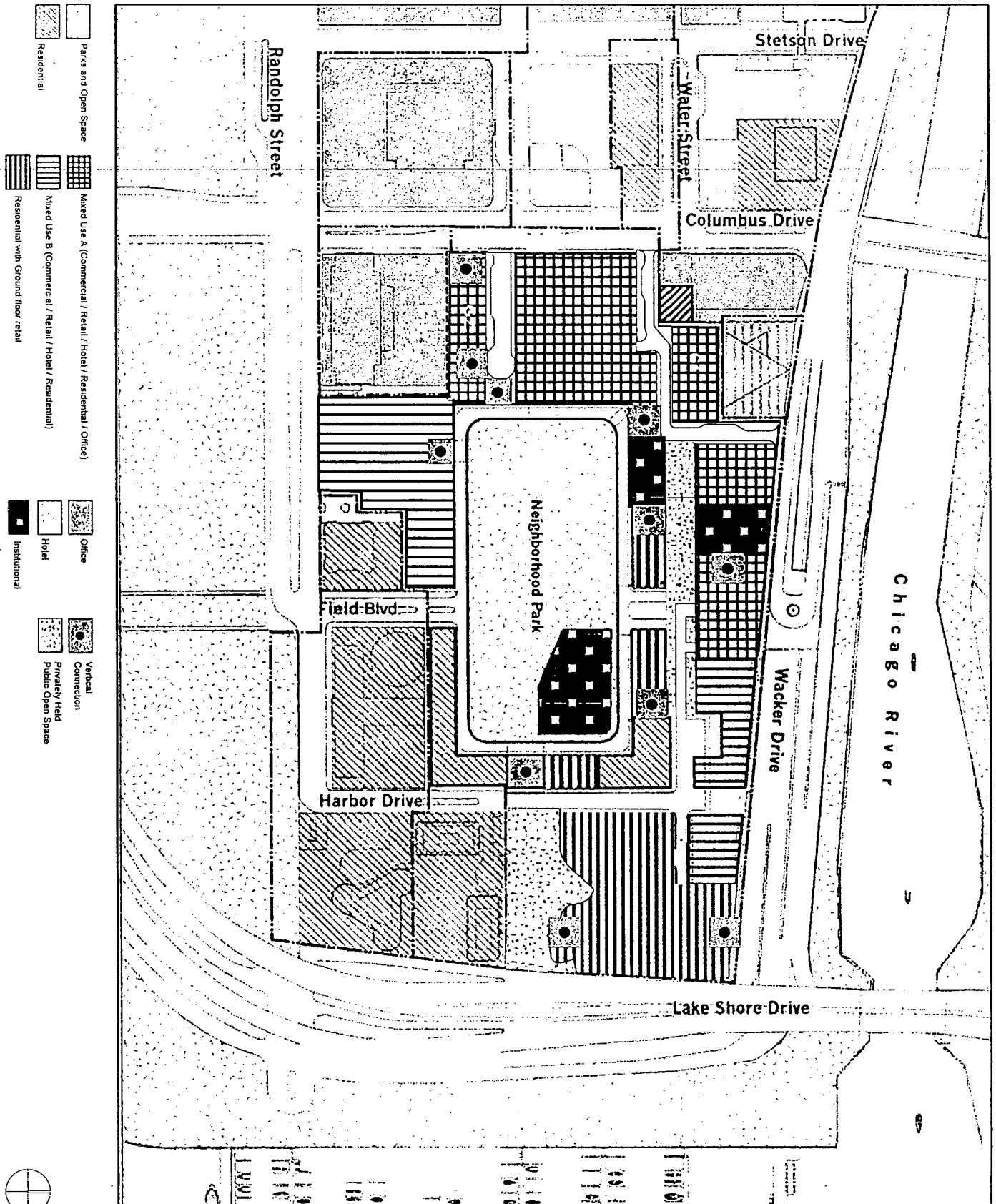
Residential Business Planned Development No. 70



Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
Introduced • September 13, 2018
Plan Commission • October 18, 2018

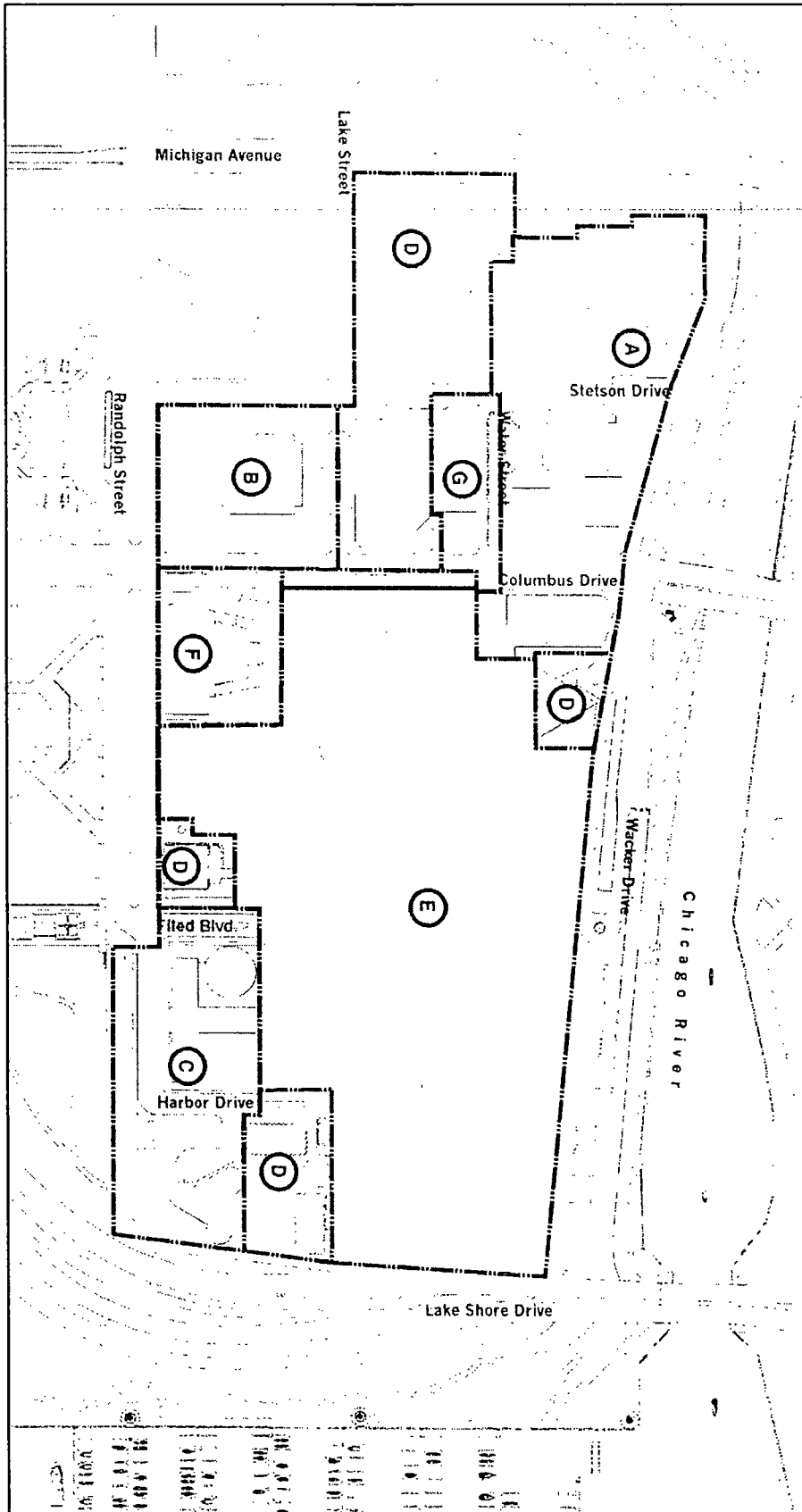
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Generalized Land Use



Address • 197-301 North Harbor Drive and 425-500 East Lakeshore Drive
 Applicant • IJKL, LLC and Lakeshore East LLC
 Introduced • September 13, 2018
 Plan Commission • October 18, 2018

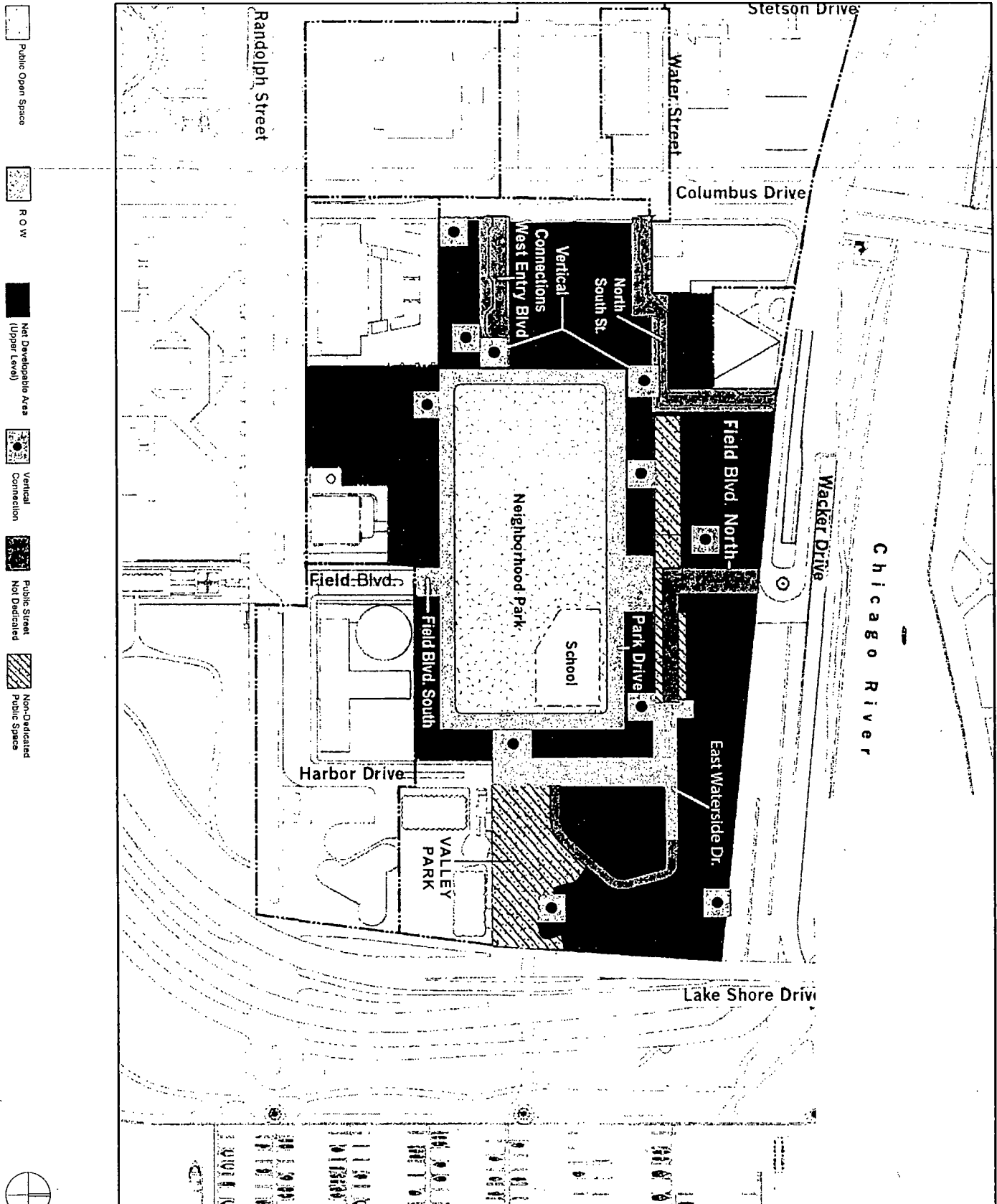
(A) Subarea Identification



Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
 Applicant • IJKL, LLC and Lakeshore East LLC
 Introduced • September 13, 2018
 Plan Commission • October 18, 2018

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Subarea E
Net Development Area

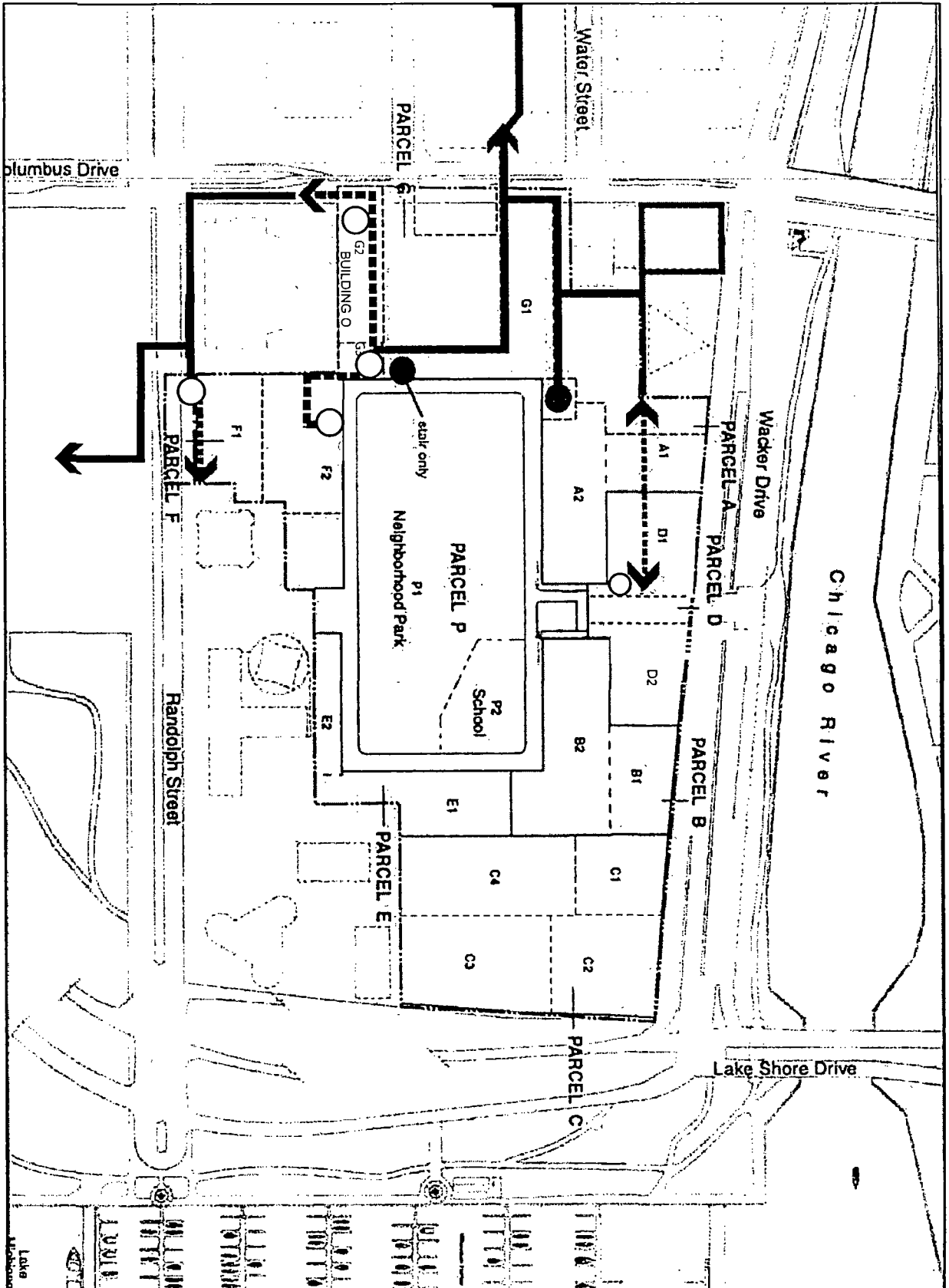


Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
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Pedestrian Walkway System - Pedway Level

-  Existing
 Vertical Connection (Subarea E)
 Pedestrian Connections
 Vertical Connection Within a Building

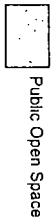


NOTE: This plan is diagrammatic in nature and is intended to show which parcels / sub-parcels should be connected to the parkway system, not the actual route for the connections.



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Subarea E Parcelization Plan - Upper Level
(+/-) elev. +47.00 to +57.33 CCD



Public Open Space



R.O.W.



Development Parcel



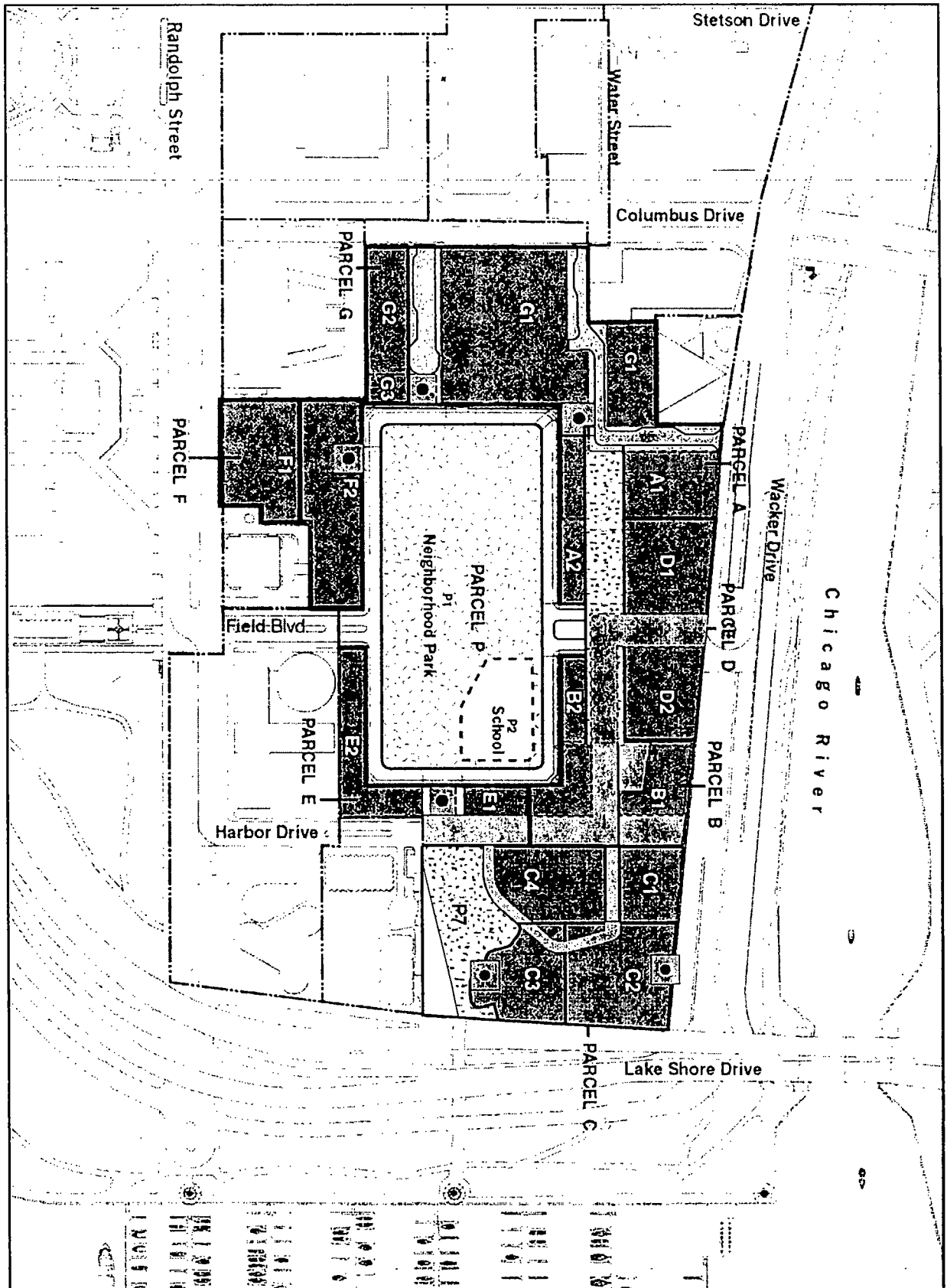
Vertical Connection



Public Street Not Dedicated



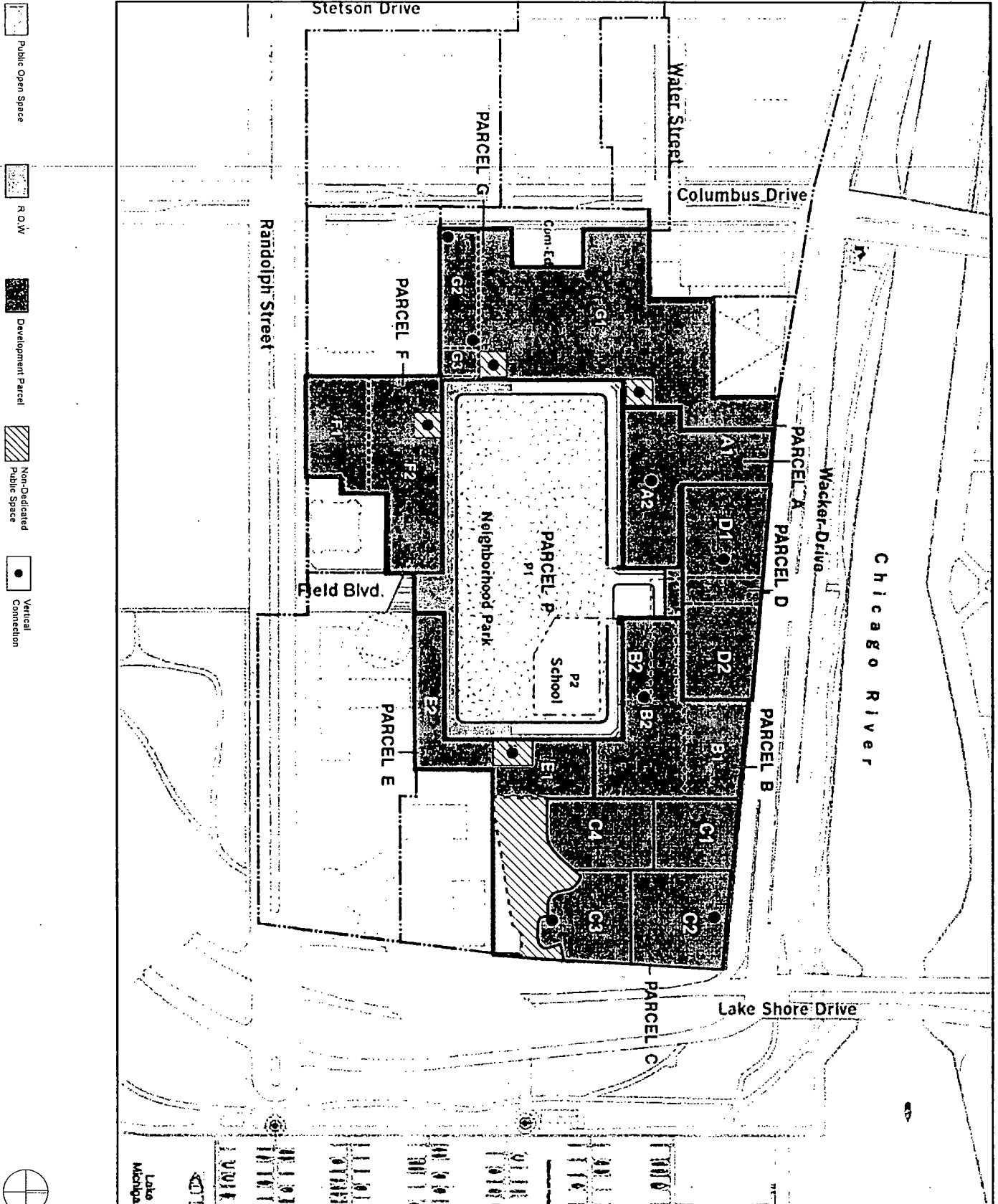
Non-Dedicated Public Space



Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJL, LLC and Lakeshore East LLC
Introduced • September 13, 2018
Plan Commission • October 18, 2018

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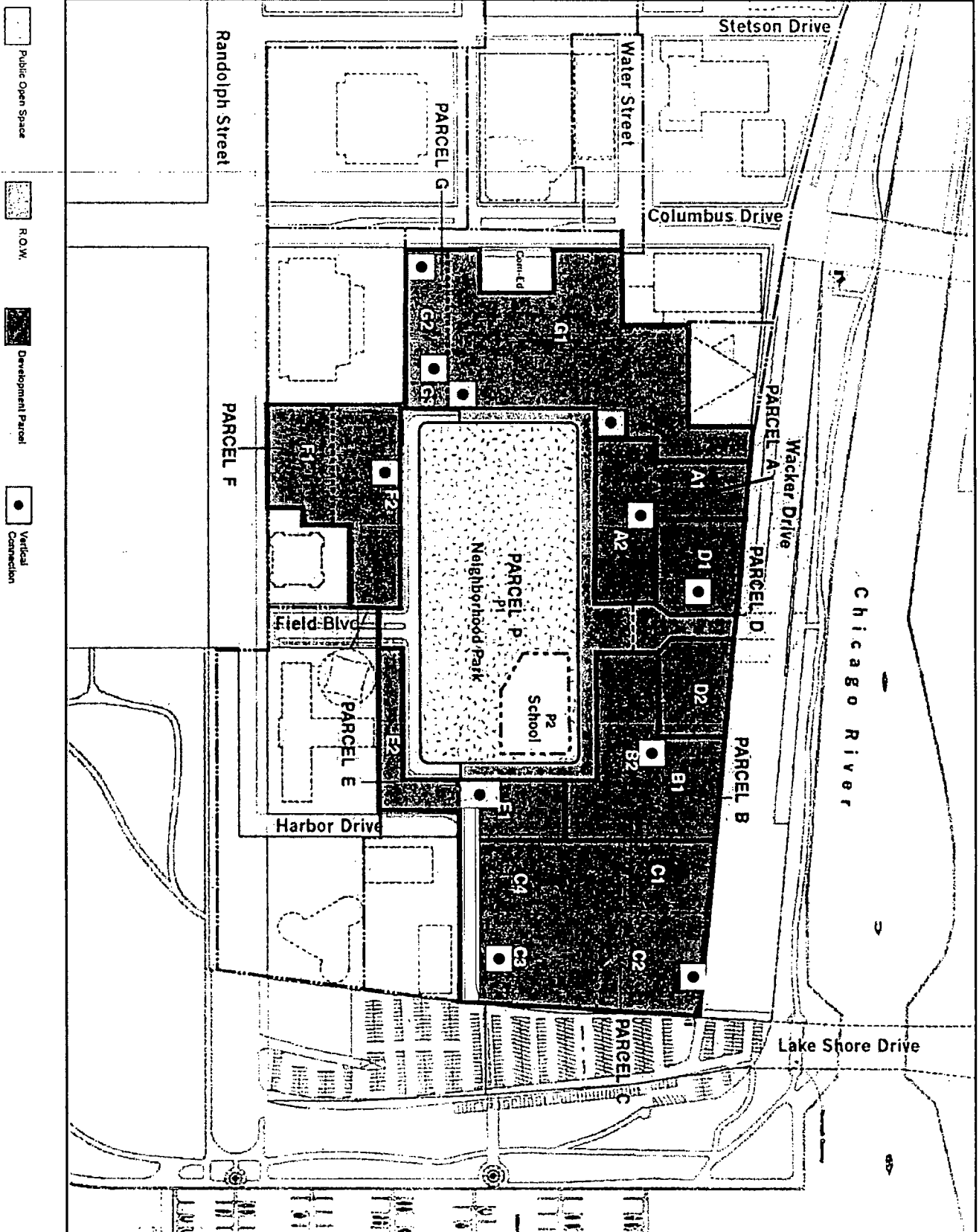
Subarea E Parcelization Plan - Intermediate Level
(+/-) elev. +27.00 CCD



Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
Introduced • September 13, 2018
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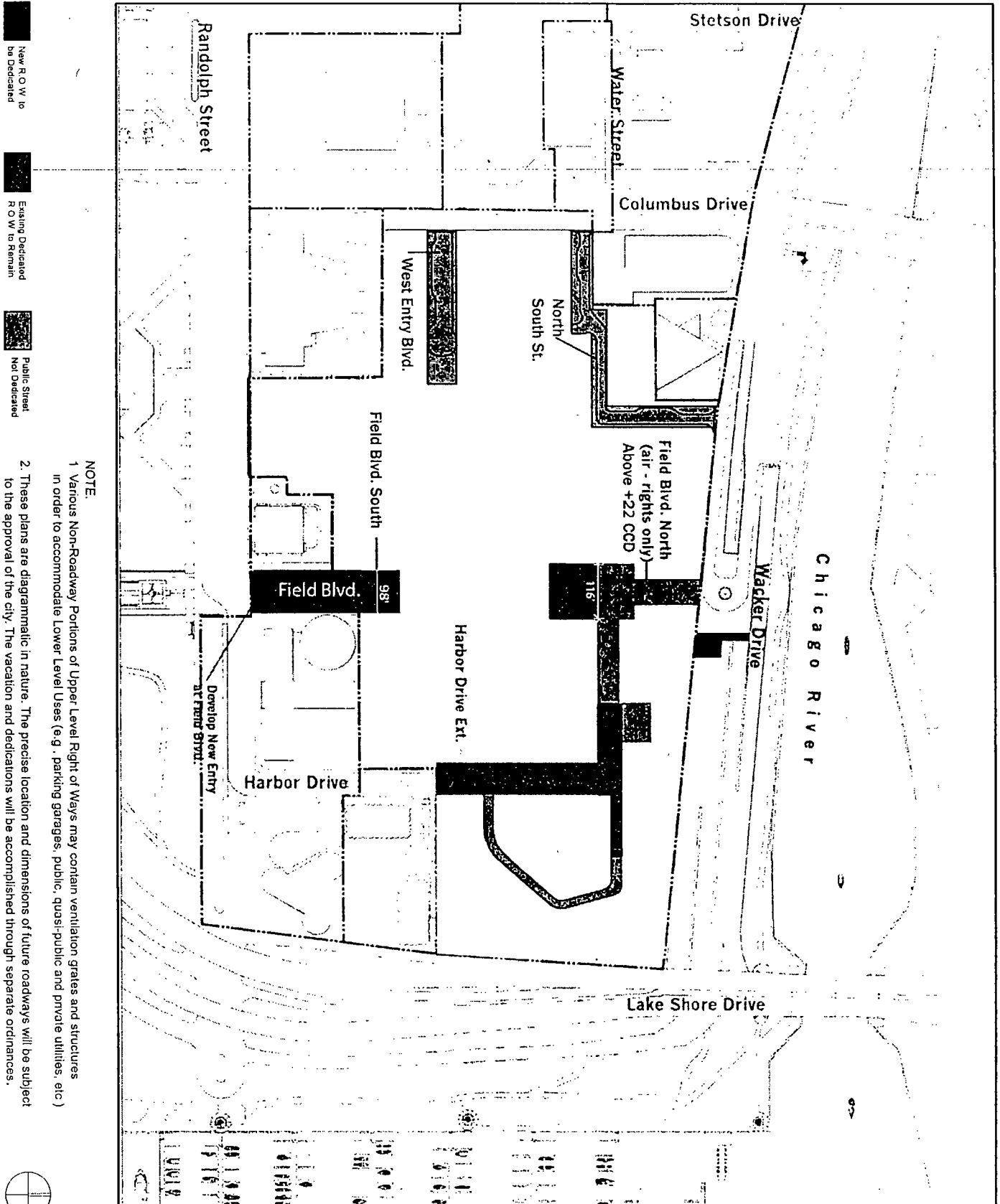
Subarea E Parcelization Plan - Lower Level
(+/-) elev. +6.00 CCD



Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
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R.O.W. Adjustments - Upper Level



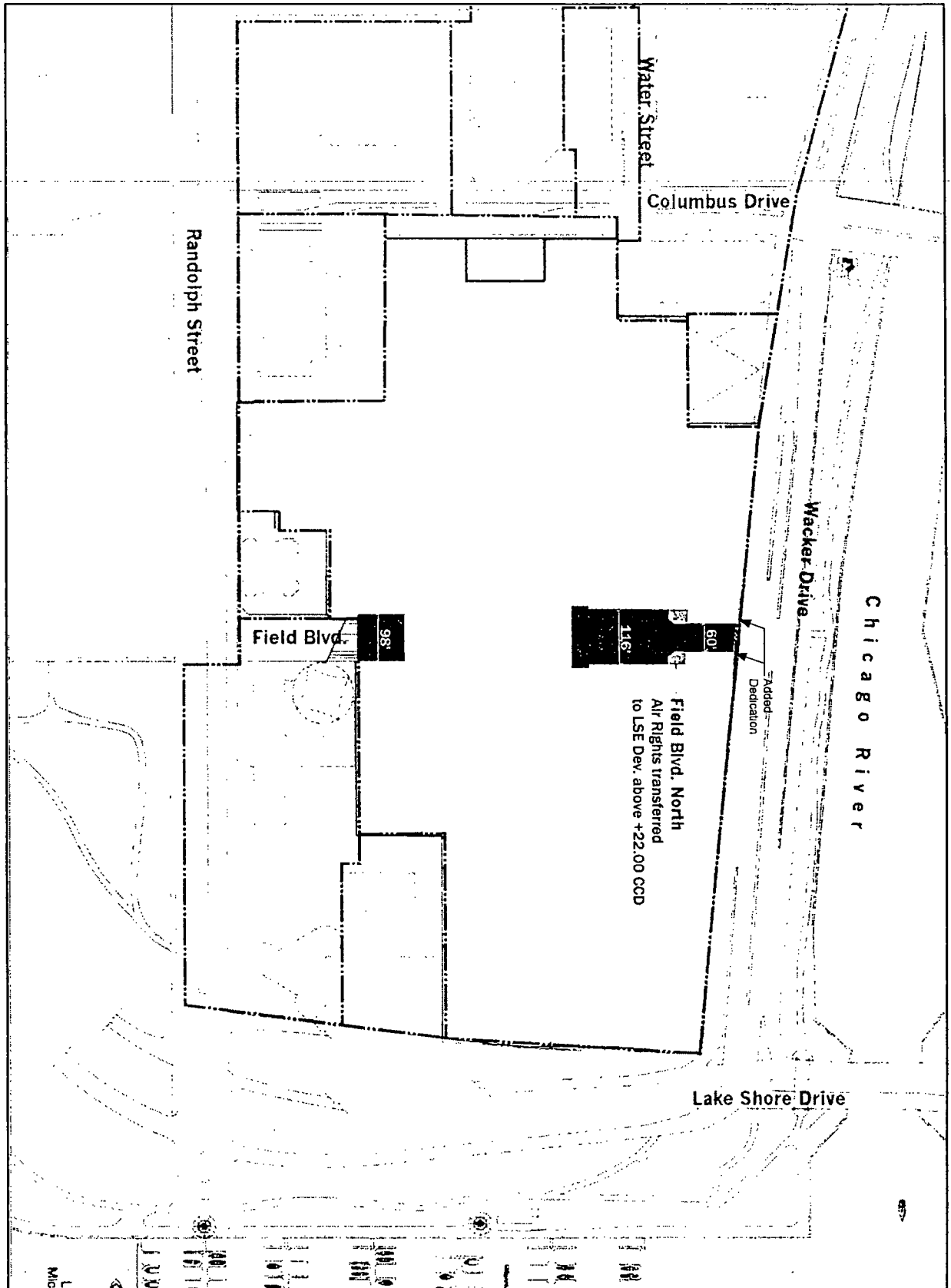
Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
 Applicant • UKL, LLC and Lakeshore East LLC
 Introduced • September 13, 2018
 Plan Commission • October 18, 2018

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R.O.W. Adjustments - Intermediate Level

Note Refer to Street Criteria Drawings for new R.O.W. dimensions & criteria

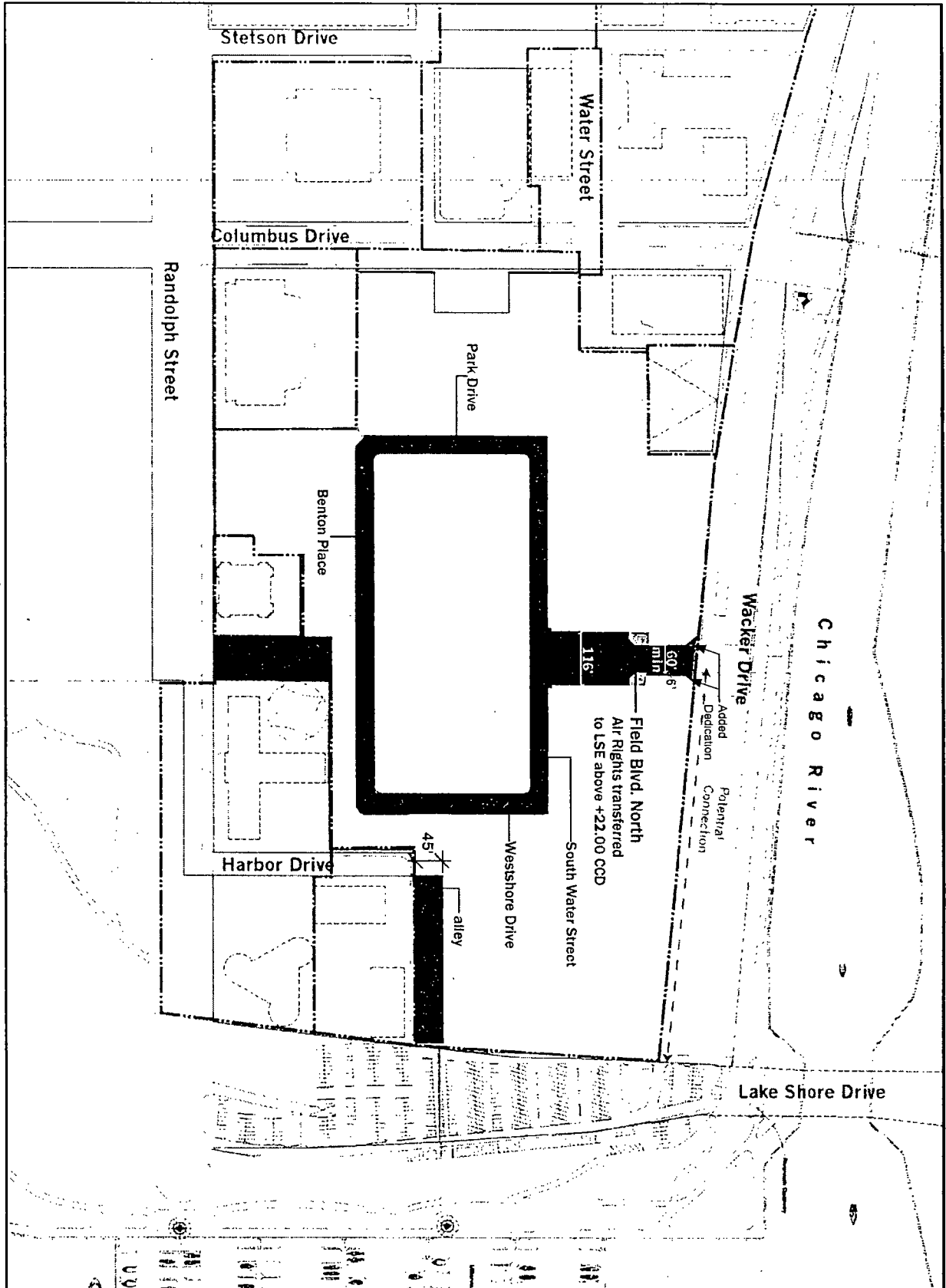
NOTE: These plans are diagrammatic in nature. The precise location and dimensions of future roadways will be subject to the approval of the city. The vacation and dedications will be accomplished through separate ordinances.



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R.O.W. Adjustments - Lower Level



R.O.W. to be
Vacated



New R.O.W. to
be Dedicated



Existing Dedicated
R.O.W. to Remain

Note: Refer to Street Criteria Drawings for new R.O.W. dimensions & criteria

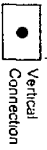
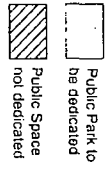
NOTE: These plans are diagrammatic in nature. The precise location and dimensions of future roadways will be subject to the approval of the city. The vacation and dedications will be accomplished through separate ordinances.



Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • JKL, LLC and Lakeshore East LLC
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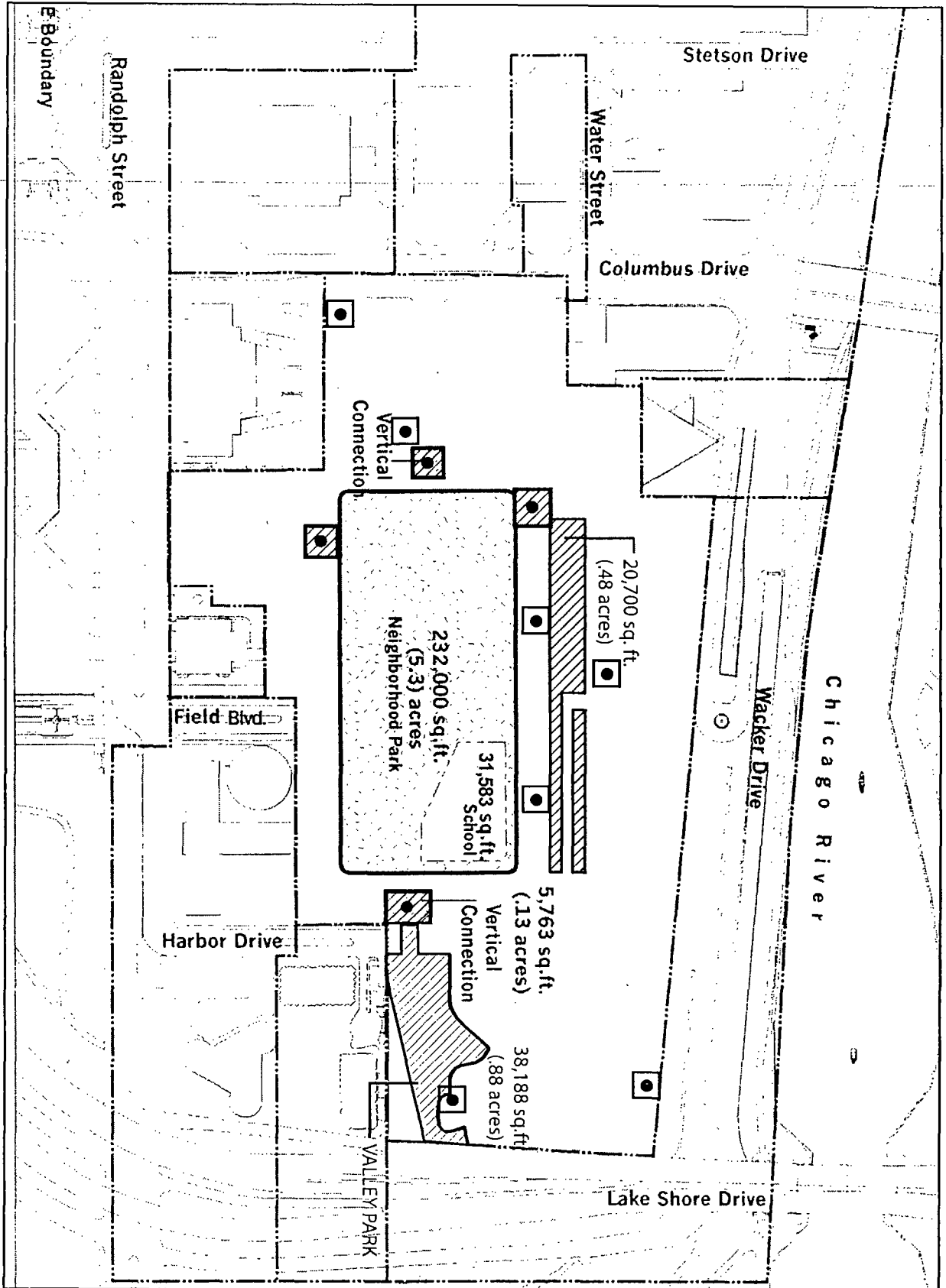
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Public Park Zone



Total Public Park: 232,000 sq.ft.
Land Area: 5.3 acres
Total School: 31,583 sq.ft.
Land Area: .73 acres

NOTE: These plans are diagrammatic in nature and they indicate the approximate location and intent of the school location, the spaces to be dedicated as public parks and the vertical connections, but not the precise locations.
 The amount of space dedicated as public park is subject to acceptance by the Chicago Park District.



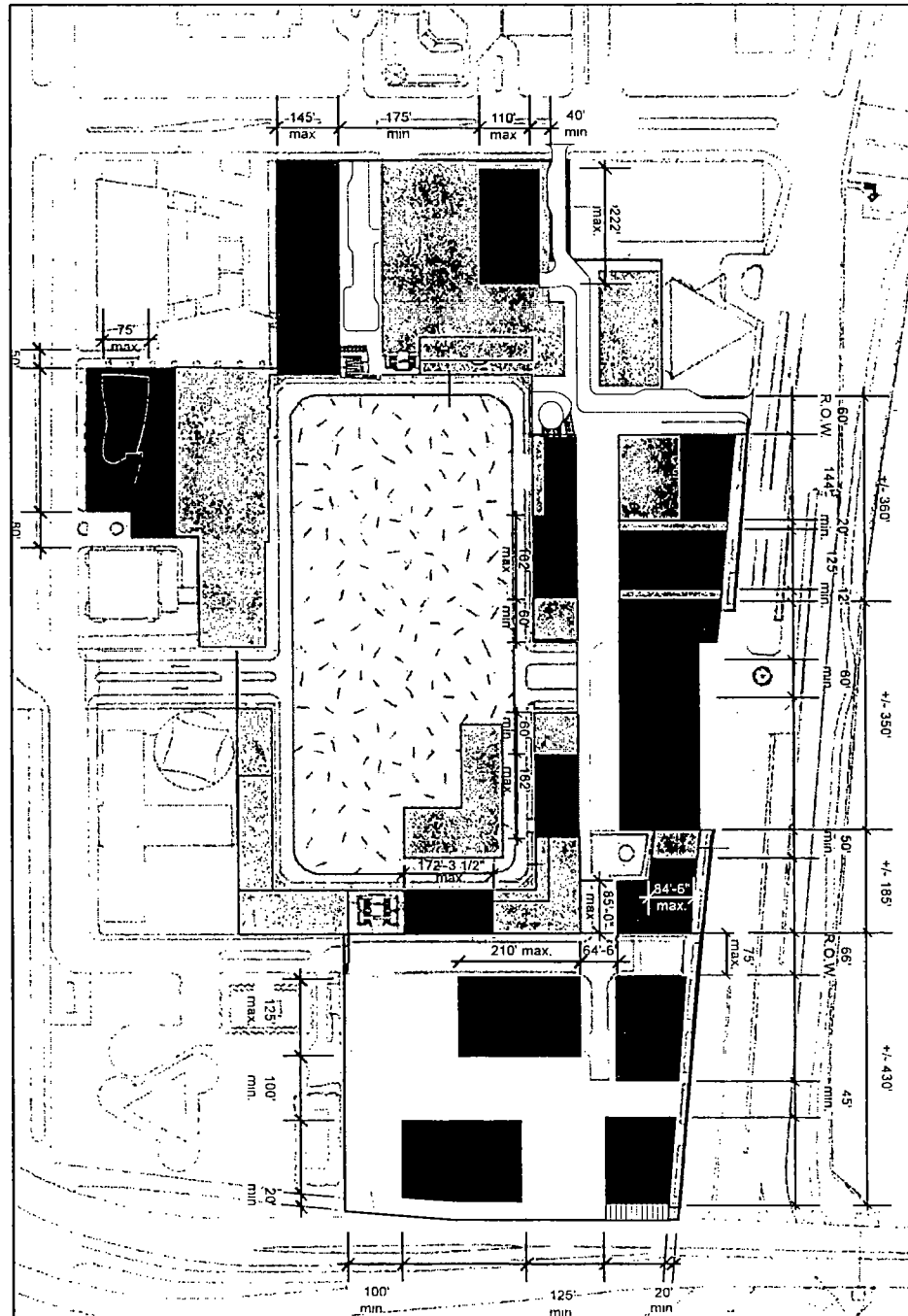
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Applicant • IJKL, LLC and Lakeshore East LLC
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Plan Commission • October 18, 2018

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Maximum Building Envelope Site Plan

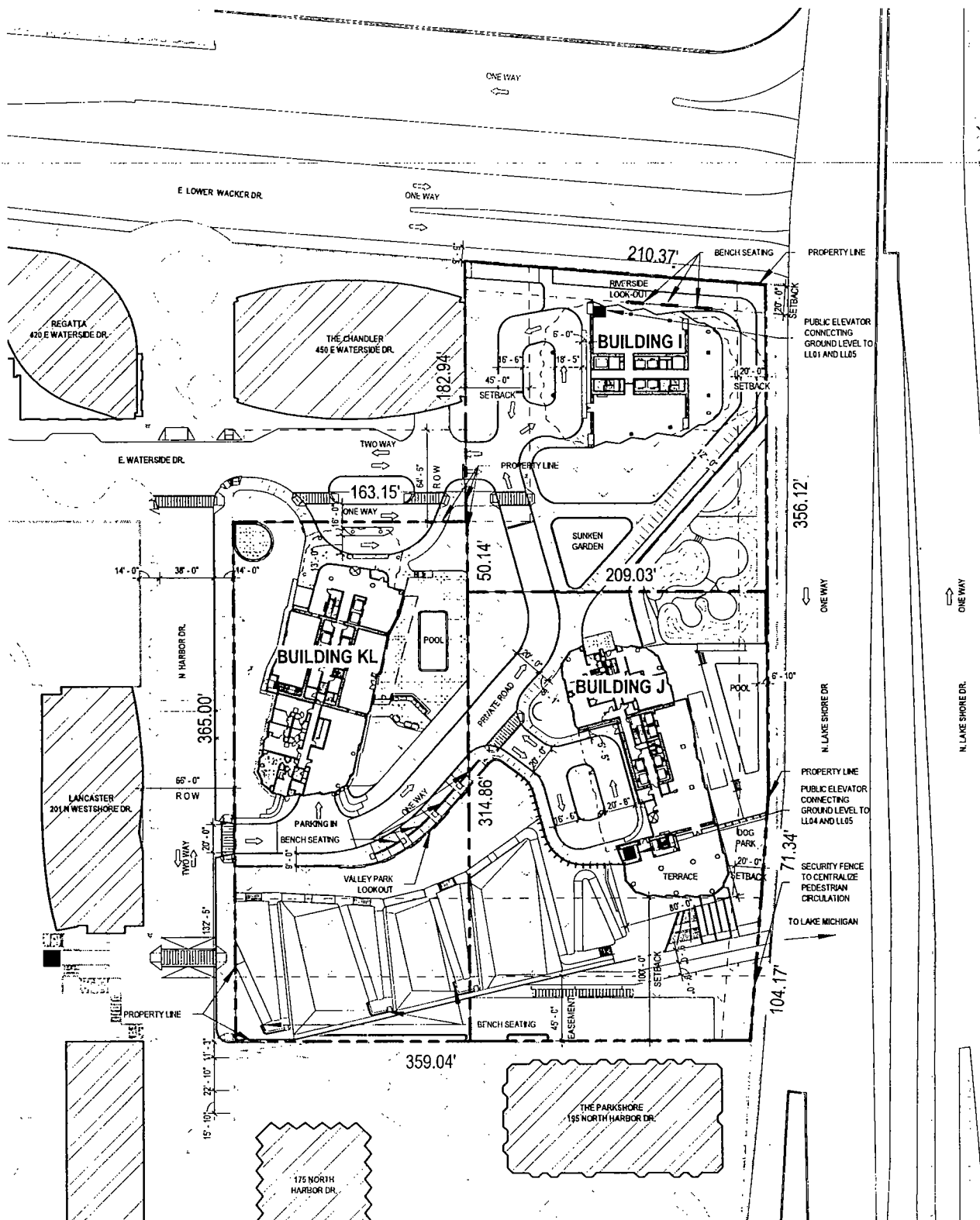
 Highrise Buildings -
Maximum Building Envelopes
 Midrise and lowrise Buildings -
Maximum Building Envelopes
 Highrise Buildings -
Maximum Building Envelopes above 20'

NOTE: All overall dimensions should be verified with land survey
NOTE: These plans are diagrammatic in nature and are only intended to show approximate locations.



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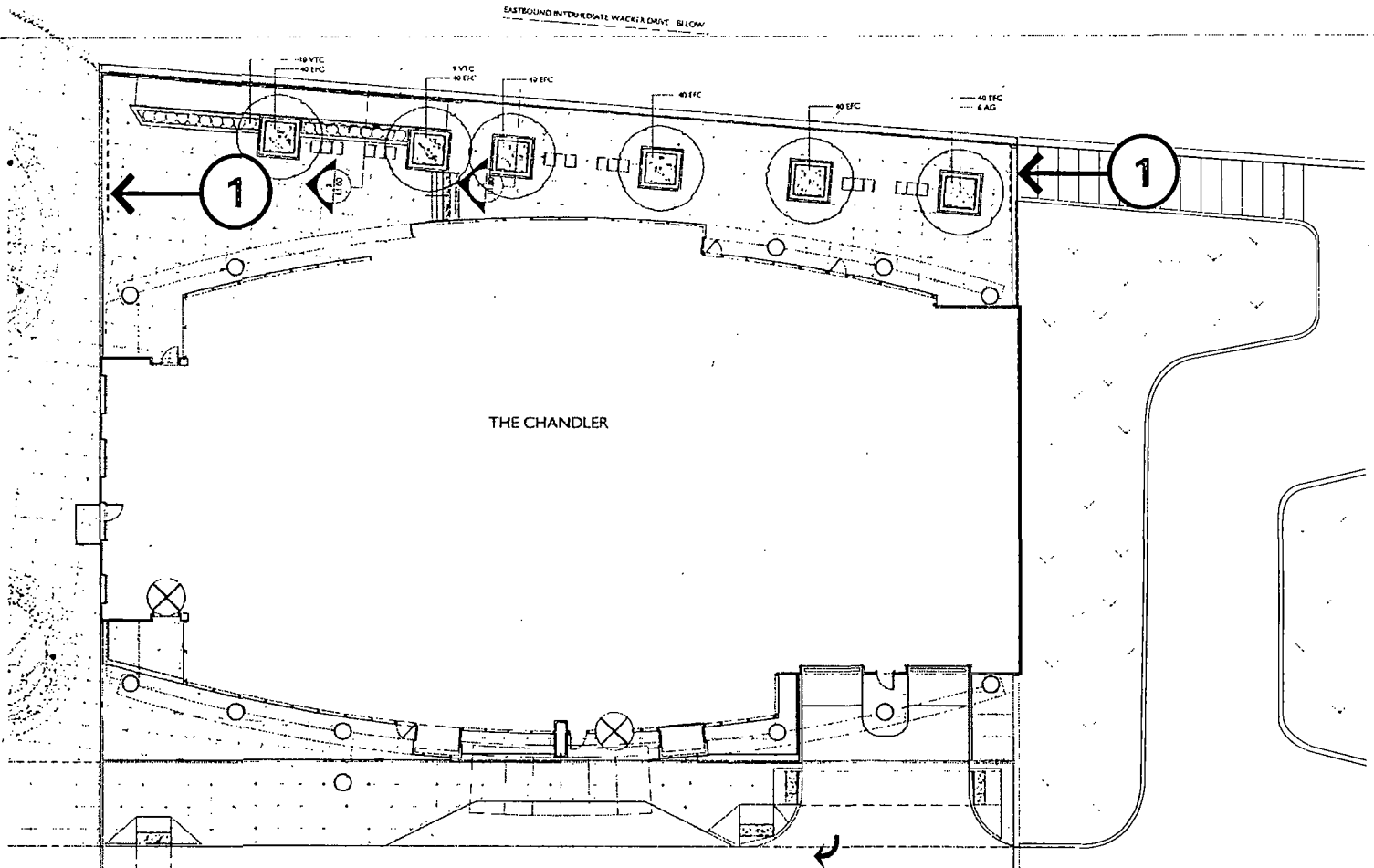
SITE PLAN

SCALE: 1" = 100'-0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
Introduced • September 13, 2018
Plan Commission • October 18, 2018



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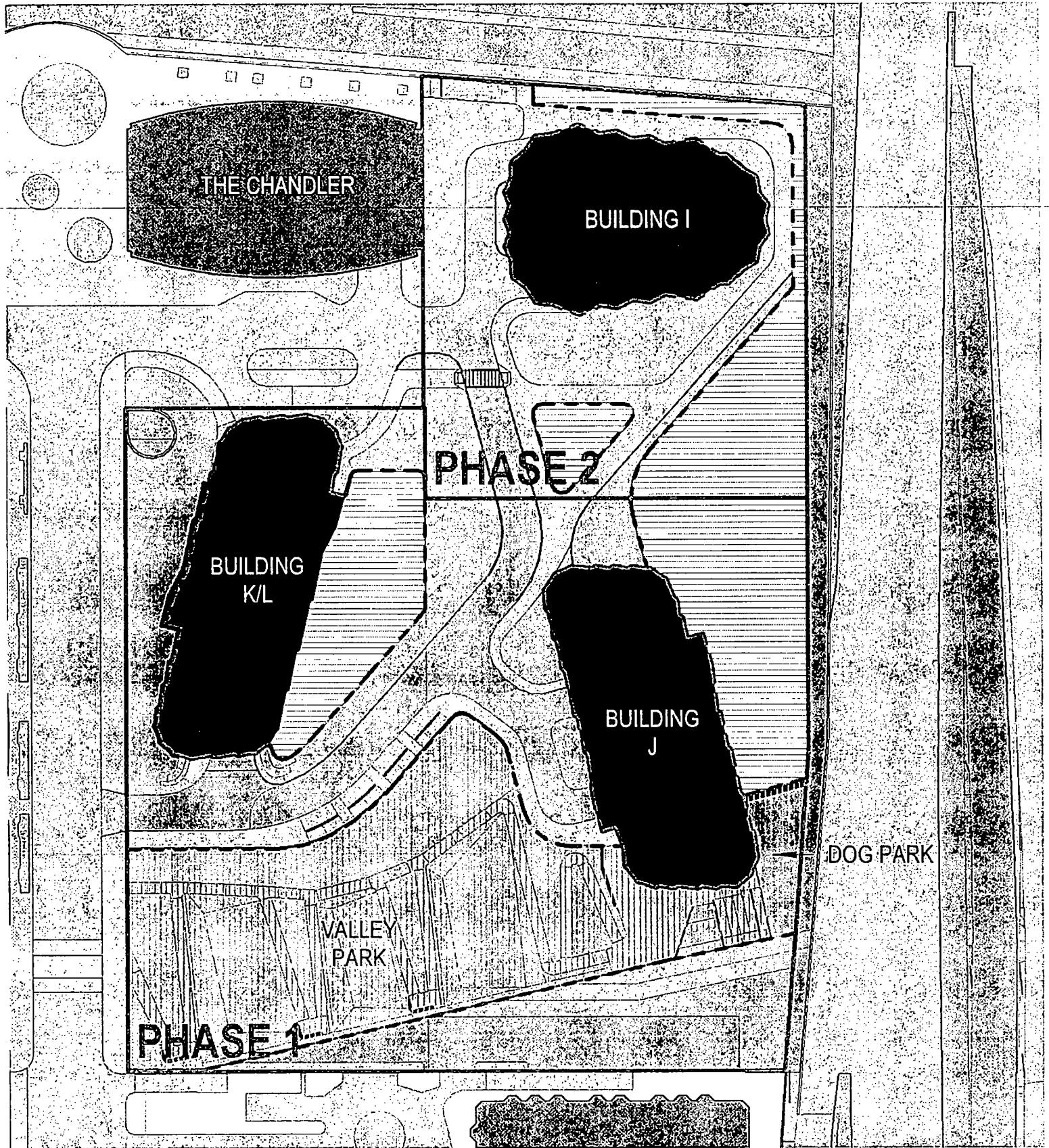


- 1** PROPOSED LOCATION FOR FENCE AND GATE AT THE CHANDLER, TO BE OPEN DURING PARK DISTRICT HOURS

ENLARGED SITE PLAN - CHANDLER GATES
SCALE: NTS

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
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Plan Commission • October 18, 2018



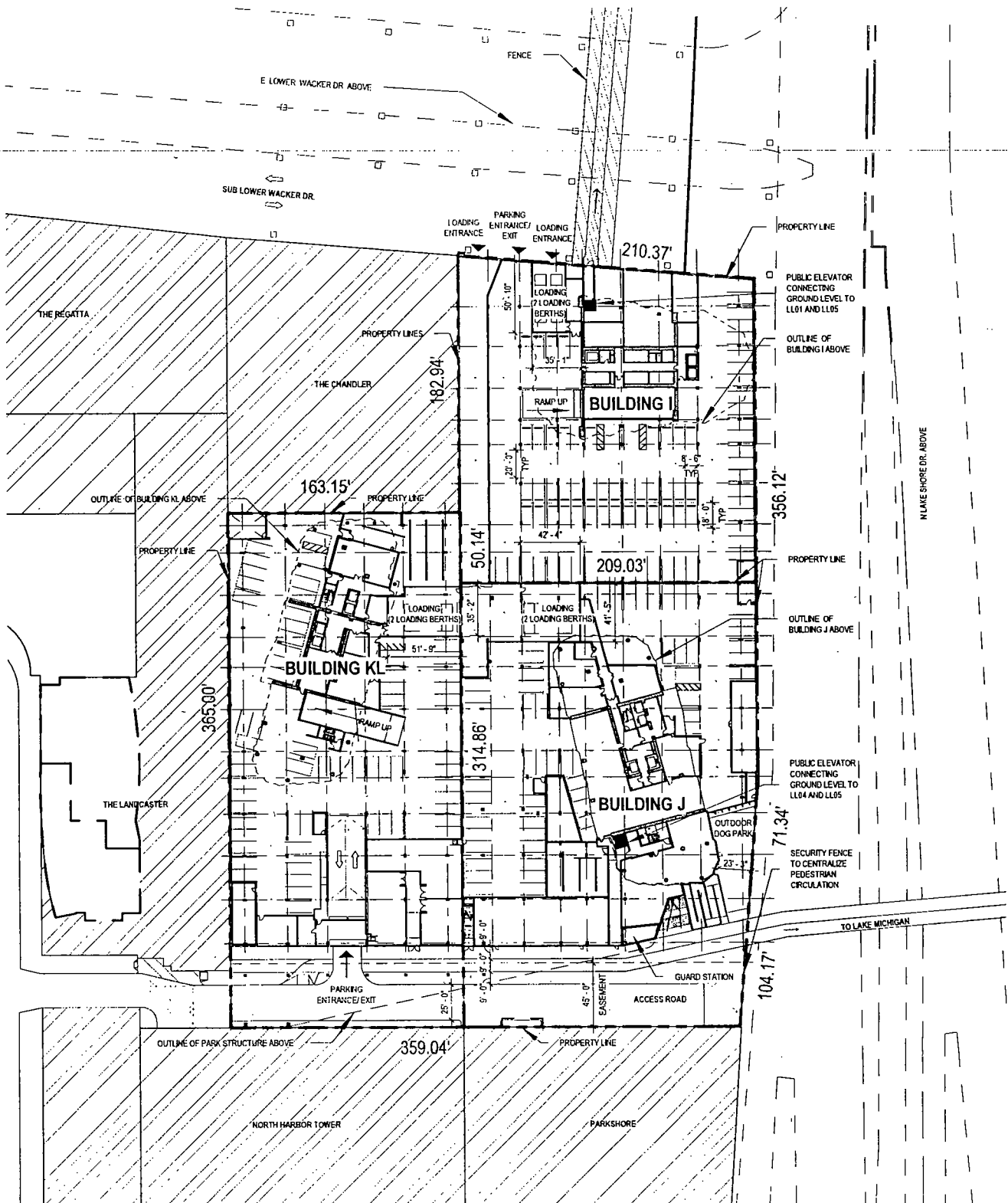


SITE PLAN - PHASING
SCALE: NTS

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
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Plan Commission • October 18, 2018



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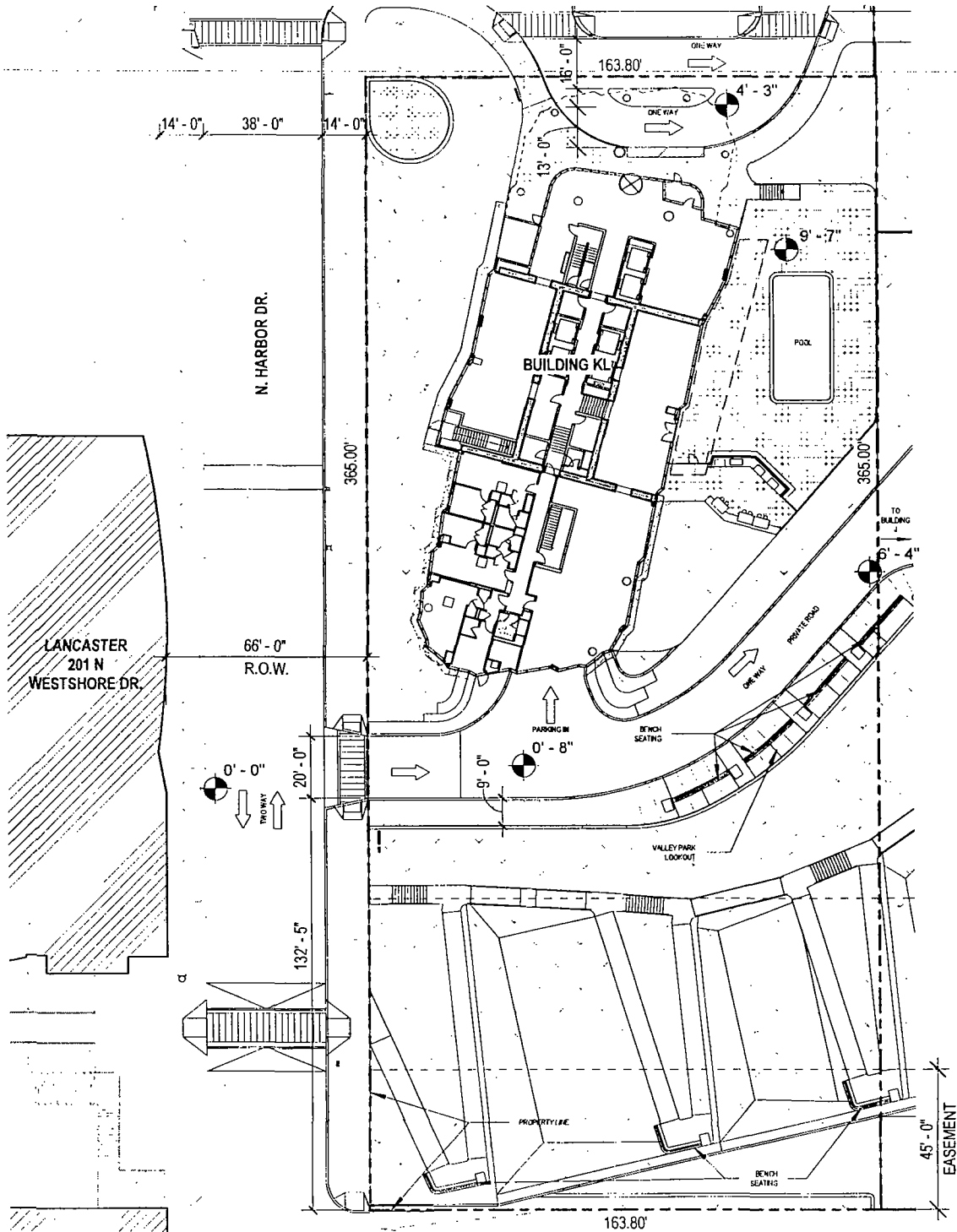
LOWER LEVEL 05

SCALE: 1" = 100'-0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
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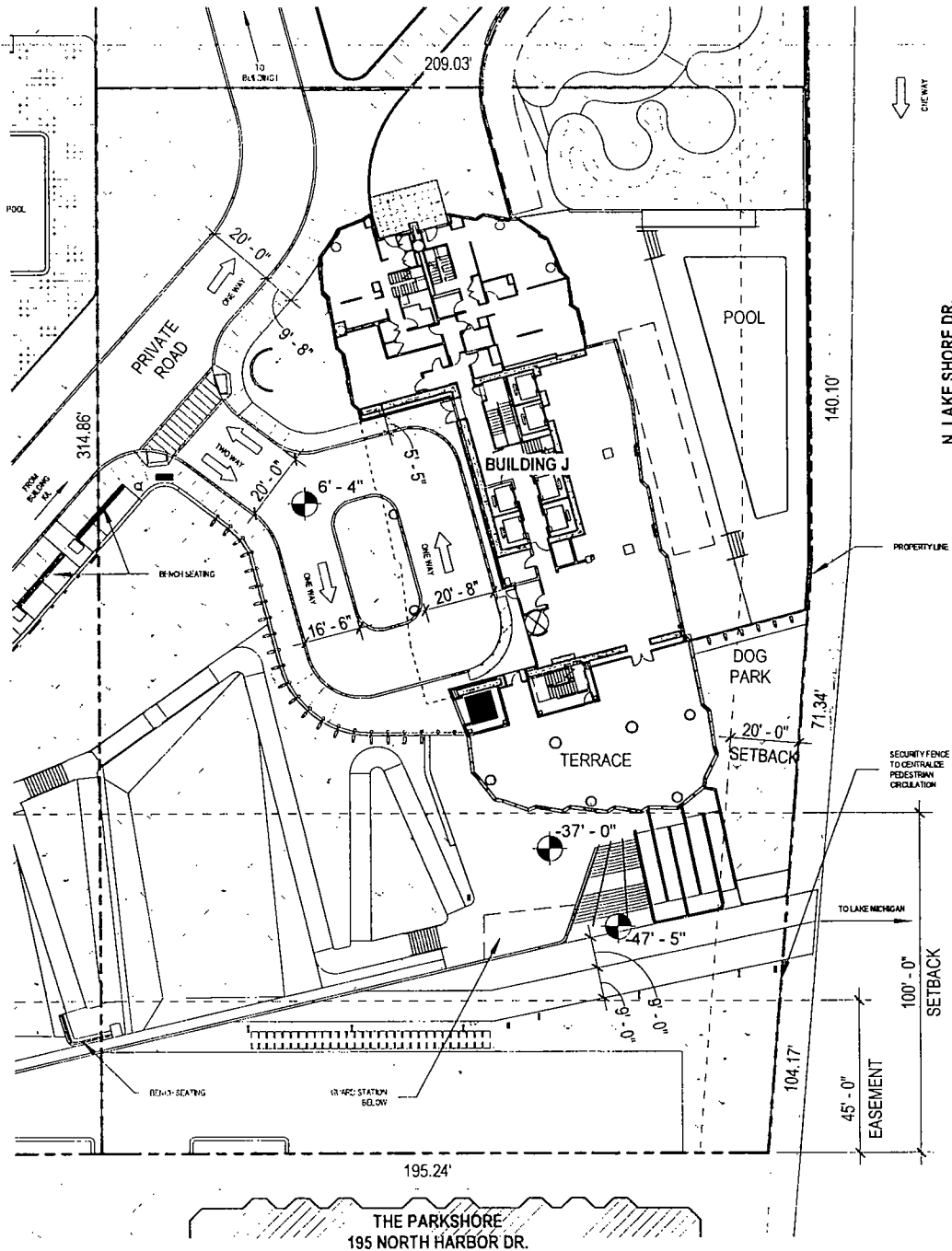


BUILDING K/L SITE PLAN
SCALE: 1" = 50'-0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
Introduced • September 13, 2018
Plan Commission • October 18, 2018



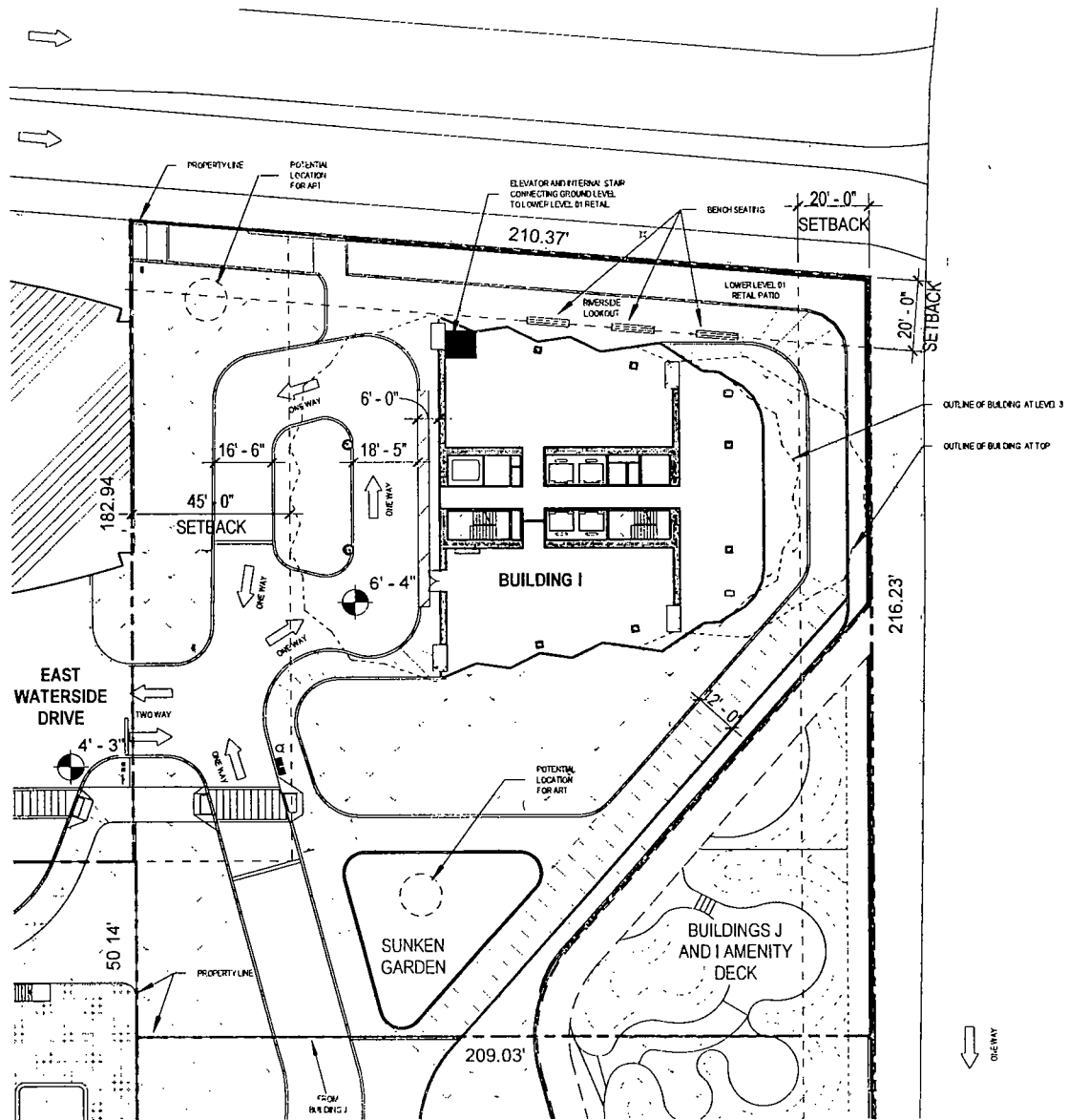
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BUILDING J SITE PLAN
SCALE: 1" = 50'-0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
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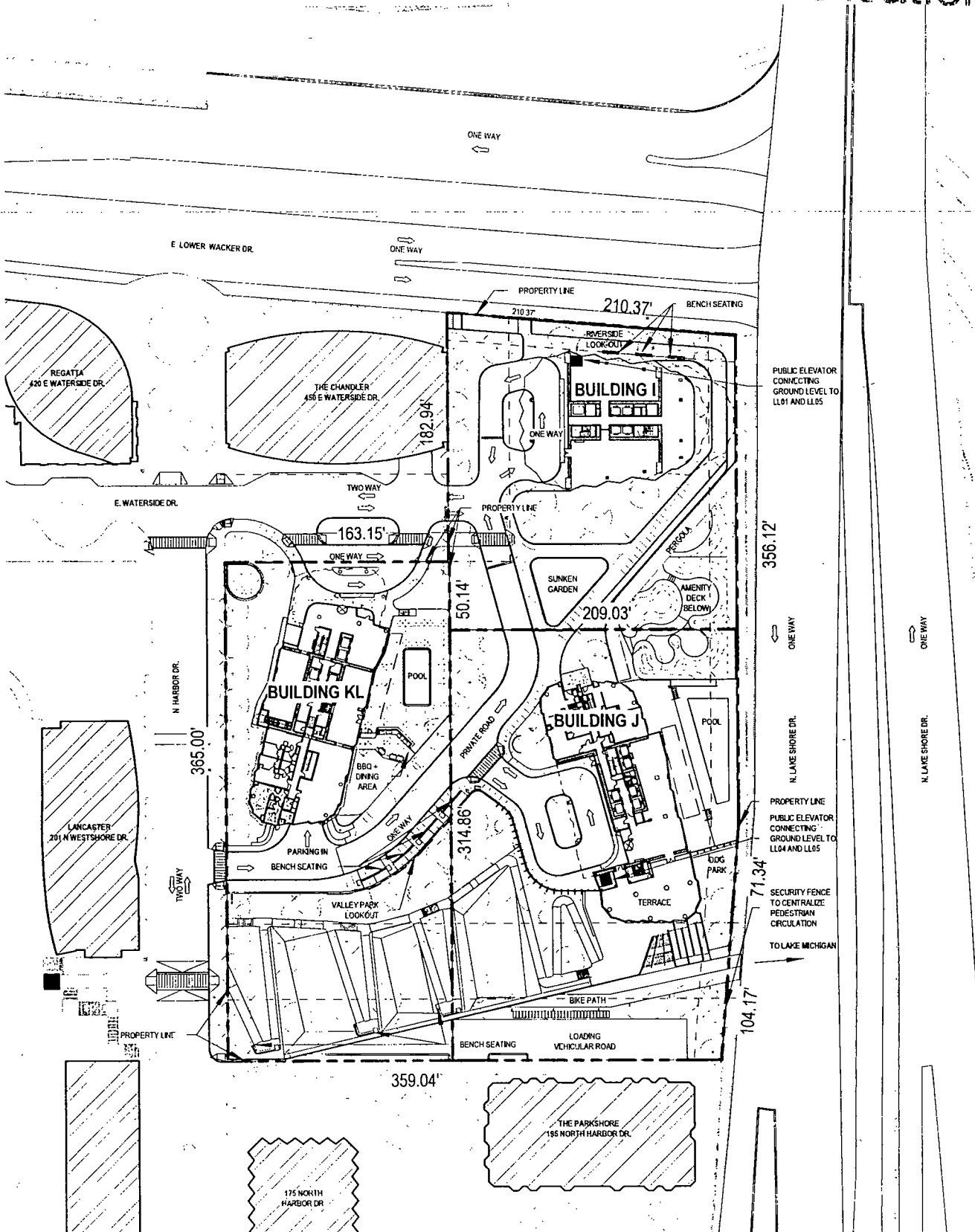




BUILDING I SITE PLAN
SCALE: 1" = 50'-0"



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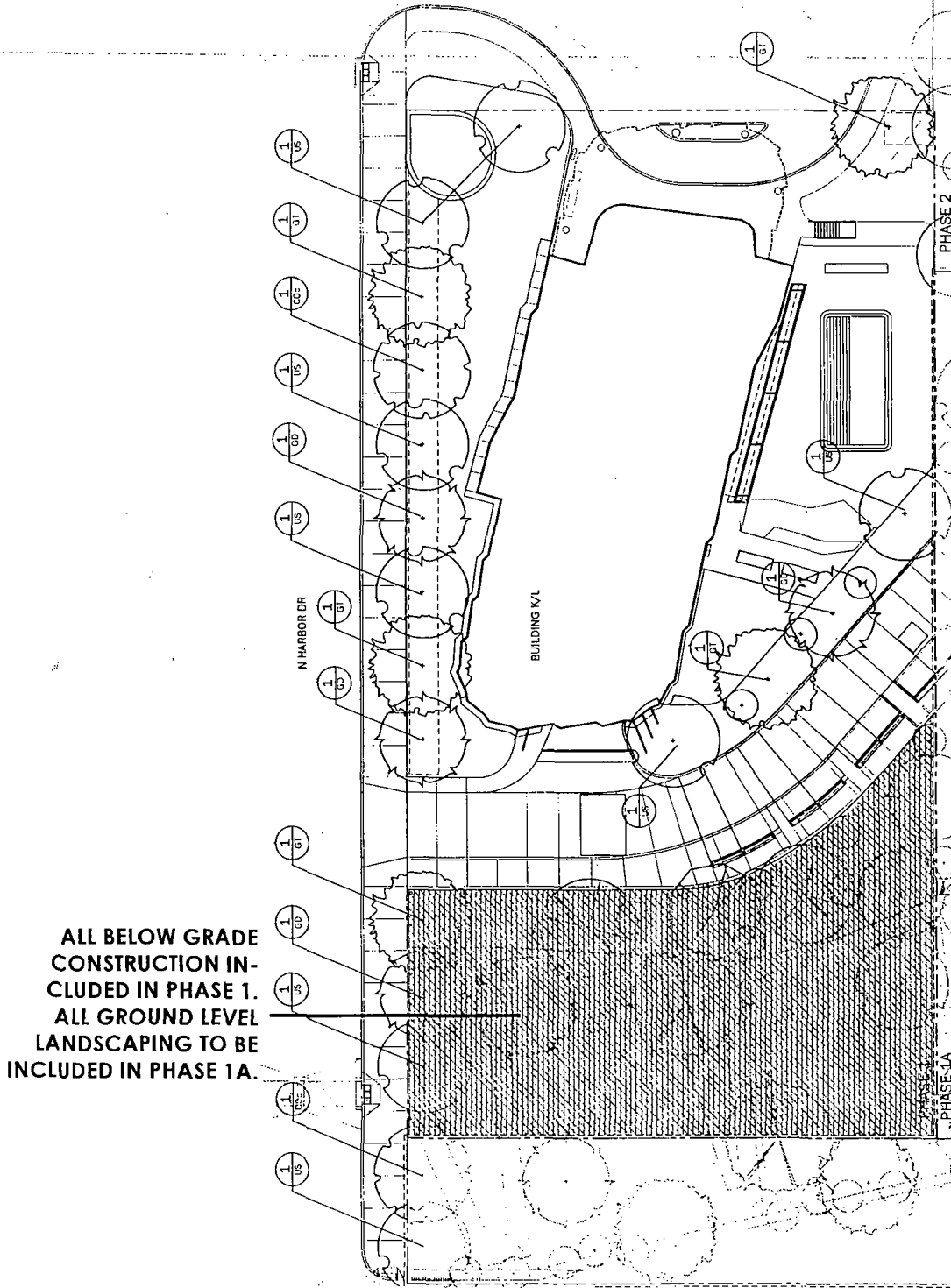


LANDSCAPE PLAN
SCALE: 1" = 100'-0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
Introduced • September 13, 2018
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E WATERSIDE DR



BUILDING K/L TREE PLAN

SCALE: 1" = 50'-0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
 Applicant • IJKL, LLC and Lakeshore East LLC
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 Plan Commission • October 18, 2018



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Planting Schedule

SYMBOL	Trees Canopy					
	QTY	LATIN NAME	COMMON NAME	* DBH Ø	SPACING	*N/NC/NN
	1	<i>Celtis occidentalis</i> 'Prairie Pride'	HACKBERRY	3"	SEE DWG	NC
	1	<i>Carya ovata</i>	SHAGBARK HICKORY	3"	SEE DWG	N
	4	<i>Gymnocladus</i> <i>dioica</i> 'Espresso' or 'Prairie Titan'	KENTUCKY COFFEETREE	3"	SEE DWG	NC
	4	<i>Gleditsia</i> <i>triacanthos</i> 'Shade Master' or 'Moraine'	HONEY LOCUST	3"	SEE DWG	NC
	6	<i>Ulmus</i> x spp 'Acolade' or 'Patriot'	ELM	3"	SEE DWG	NC

*N = Native Species
NC = Native Cultivar
NN = Non-Native Species

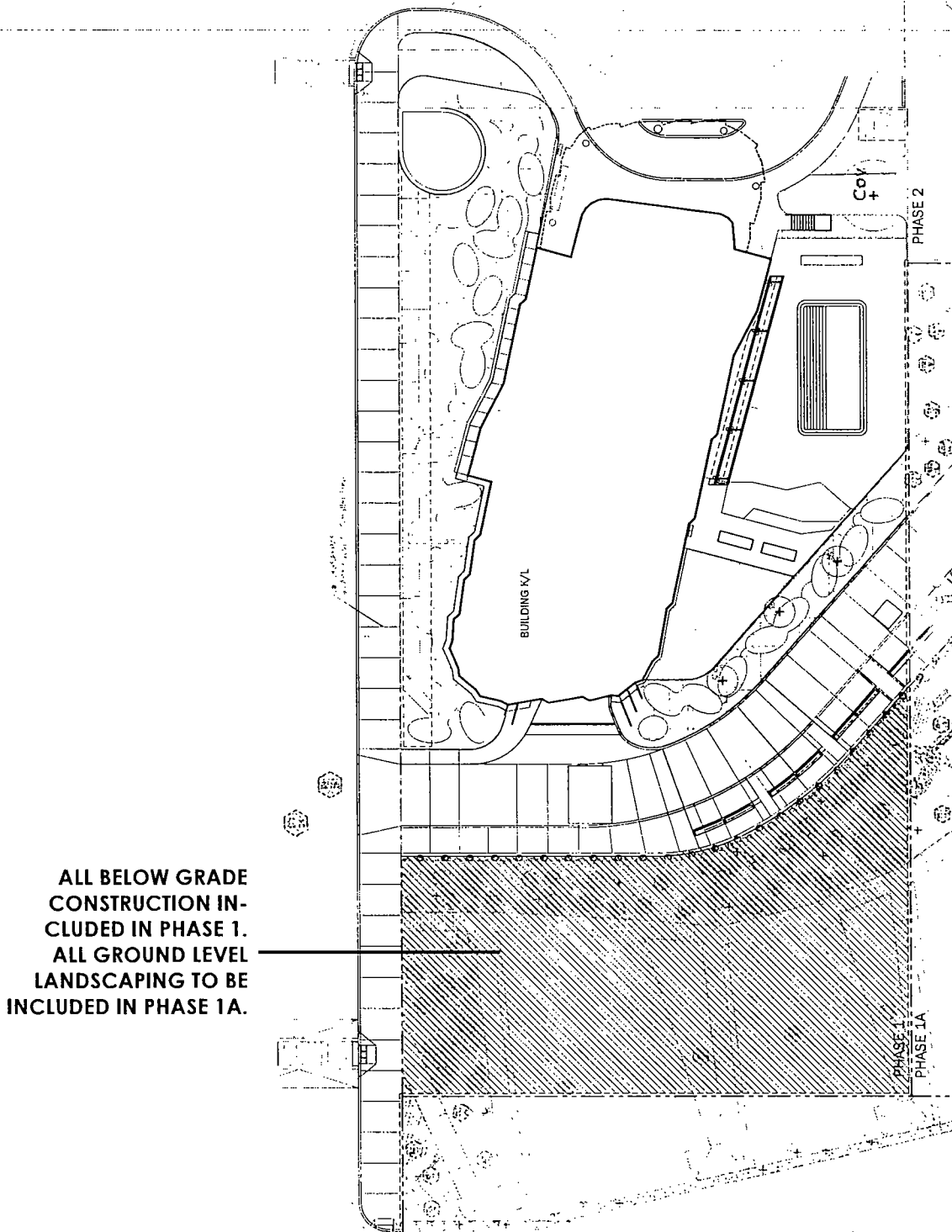
**DBH Ø = Diameter
at Breast Height (4')

SYMBOL	Trees Understory					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	1	<i>Cercis canadensis</i>	EASTERN REDBUD	70" WB	SEE DWG	N
	1	<i>Hamamelis</i> <i>virginiana</i>	WITCH HAZEL	70" WB	SEE DWG	N
	1	<i>Sassafras albidum</i>	SASSAFRAS	70" WB	SEE DWG	N

BUILDING K/L TREE SCHEDULE

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
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BUILDING K/L LANDSCAPE PLAN

SCALE: 1" = 50'-0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive

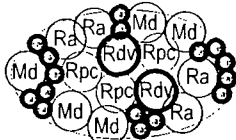
Applicant • IJKL, LLC and Lakeshore East LLC

Introduced • September 13, 2018

Plan Commission • October 18, 2018



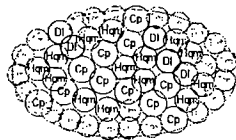
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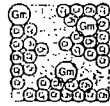
ZONE A (8' x 14' OVAL)



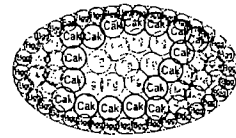
ZONE D (6' x 6' MODULE)



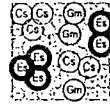
ZONE B (8' x 14' OVAL)



ZONE E (6' x 6' MODULE)



ZONE C (8' x 14' OVAL)



ZONE F (6' x 6' MODULE)

SYMBOL	Shrub And Groundcover Zones		
	ZONE	TOTAL AREA	DESCRIPTION
	A	1,367 SF	SHRUB COVER MODULE
	B	1,182 SF	SHRUB COVER MODULE
	C	1,246 SF	SHRUB COVER MODULE
	D	2,273 SF	GROUNDCOVER MODULE
	E	2,451 SF	GROUNDCOVER MODULE
	F	361 SF	GROUNDCOVER MODULE
	G	1,765 SF	SOD

SYMBOL	Climbing Plants					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	15	Campsis radicans	TRUMPET VINE	#1 (1 GAL)	112"	N

*N = Native Species
NC = Native Cultivar
NN = Non-Native Species

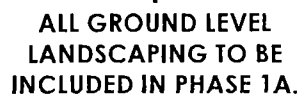
**DBH Ø = Diameter at Breast Height (4")

Planting Schedule

SYMBOL	Shrubs					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	510	Buxus 'Green Gem'	GREEN GEM BOXWOOD	12" FP	±12"	NN
	240	Cornus alba 'Kelley'	KELSEY'S DWARF RED OSIER DOGWOOD	#5 (5 GAL)	±30"	NC
	150	Comptonia peregrina	SWEET FERN	#5 (5 GAL)	±30"	N
	65	Diervilla lonicera	DWARF HONEYSUCKLE	#3 (3 GAL)	±36"	N
	125	Fothergilla gardenii	WITCH ALDER	#5 (5 GAL)	±36"	NC
	190	Hydrangea quercifolia 'Munchkin'	MUNCHKIN OAKLEAF HYDRANGEA	#5 (5 GAL)	±42"	NN
	265	Kalmia angustifolia-rubra	SHEEP LAUREL	#2 (2 GAL)	±30"	N
	210	Mahonia aquifolium 'Compacta'	COMPACT OREGON GRAPE	#2 (2 GAL)	±36"	NC
	75	Microbiota decussata 'Celtic Pride'	CELTIC PRIDE CYPRESS	#3 (3 GAL)	±48"	NN
	60	Rhus aromatica 'Gro Low'	GRO-LOW FRAGRANT SUMAC	#2 (2 GAL)	±48"	N
	31	Rhododendron 'Delaware Valley White'	EVERGREEN AZALEA	24" FP	±42"	NN
	46	Rhododendron 'Poukhanense Compacta'	KOREAN AZALEA	24" FP	±48"	NN
	310	Taxus meadia 'Evelrow'	EVLRLLOW YEW	24" FP	±48"	NN

SYMBOL	Groundcover Herbaceous					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	2,275	Asarum canadense	WILD GINGER	6" FP	±12"	N
	760	Epimedium x rubrum	RED BARRENROOT	#1 (1 GAL)	±12"	NN
	235	Geranium maculatum	WILD GERANIUM	#2 (2 GAL)	±18"	NN
	125	Lamium galeobdolon	YELLOW ARCHANGEL	#2 (2 GAL)	±24"	NN
	190	Polystichum acrostichoides	CHRISTMAS FERN	#2 (2 GAL)	±18"	N
	340	Polymonium reptans	JACOB'S LADDER	6" FP	±12"	N
	1,430	Sedum ternatum	THREE-LEAVED STONECROP	#1 (1 GAL)	18"	N

SYMBOL	Groundcover Grasses/sedges					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	1,565	Carex hirtifolia	HAIRY WOOD SEDGE	#1 (1 GAL)	±12"	N
	1,565	Carex jamesii	GRASS SEDGE	#1 (1 GAL)	±12"	N
	815	Carex rosea	CURLY-STYLED WOOD SEDGE	#1 (1 GAL)	±12"	N
	50	Carex sprengei	LONG-BEAKED SEDGE	#1 (1 GAL)	±18"	N
	50	Eragrostis spectabilis	PURPLE LOVE GRASS	#1 (1 GAL)	±18"	N



SCALE: 1" = 50'-0"

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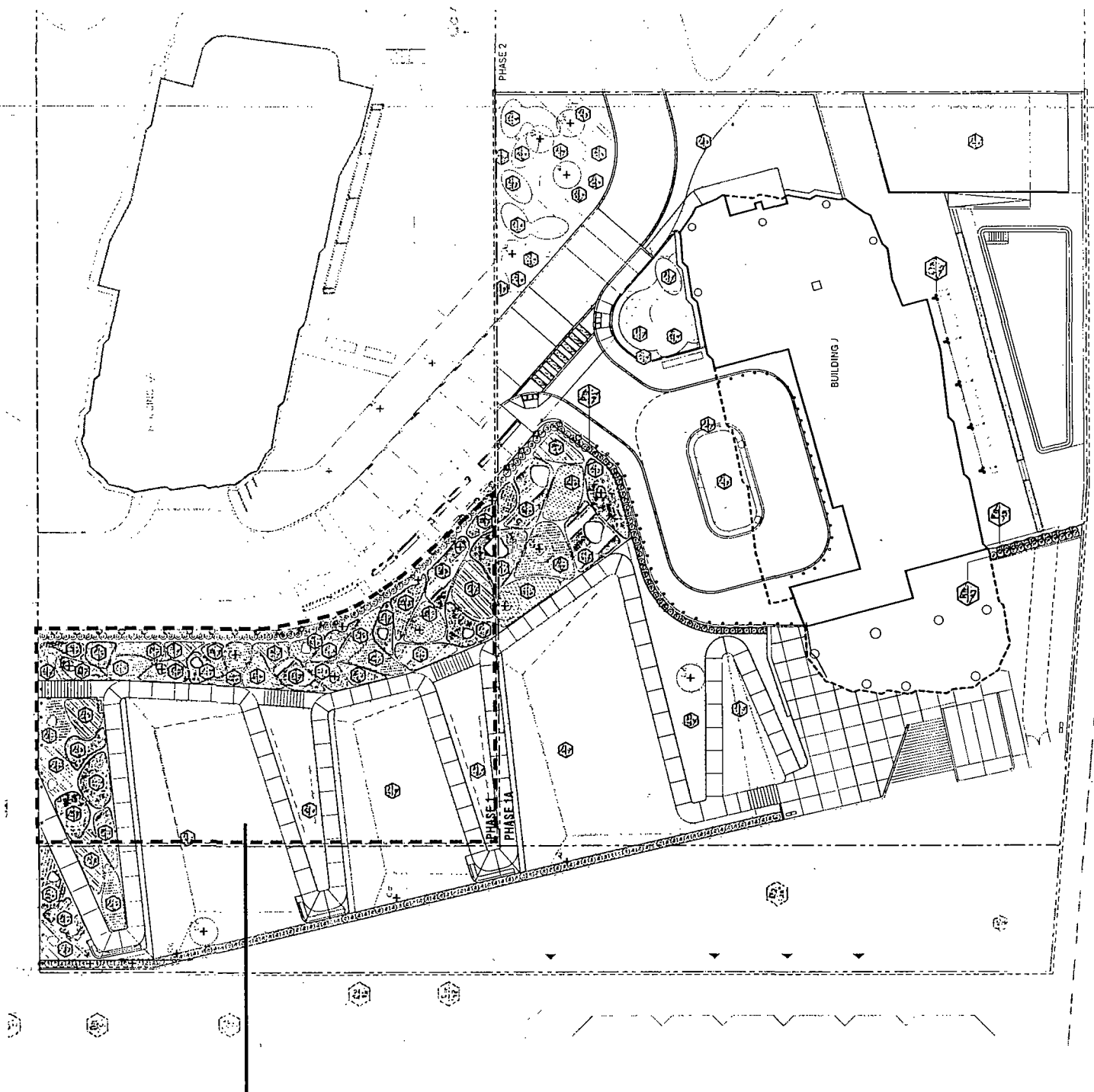
SYMBOL	Trees Canopy					
	QTY	LATIN NAME	COMMON NAME	**DBH Ø	SPACING	*N/NC/NN
	3	Cellis occidentalis 'Prairie Pride'	HACKBERRY	3"	SEE DWG	NC
	8	Gymnocladus dioica 'Espresso' or 'Prairie Titan'	KENTUCKY COFFEETREE	3"	SEE DWG	NC
	7	Gleditsia triacanthos 'Shade Master' or 'Moraine'	HONEY LOCUST	3"	SEE DWG	NC
	2	Pinus banksiana	JACK PINE	3"	SEE DWG	N
	5	Populus deltoides 'Souixland'	EASTERN COTTONWOOD	3"	SEE DWG	NC
	1	Quercus macrocarpa	BUR OAK	3"	SEE DWG	N
	2	Quercus muehlenbergii	CHINKAPIN OAK	3"	SEE DWG	N
	2	Styphnolobium japonicum	JAPANESE PAGODATREE	3"	SEE DWG	NN
	4	Ulmus x spp 'Acolade' or 'Patriot'	ELM	3"	SEE DWG	NC

SYMBOL	Trees Understory					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	9	Amelanchier laevis (multistem)	ALLEGHENY SERVICEBERRY	70" WB	SEE DWG	N
	2	Crataegus crus-galli (multistem)	COCKSPUR HAWTHORN	70" WB	SEE DWG	N
	12	Cercis canadensis (multistem)	EASTERN REDBUD	70" WB	SEE DWG	N
	2	Hamamelis virginiana (multistem)	WITCH HAZEL	70" WB	SEE DWG	N
	4	Sassafras albidum (multistem)	SASSAFRAS	70" WB	SEE DWG	N

*N = Native Species
NC = Native Cultivar
NN = Non-Native Species

**DBH Ø = Diameter
at Breast Height (4')

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ALL GROUND LEVEL
LANDSCAPING TO BE
INCLUDED IN PHASE 1A.

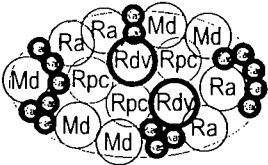
BUILDING J LANDSCAPE PLAN

SCALE: 1" = 50'-0"

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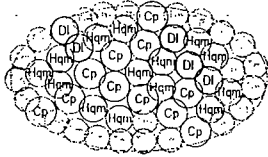
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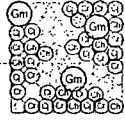
ZONE A (8' x 14' OVAL)



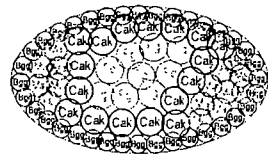
ZONE D (6' x 6' MODULE)



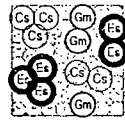
ZONE B (8' x 14' OVAL)



ZONE E (6' x 6' MODULE)



ZONE C (8' x 14' OVAL)



ZONE F (6' x 6' MODULE)

SYMBOL	Shrubs					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	847	Buxus 'Green Gem'	GREEN GEM BOXWOOD	12" FP	±12"	NN
	85	Cornus alba 'Kelsey'	KELSEY'S DWARF RED OSIER DOGWOOD	#5 (5 GAL)	±30"	NC
	84	Carpinus betulus	HORNBEAM (hedge)	#5 (3 GAL)	±32"	N
	311	Comptonia peregrina	SWEET FERN	#5 (5 GAL)	±30"	N
	149	Cornus sericea 'Isen'	RED OSIER DOGWOOD	24" FP	±32"	NC
	104	Dirca palustris	LEATHERWOOD (hedge)	#5 (5 GAL)	±30"	N
	242	Diervilla lonicera	DWARF HONEYSUCKLE	#3 (3 GAL)	±36"	N
	45	Fothergilla gardenii	WITCH ALDER	#5 (5 GAL)	±36"	NC
	66	Hydrangea quercifolia 'Munchkin'	MUNCHKIN OAKLEAF HYDRANGEA	#5 (5 GAL)	±42"	NN
	204	Kalmia angustifolia-rubra	SHEEP LAUREL	#2 (2 GAL)	±30"	N
	340	Mahonia aquifolium 'Compacta'	COMPACT OREGON GRAPE	#2 (2 GAL)	±36"	NC
	247	Microbiota decussata 'Celtic Pride'	CELTIC PRIDE CYPRESS	#3 (3 GAL)	±48"	NN
	138	Rhus aromatica 'Gor Low'	GRO-LOW FRAGRANT SUMAC	#2 (2 GAL)	±48"	N
	24	Rhododendron 'Delaware Valley White'	EVERGREEN AZALEA	24" FP	±42"	NN
	36	Rhododendron 'Poukhanense Compacta'	KOREAN AZALEA	24" FP	±48"	NN
	603	Taxus canadensis 'Everlow'	EVERLOW YEW	24" FP	±48"	NN

SYMBOL	Groundcover Herbaceous					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	714	Asarum canadense	WILD GINGER	6" FP	±12"	N
	238	Epimedium x rubrum	RED BARRENROOT	#1 (1 GAL)	±12"	NN
	112	Geranium maculatum	WILD GERANIUM	#2 (2 GAL)	±18"	NN
	40	Laminium galeobdolon	YELLOW ARCHWEE	#2 (2 GAL)	±24"	NN
	59	Polystichum acrostichoides	CHRISTMAS FERN	#2 (2 GAL)	±18"	N
	764	Polymonium reptans	JACOB'S LADDER	6" FP	±12"	N
	318	Sedum ternatum	THREE-LEAVED STONECROP	#1 (1 GAL)	±8"	N

SYMBOL	Groundcover Grasses/sedges					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	349	Carex hirtifolia	HAIRY WOOD SEDGE	#1 (1 GAL)	±12"	N
	349	Carex jamesii	GRASS SEDGE	#1 (1 GAL)	±12"	N
	182	Carex rosea	CURLY-STYLED WOOD SEDGE	#1 (1 GAL)	±12"	N
	152	Carex sprengelii	LONG-BEAKED SEDGE	#1 (1 GAL)	±18"	N
	112	Eragrostis spectabilis	PURPLE LOVE GRASS	#1 (1 GAL)	±18"	N

SYMBOL	Shrub And Groundcover Zones		
	ZONE	TOTAL AREA	DESCRIPTION
	A	1,057 SF	SHRUB COVER MODULE
	B	412 SF	SHRUB COVER MODULE
	C	441 SF	SHRUB COVER MODULE
	D	714 SF	GROUNDCOVER MODULE
	E	546 SF	GROUNDCOVER MODULE
	F	809 SF	GROUNDCOVER MODULE
	G	23,482 SF	SOD

SYMBOL	Climbing Plants					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	15	Campsis radicans	TRUMPET VINE	#1 (1 GAL)	±12"	N
	53	Parthenocissus quinquefolia	VIRGINIA CREEPER	#1 (1 GAL)	±12"	N
	28	Parthenocissus tricuspidata	BOSTON IVY	#1 (1 GAL)	±8'	NN

*N = NATIVE SPECIES
NC = NATIVE CULTIVAR
NN = NON-NATIVE SPECIES

**DBH Ø = DIAMETER AT BREAST HEIGHT (4')

BUILDING J LANDSCAPE SCHEDULES

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SCALE: 1" = 50'-0"

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Planting Schedule

SYMBOL	Trees Canopy					
	QTY	LATIN NAME	COMMON NAME	**DBH Ø	SPACING	*N/NC/NN
	1	<i>Celtis occidentalis</i> 'Prairie Pride'	HACKBERRY	3"	SEE DWG	NC
	1	<i>Carya ovata</i>	SHAGBARK HICKORY	3"	SEE DWG	N
	4	<i>Gymnocladus dioica</i> 'Espresso' or 'Prairie Titan'	KENTUCKY COFFEETREE	3"	SEE DWG	NC
	4	<i>Gleditsia triacanthos</i> 'Shade Master' or 'Moraine'	HONEY LOCUST	3"	SEE DWG	NC
	6	<i>Ulmus x spp</i> 'Acolade' or 'Patriot'	ELM	3"	SEE DWG	NC

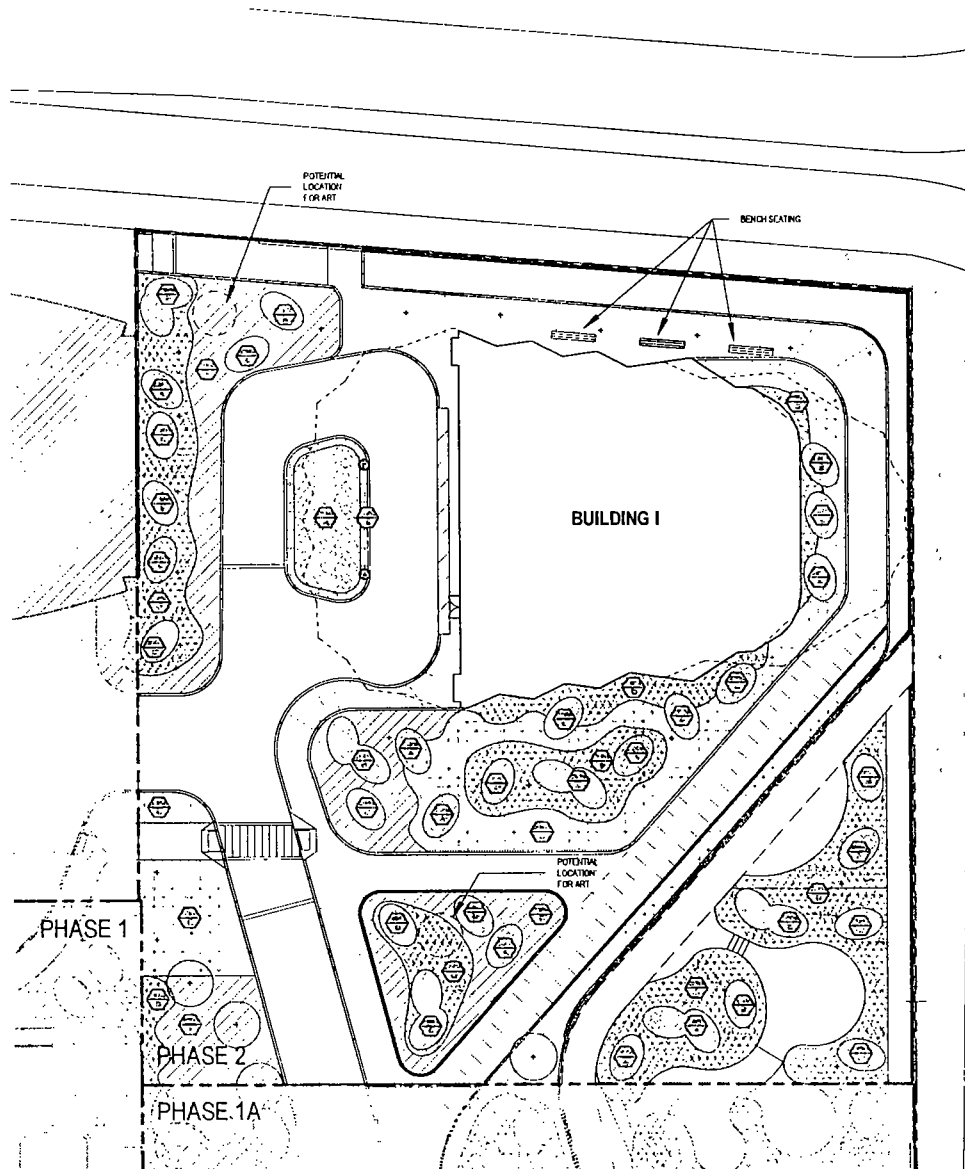
*N = Native Species
 NC = Native Cultivar
 NN = Non-Native Species

**DBH Ø = Diameter
 at Breast Height (4')

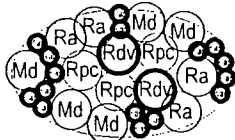
SYMBOL	Trees Understory					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	1	<i>Cercis canadensis</i>	EASTERN REDBUD	70" WB	SEE DWG	N
	1	<i>Hamamelis virginiana</i>	WITCH HAZEL	70" WB	SEE DWG	N
	1	<i>Sassafras albidum</i>	SASSAFRAS	70" WB	SEE DWG	N

BUILDING I TREE SCHEDULE

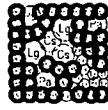
Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive •
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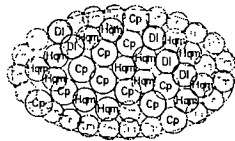
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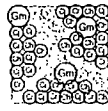
ZONE A (8' x 14' OVAL)



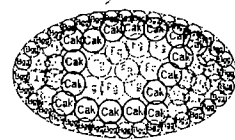
ZONE D (6' x 6' MODULE)



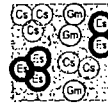
ZONE B (8' x 14' OVAL)



ZONE E (6' x 6' MODULE)



ZONE C (8' x 14' OVAL)



ZONE F (6' x 6' MODULE)

SYMBOL	Shrub And Groundcover Zones		
	ZONE	TOTAL AREA	DESCRIPTION
	A	1,367 SF	SHRUB COVER MODULE
	B	1,182 SF	SHRUB COVER MODULE
	C	1,246 SF	SHRUB COVER MODULE
	D	2,273 SF	GROUNDCOVER MODULE
	E	2,451 SF	GROUNDCOVER MODULE
	F	361 SF	GROUNDCOVER MODULE
	G	1,765 SF	SOD

SYMBOL	Climbing Plants					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	15	Campsis radicans	TRUMPET VINE	#1 (1 GAL)	±12'	N

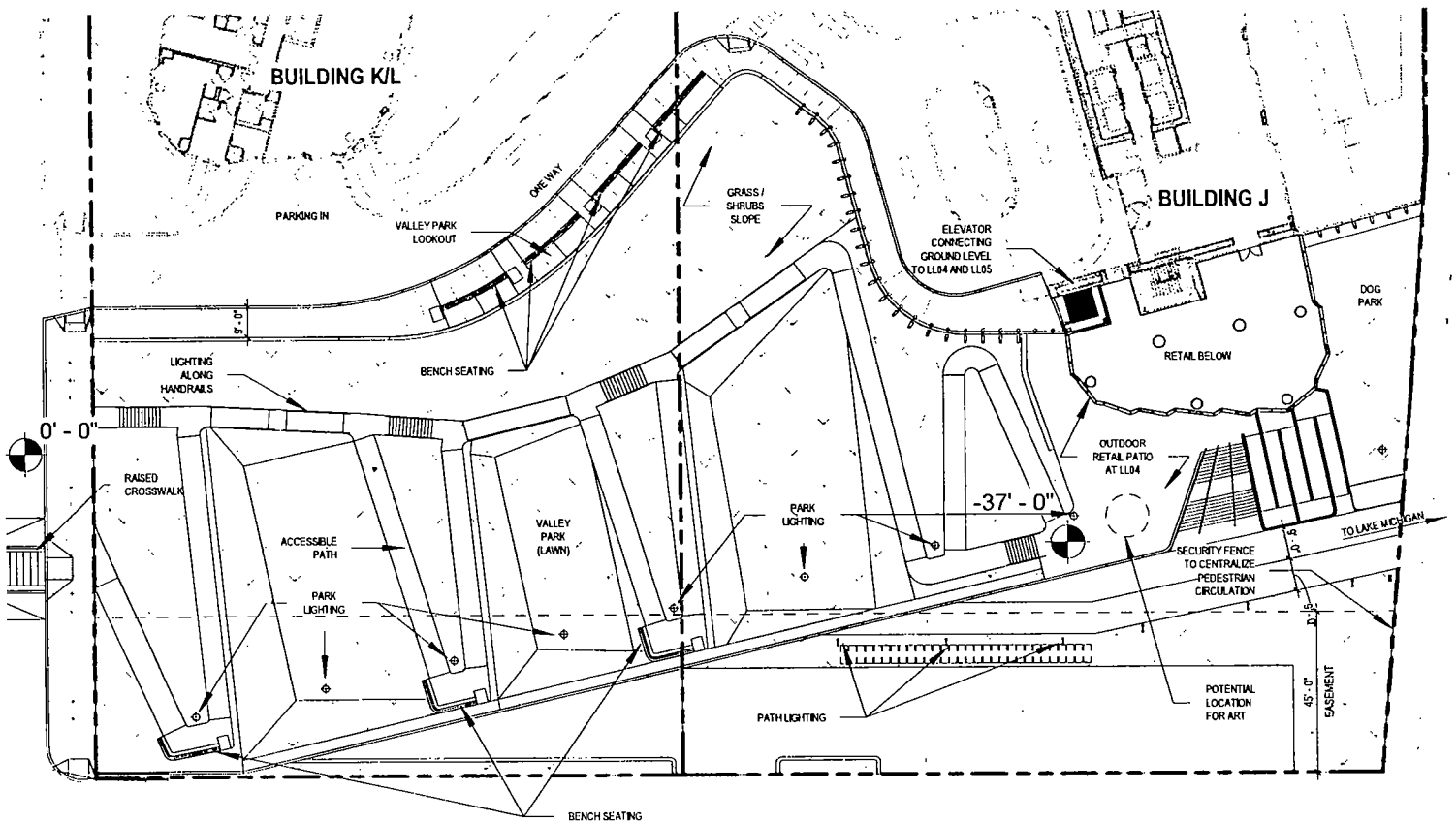
*N = Native Species
 NC = Native Cultivar
 NN = Non-Native Species
 **DBH Ø = Diameter at Breast Height (4')

Planting Schedule

SYMBOL	Shrubs					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	510	Buxus 'Green Gem'	GREEN GEM BOXWOOD	12" FP	±12"	NN
	240	Cornus alba 'Kelsey'	KELSEY'S DWARF RED OSIER DOGWOOD	#5 (5 GAL)	±30"	NC
	150	Comptonia peregrina	SWEET FERN	#5 (5 GAL)	±30"	N
	65	Diervilla lonicera	DWARF HONEYSUCKLE	#3 (3 GAL)	±36"	N
	125	Fothergilla gardenii	WITCH ALDER	#5 (5 GAL)	±36"	NC
	190	Hydrangea quercifolia 'Munchkin'	MUNCHKIN OAKLEAF HYDRANGEA	#5 (5 GAL)	±42"	NN
	265	Kalmia angustifolia-ubra	SHEEP LAUREL	#2 (2 GAL)	±30"	N
	210	Mahonia aquifolium 'Compacta'	COMPACT OREGON GRAPE	#2 (2 GAL)	±36"	NC
	75	Microbiota decussata 'Celtic Pride'	CELTIC PRIDE CYPRESS	#3 (3 GAL)	±48"	NN
	60	Rhus aromatica 'Eor Low'	GRO-LOW FRAGRANT SUMAC	#2 (2 GAL)	±48"	N
	31	Rhododendron 'Delaware Valley White'	EVERGREEN AZALEA	24" FP	±42"	NN
	45	Rhododendron 'Poukhanense Compacta'	KOREAN AZALEA	24" FP	±48"	NN
	310	Taxus media 'Everlow'	EVERLOW YEW	24" FP	±48"	NN

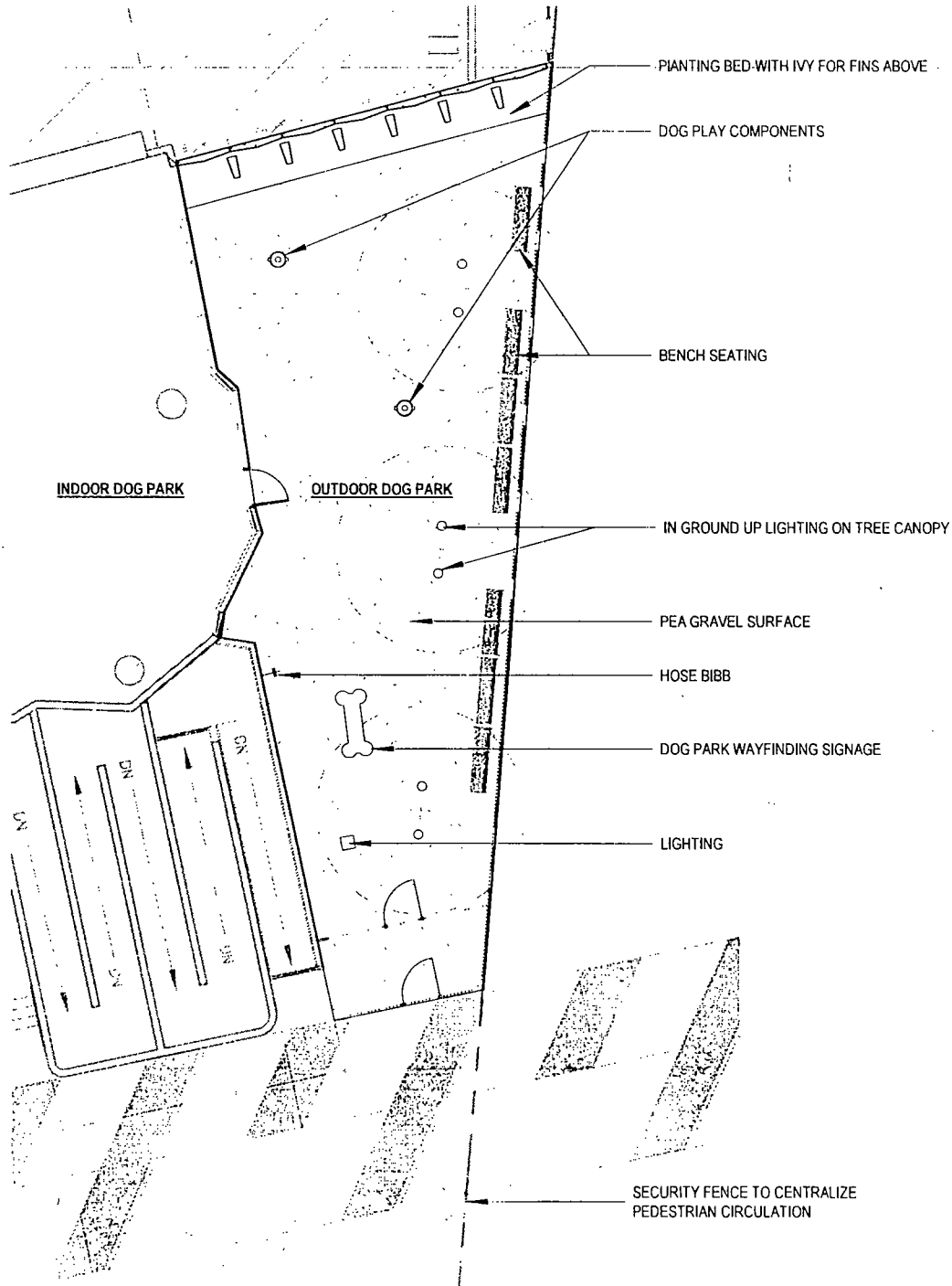
SYMBOL	Groundcover Herbaceous					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	2,275	Asarum canadense	WILD GINGER	6" FP	±12"	N
	760	Epimedium x rubrum	RED BARRENROOT	#1 (1 GAL)	±12"	NN
	235	Geranium maculatum	WILD GERANIUM	#2 (2 GAL)	±18"	NN
	125	Lamium galeobdolon	YELLOW ARCHANGEL	#2 (2 GAL)	±24"	NN
	190	Polystichum acrostichoides	CHRISTMAS FERN	#2 (2 GAL)	±18"	N
	340	Polygonum reptans	JACOB'S LADDER	6" FP	±12"	N
	1,430	Sedum ternatum	THREE-LEAVED STONECROP	#1 (1 GAL)	±18"	N

SYMBOL	Groundcover Grasses/sedges					
	QTY	LATIN NAME	COMMON NAME	SIZE	SPACING	*N/NC/NN
	1,565	Carex hirtifolia	HAIRY WOOD SEDGE	#1 (1 GAL)	±12"	N
	1,565	Carex jamesii	GRASS SEDGE	#1 (1 GAL)	±12"	N
	815	Carex rosea	CURLY-STYLED WOOD SEDGE	#1 (1 GAL)	±12"	N
	50	Carex sprengei	LONG-BEAKED SEDGE	#1 (1 GAL)	±18"	N
	50	Eragrostis spectabilis	PURPLE LOVE GRASS	#1 (1 GAL)	±18"	N



ENLARGED VALLEY PARK PLAN
SCALE: 1" = 50'-0"



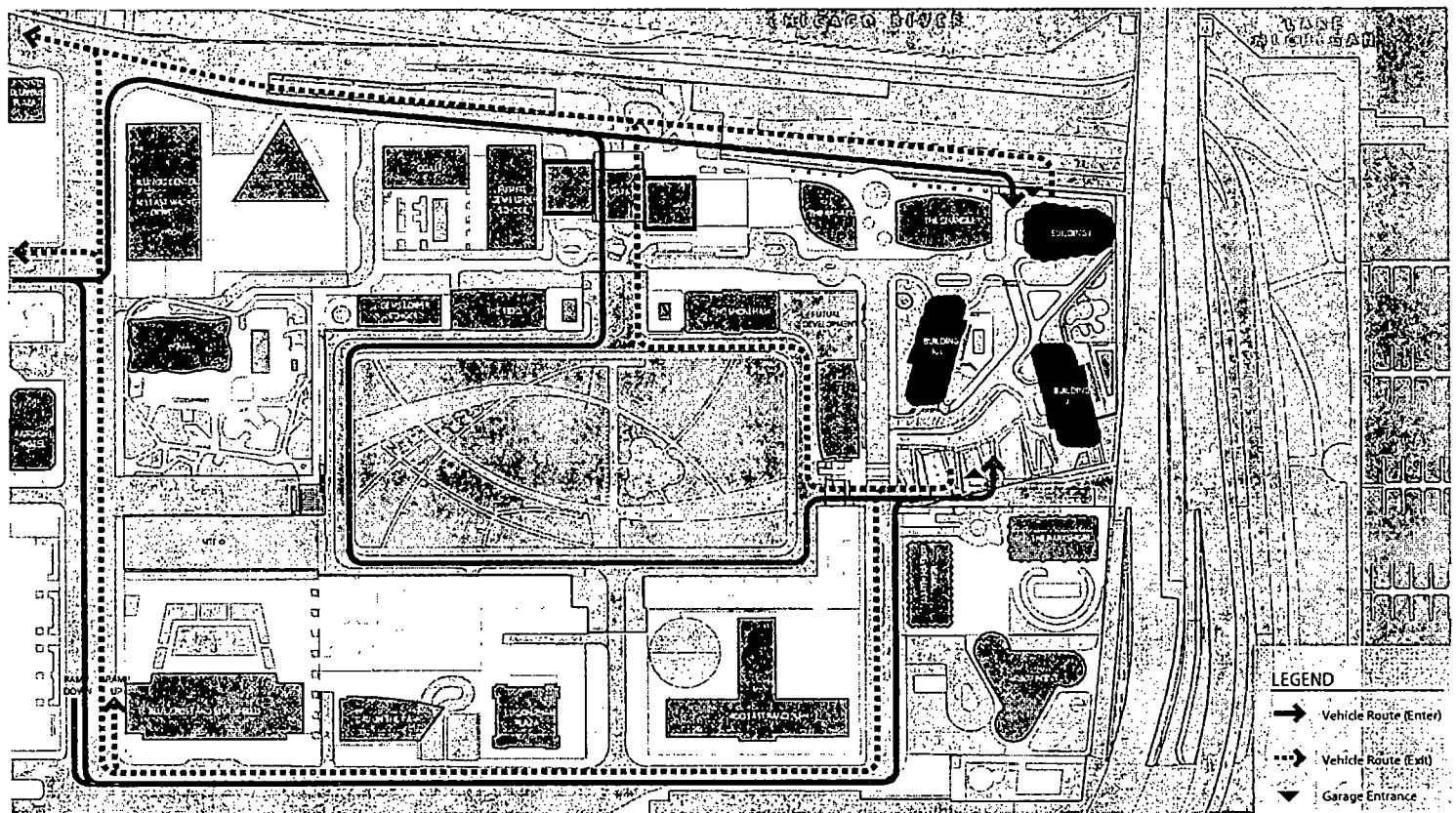


ENLARGED DOG PARK PLAN

SCALE: 1/16" = 1'-0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
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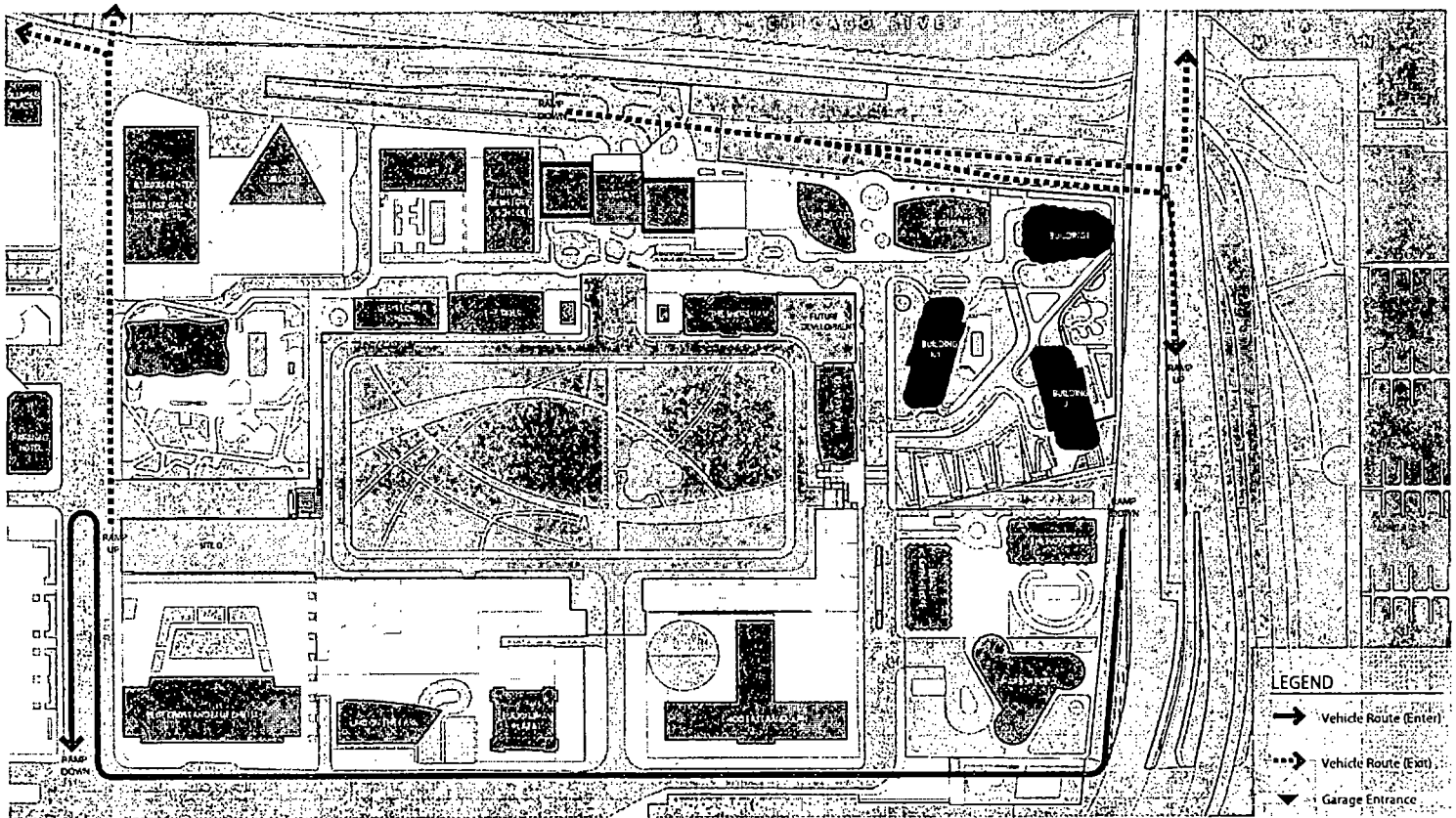




LOWER LEVEL VEHICULAR ACCESS PLAN

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
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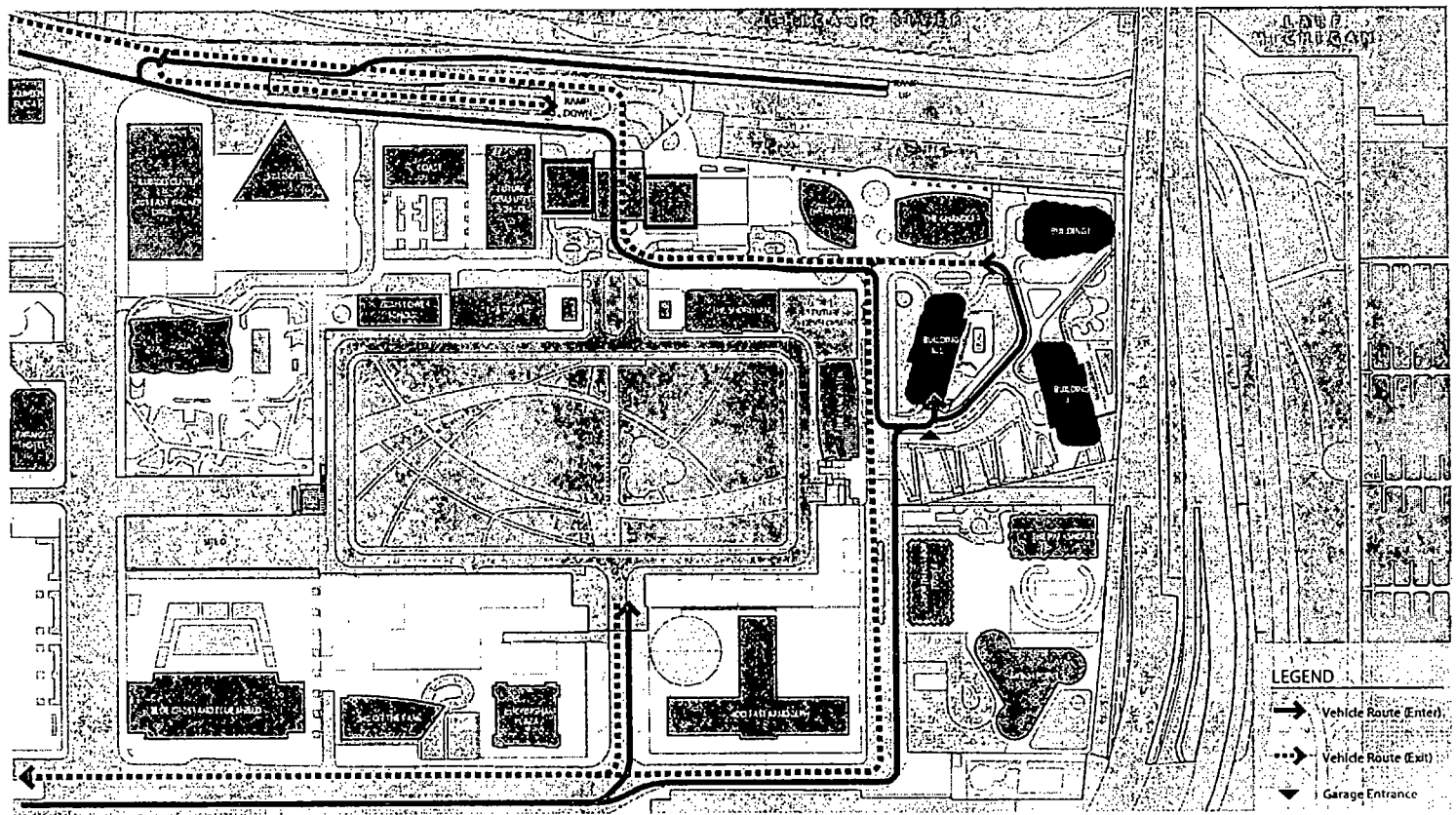


INTERMEDIATE LEVEL VEHICULAR ACCESS PLAN

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
Introduced • September 13, 2018
Plan Commission • October 18, 2018



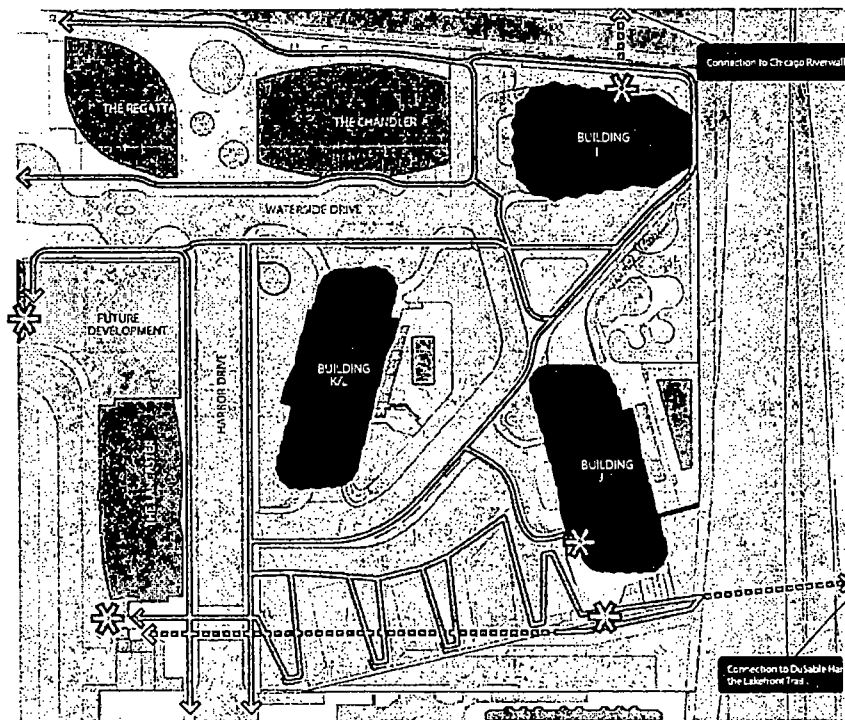
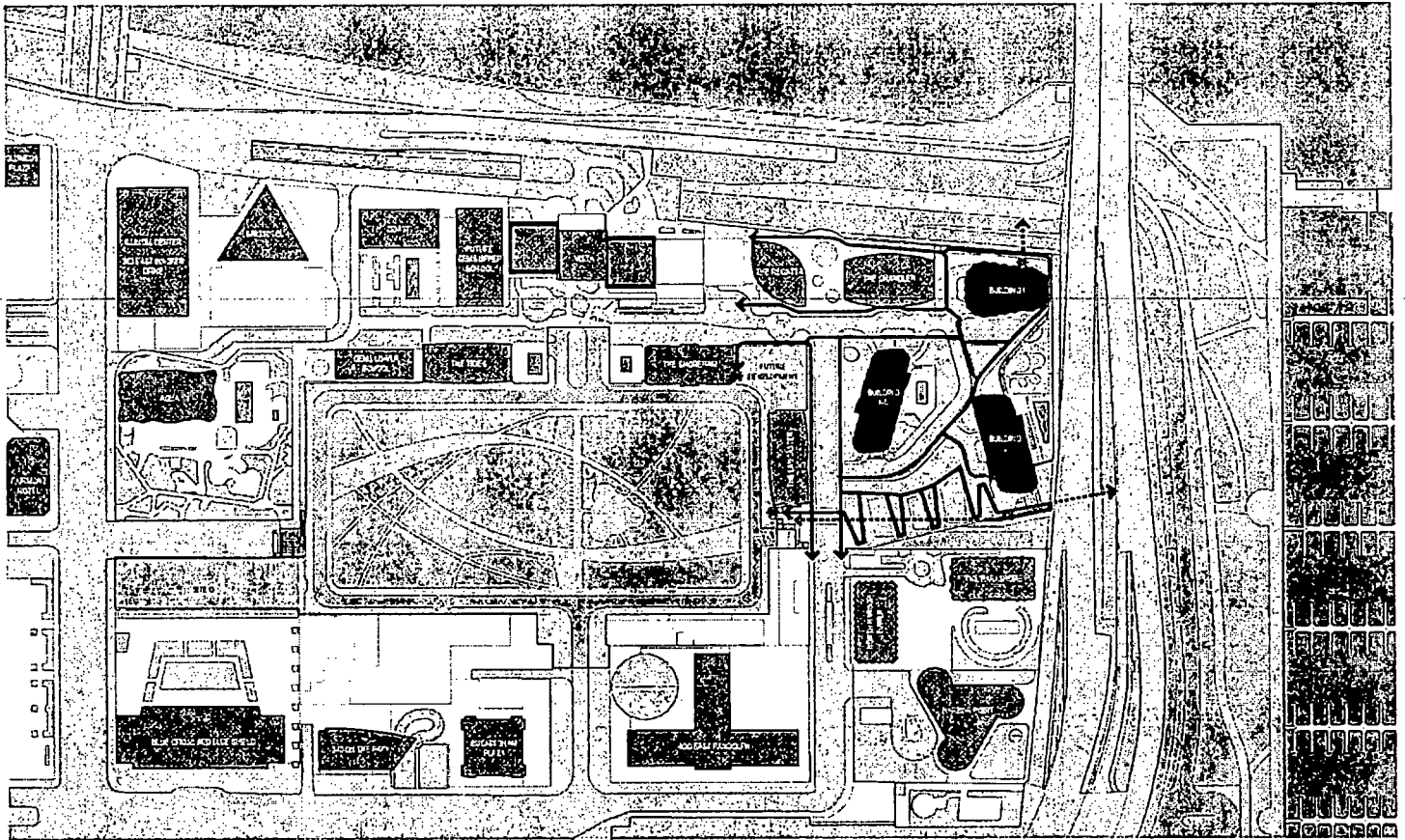
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UPPER LEVEL VEHICULAR ACCESS PLAN

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
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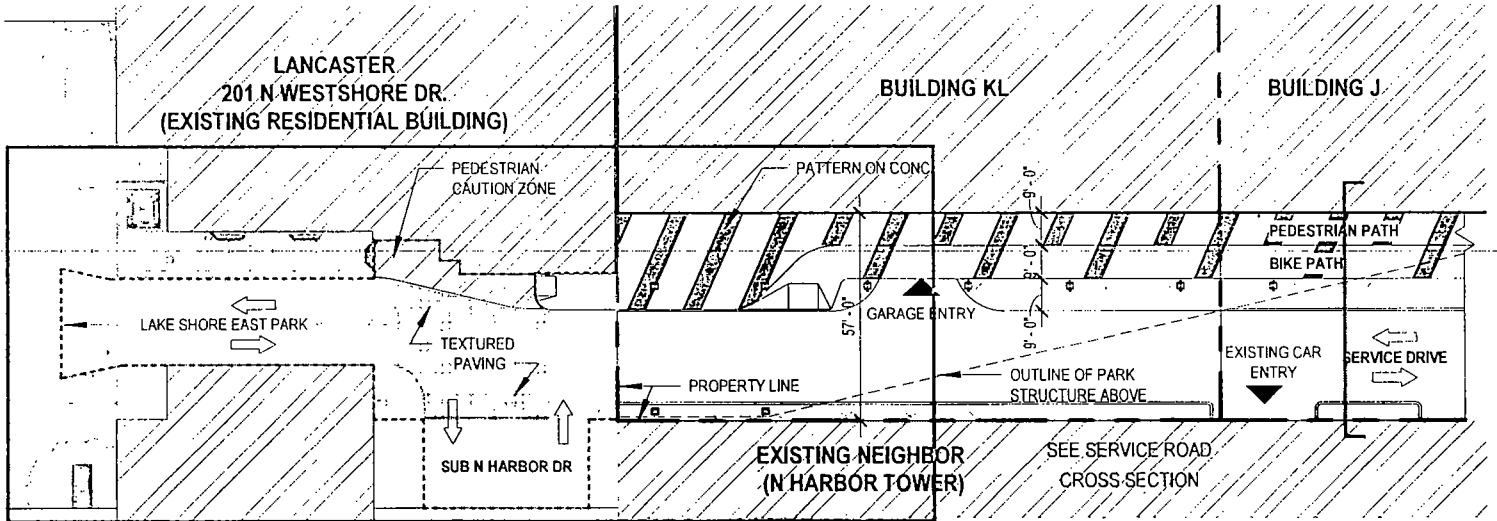


LEGEND	
	Pedestrian Route
	Vertical Circulation
	Security Post

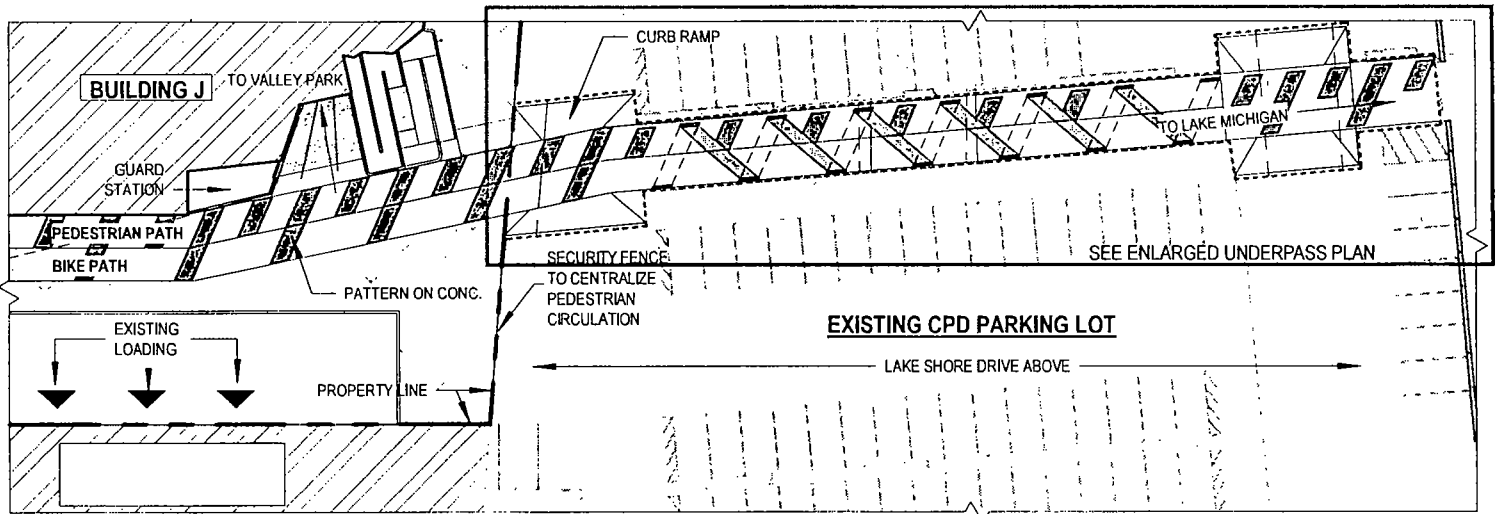
PEDESTRIAN ACCESS PLAN

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
 Applicant • IJKL, LLC and Lakeshore East LLC
 Introduced • September 13, 2018
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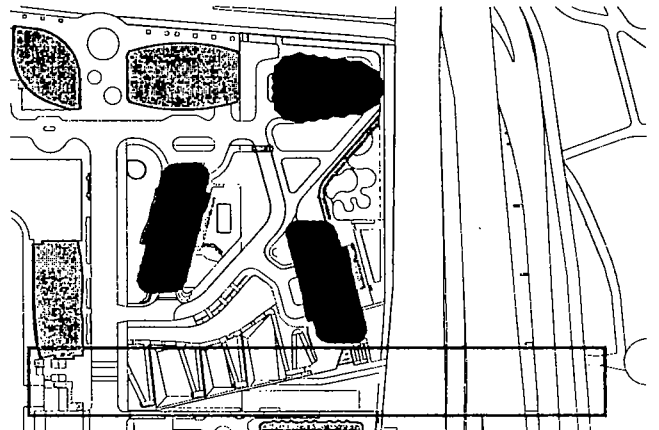




WEST SERVICE ROAD



EAST SERVICE ROAD AND LAKE SHORE DRIVE UNDERPASS



***ANY IMPROVEMENTS UNDER LAKE SHORE DRIVE ARE SUBJECT TO APPROVAL OF THE CHICAGO PARK DISTRICT AND/OR OTHER APPROPRIATE AUTHORITIES**

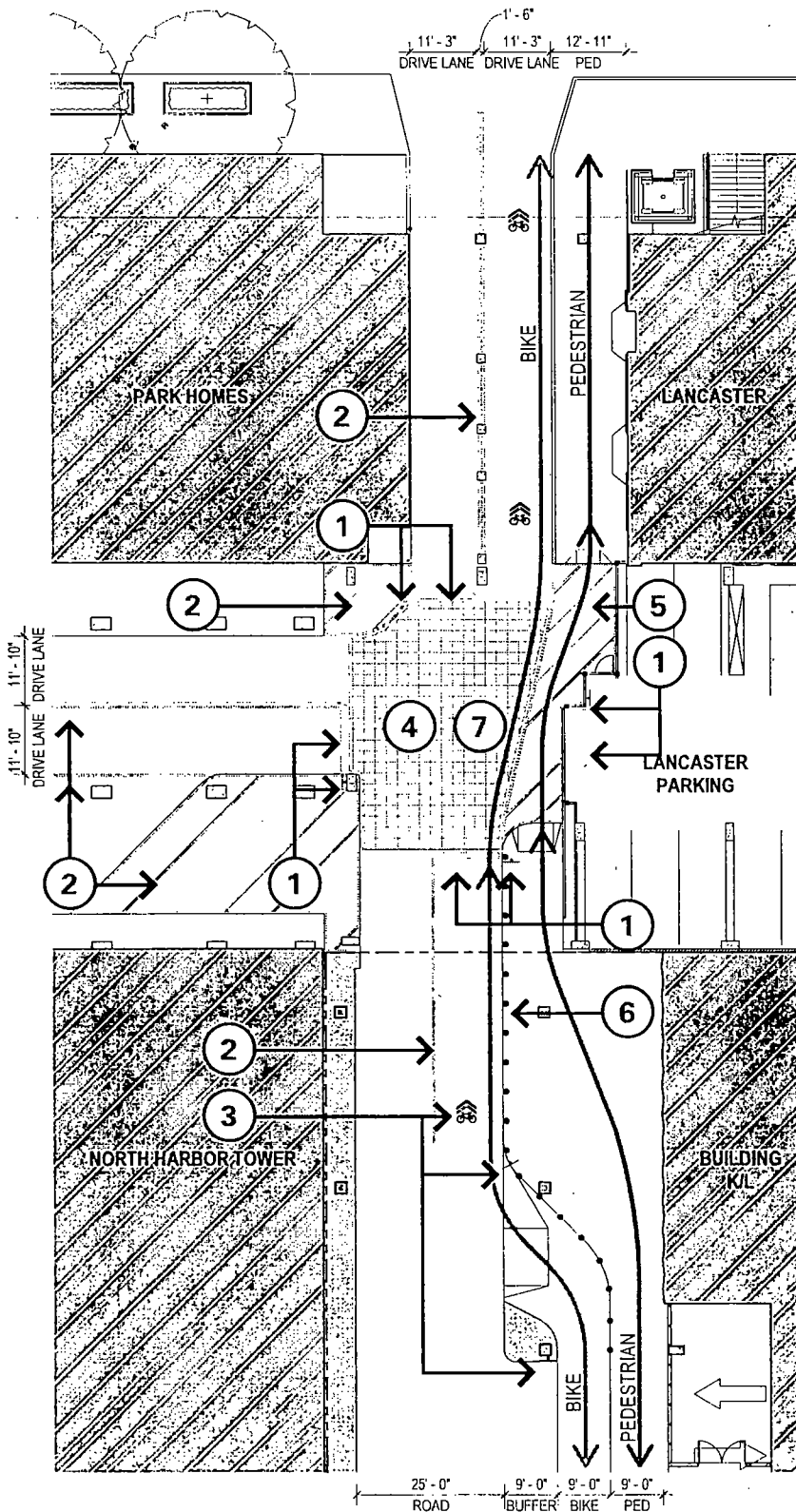
SERVICE ROAD PLAN

SCALE: 1" = 50'-0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
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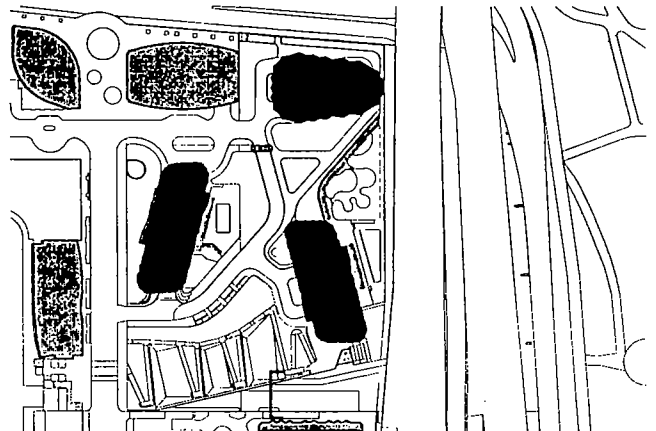
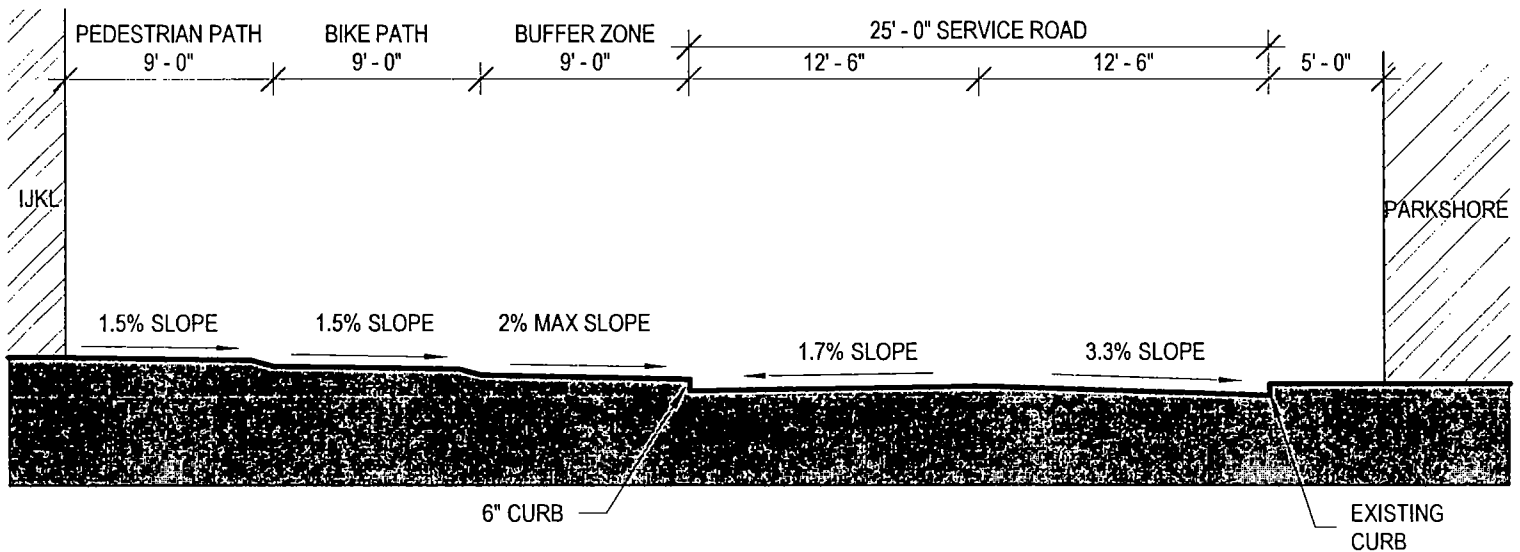


- 1 PROVIDE STOP SIGNS WITH STOP BARS TO ESTABLISH ALL-WAY STOP
- 2 PROVIDE LANE EDGE STRIPING TO HELP CLARIFY TRAFFIC CONTROL
- 3 PROVIDE "SHARROW" PAVEMENT MARKINGS AND CAUTION/MERGE SIGNAGE TO INDICATE BICYCLE LANE MERGING WITH DRIVE LANE
- 4 PROVIDE TEXTURED PAVING MATERIAL TO SLOW BICYCLE SPEEDS AND INCREASE AWARENESS OF INTERSECTION FOR APPROACHING DRIVERS
- 5 REFLECTIVE STRIPING APPLIED TO ASPHALT DESIGNATING PEDESTRIAN CAUTION ZONE
- 6 PROVIDE BOLLARDS TO PREVENT BICYCLES FROM USING PEDESTRIAN PATH
- 7 INSTALL FLASHING RED SIGNAL TO REINFORCE ALL-WAY STOP TRAFFIC CONTROL

LOWER HARBOR-SERVICE ROAD INTERSECTION
SCALE: N.T.S.



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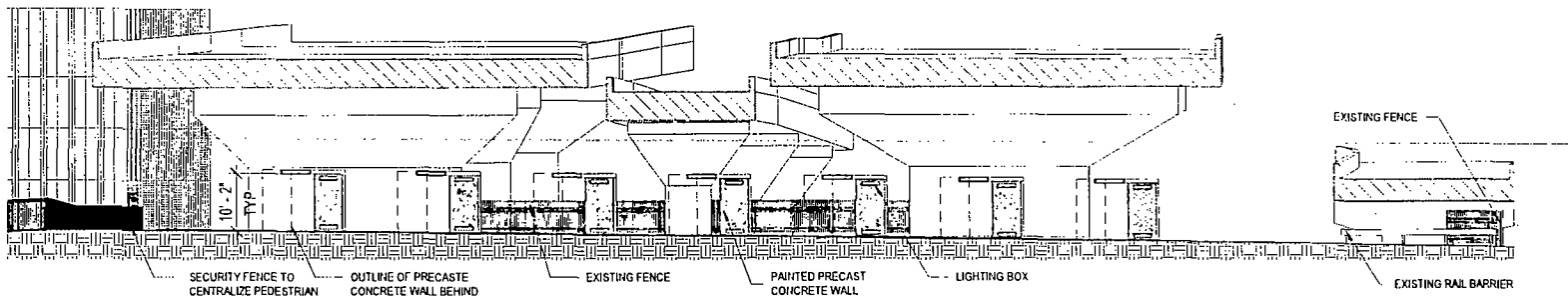


SERVICE ROAD CROSS SECTION
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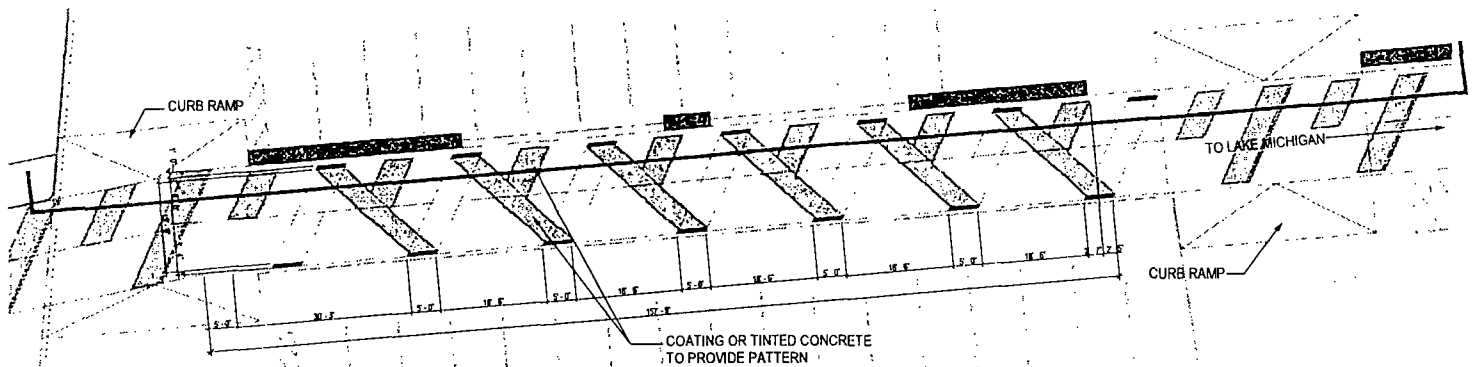
Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
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ENLARGED UNDERPASS SECTION

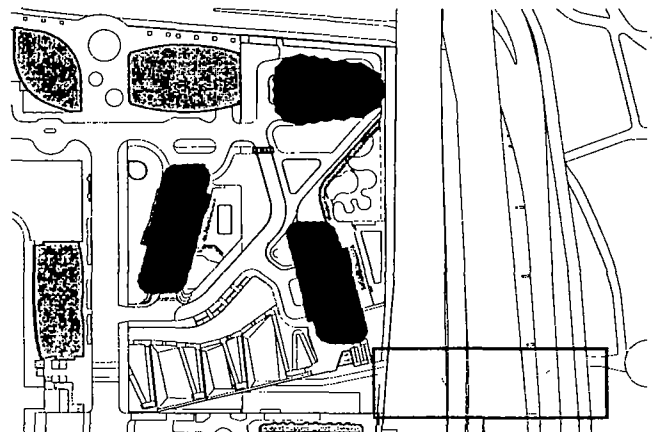


ENLARGED UNDERPASS PLAN

***ANY IMPROVEMENTS UNDER LAKE SHORE DRIVE ARE SUBJECT TO APPROVAL OF THE CHICAGO PARK DISTRICT AND/OR OTHER APPROPRIATE AUTHORITIES**

LAKE SHORE DRIVE UNDERPASS

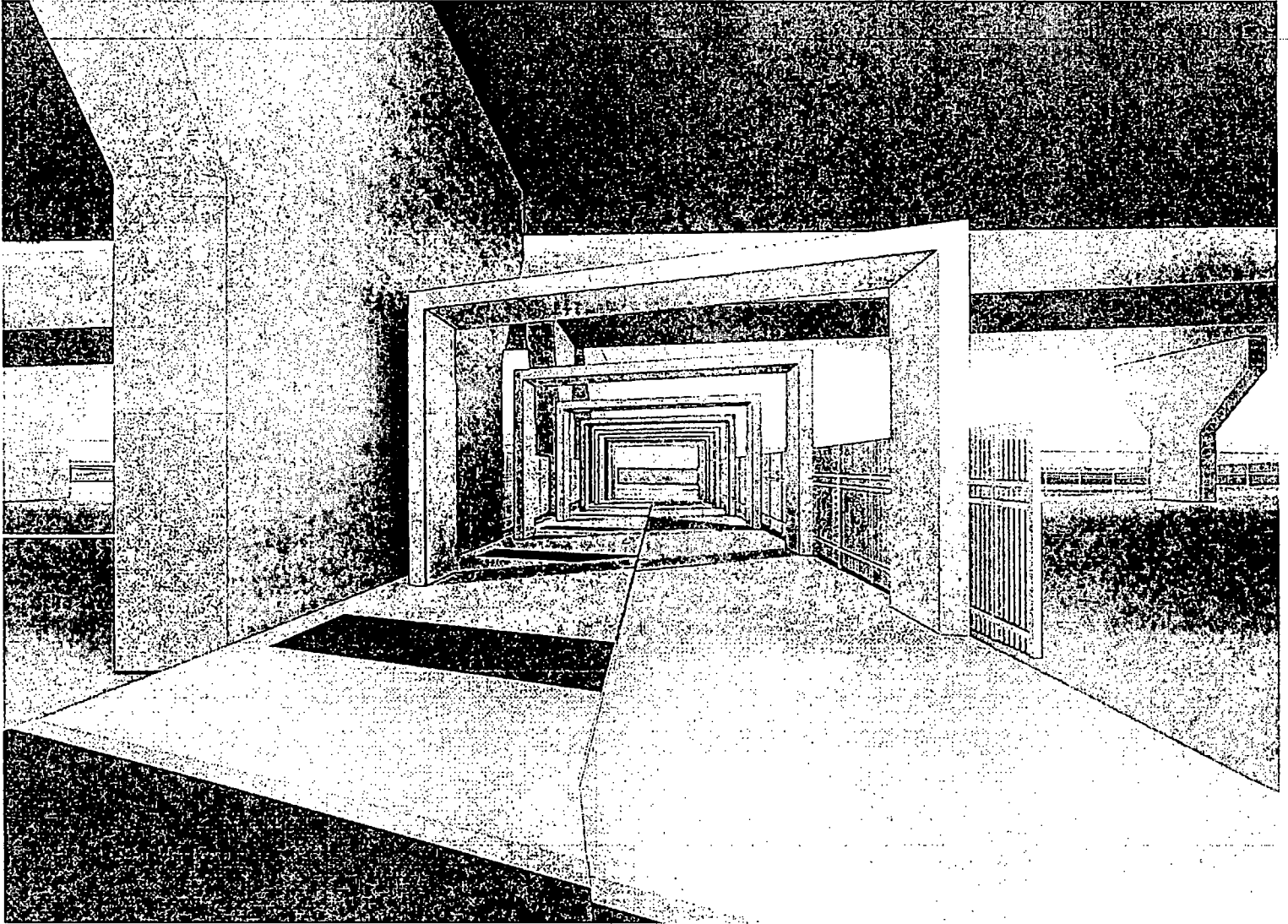
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Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
 Applicant • IJKL, LLC and Lakeshore East LLC
 Introduced • September 13, 2018
 Plan Commission • October 18, 2018

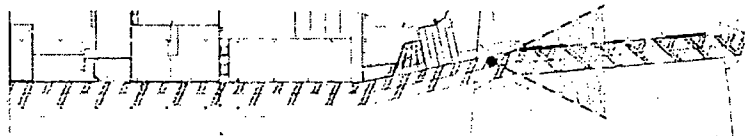


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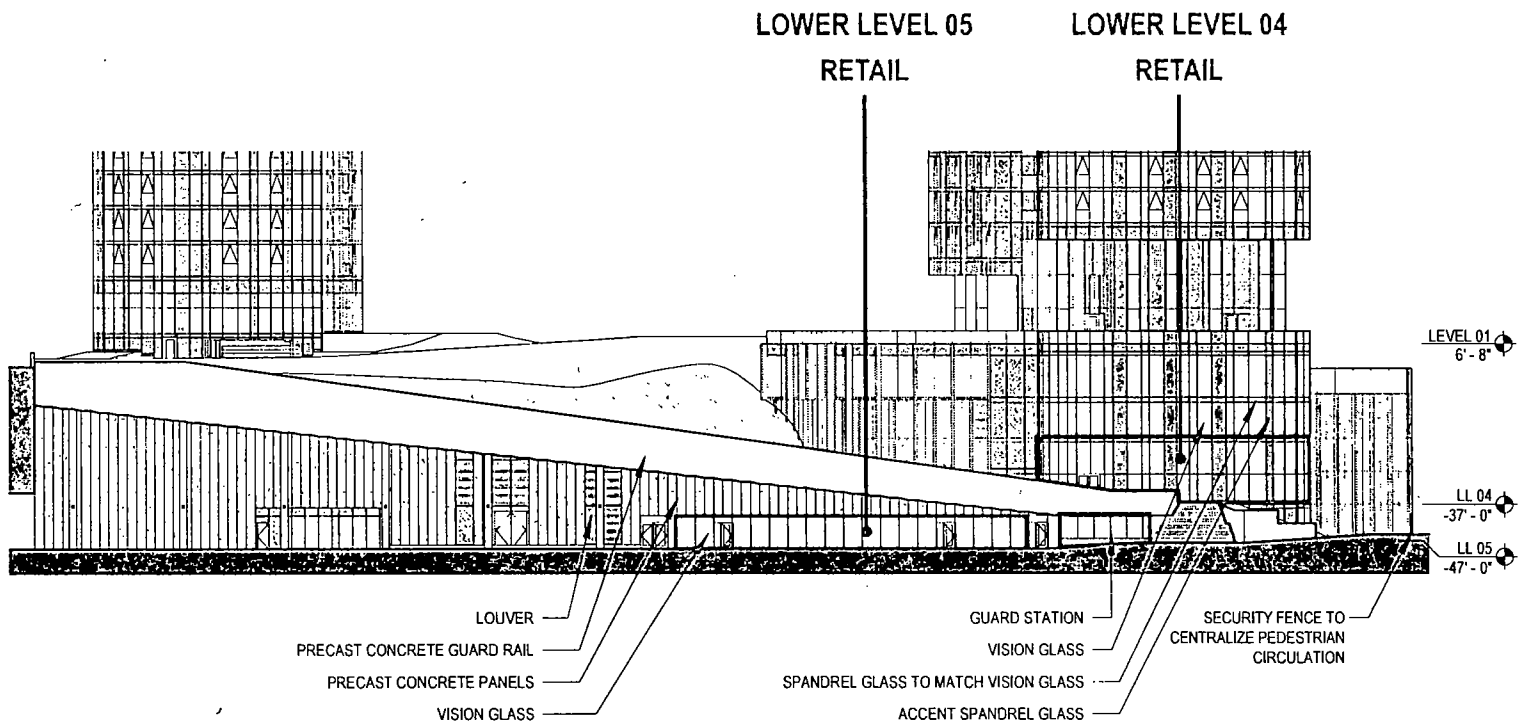
***ANY IMPROVEMENTS UNDER LAKE SHORE DRIVE ARE SUBJECT TO APPROVAL OF THE CHICAGO PARK DISTRICT AND/OR OTHER APPROPRIATE AUTHORITIES**

LAKE SHORE DRIVE UNDERPASS - RENDERING
NTS



Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
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LOWER LEVEL 05 BASE ELEVATION

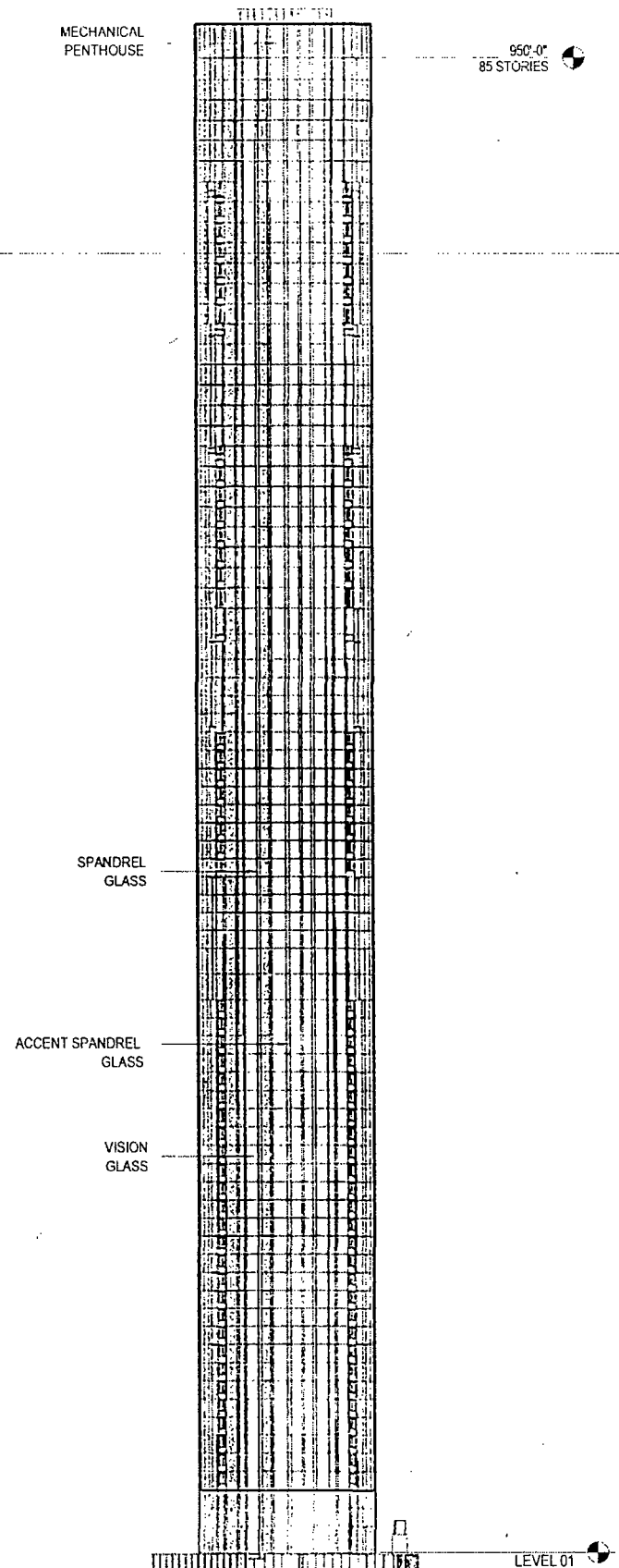
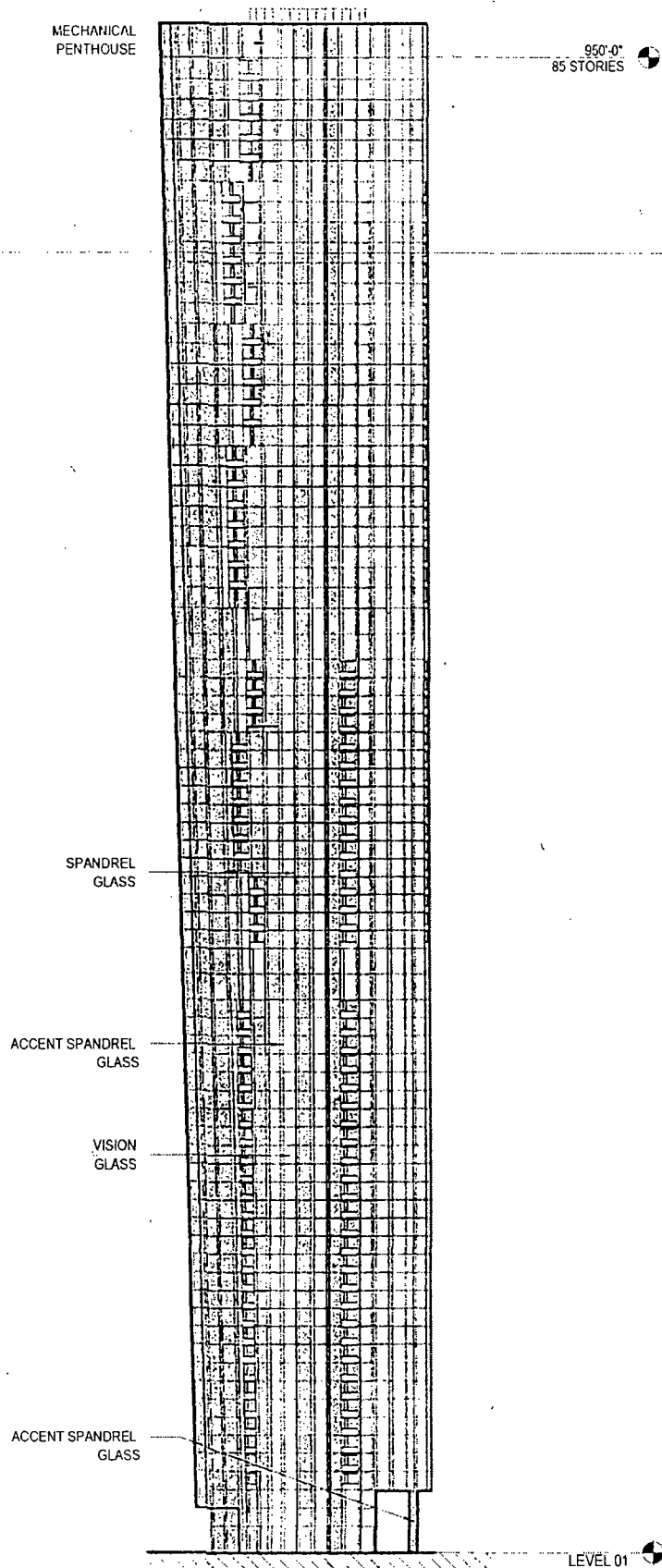
SCALE: 1" = 50'-0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive

Applicant • IJKL, LLC and Lakeshore East LLC

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BUILDING I - NORTH AND EAST ELEVATIONS

SCALE: 1" = 100'-0"

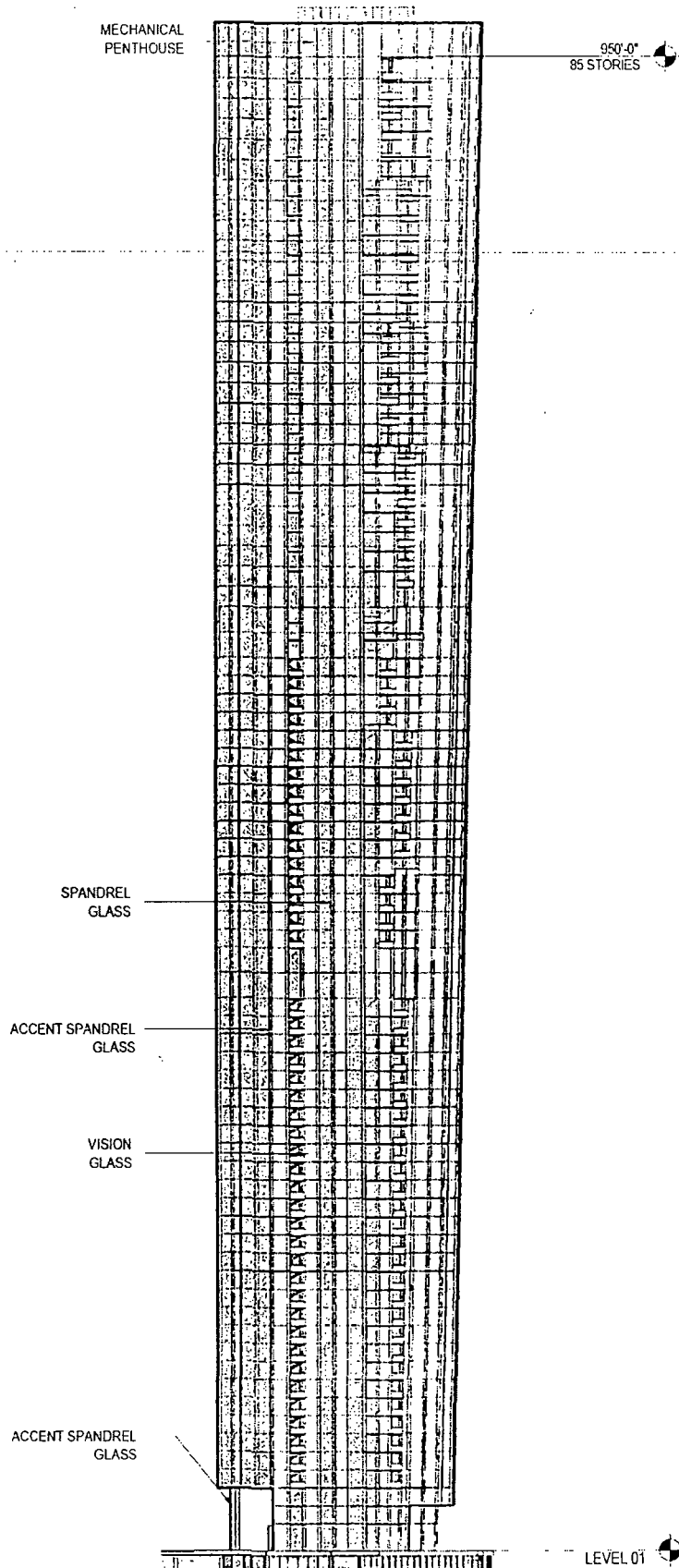
Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive

Applicant • IJKL, LLC and Lakeshore East LLC

Introduced • September 13, 2018

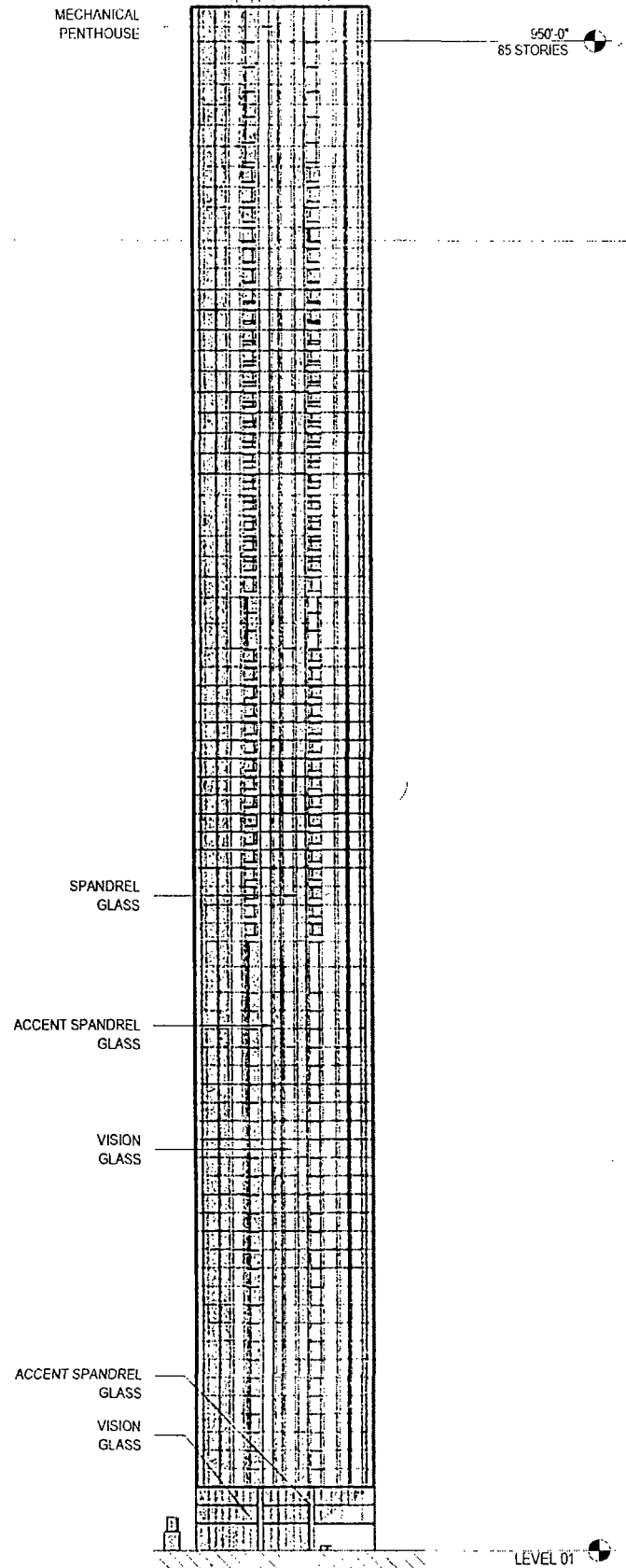
Plan Commission • October 18, 2018

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BUILDING I - SOUTH AND WEST ELEVATIONS

SCALE: 1" = 100'-0"



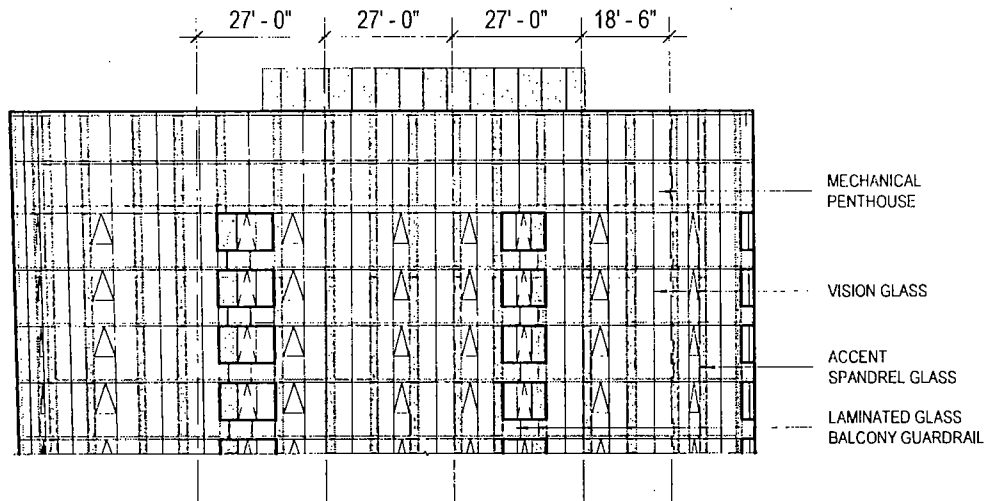
Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive

Applicant • IJKL, LLC and Lakeshore East LLC

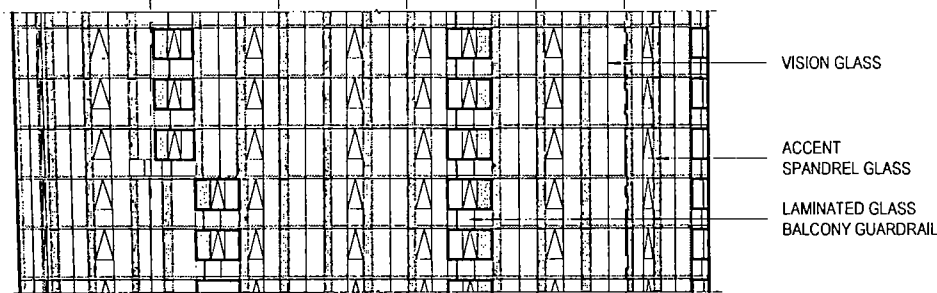
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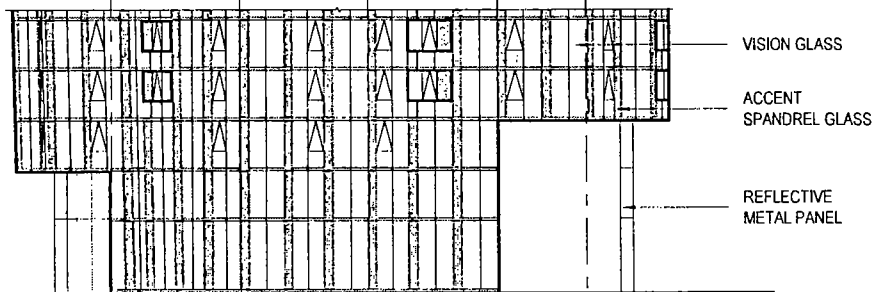
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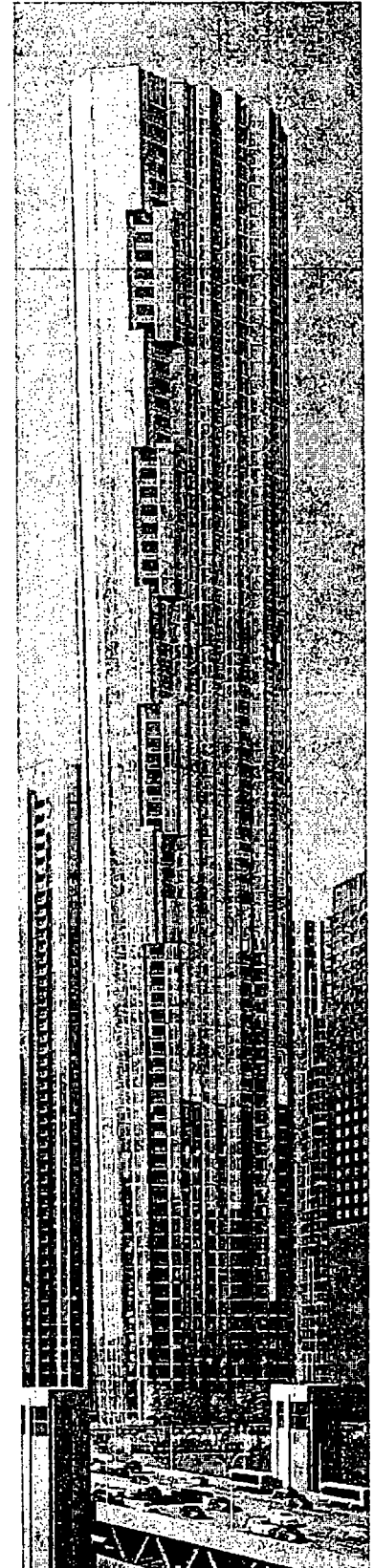
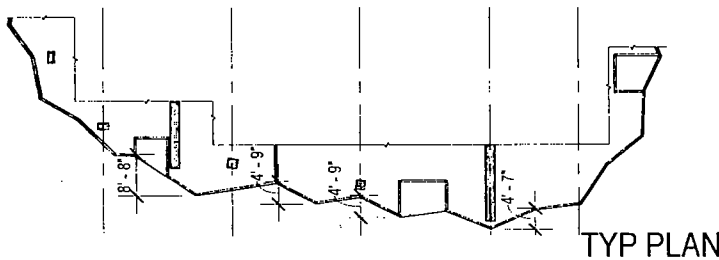
TOP



MIDDLE



BASE



BUILDING I - ENLARGED NORTH ELEVATION

SCALE: 1" = 40'-0"

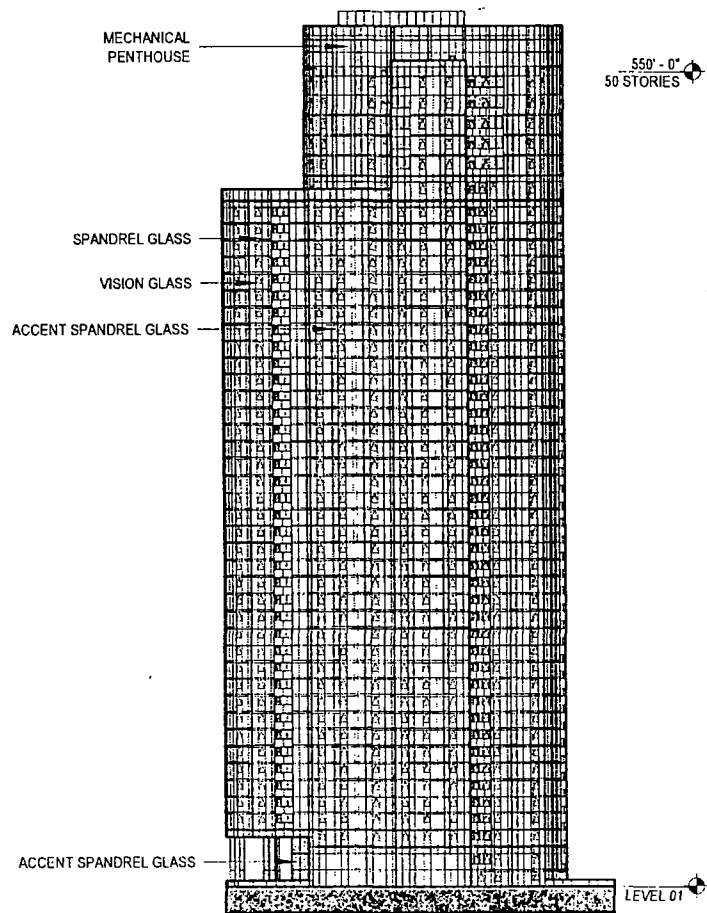
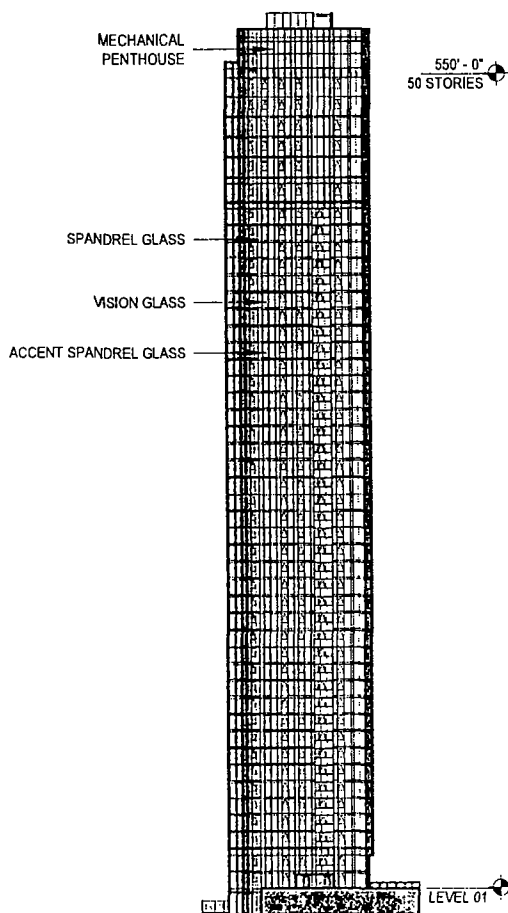
Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive

Applicant • IJKL, LLC and Lakeshore East LLC

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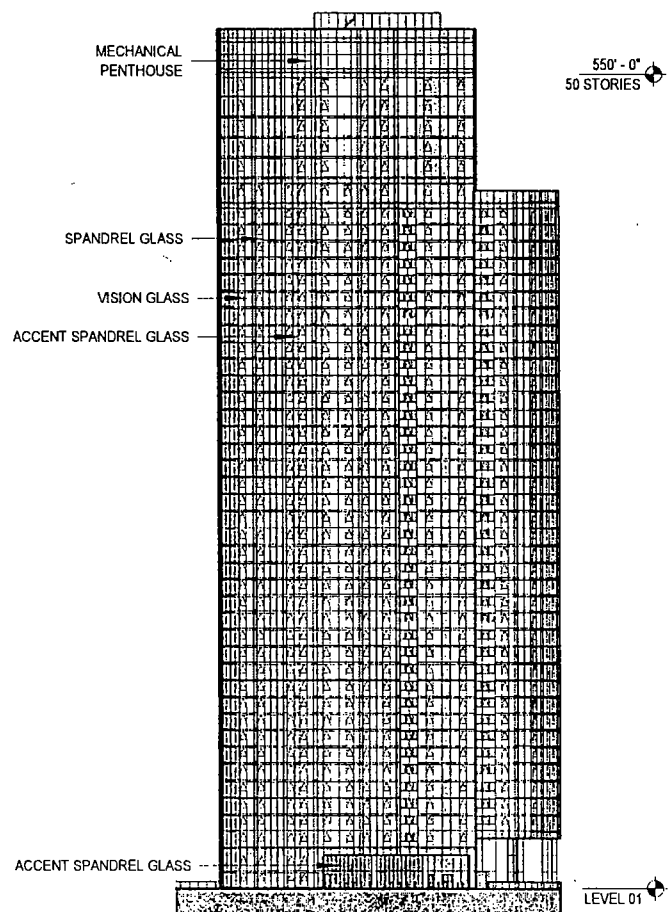
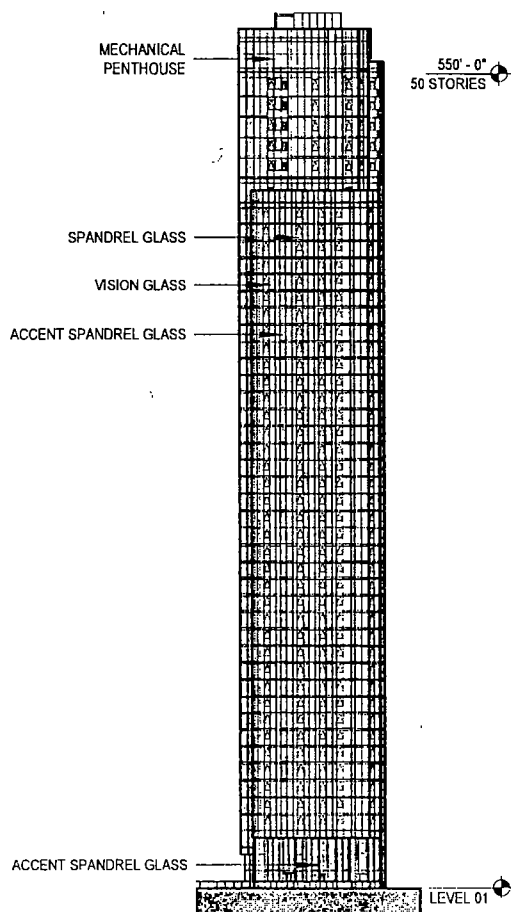
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BUILDING J- NORTH AND EAST ELEVATIONS
SCALE: 1" = 100'- 0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
Introduced • September 13, 2018
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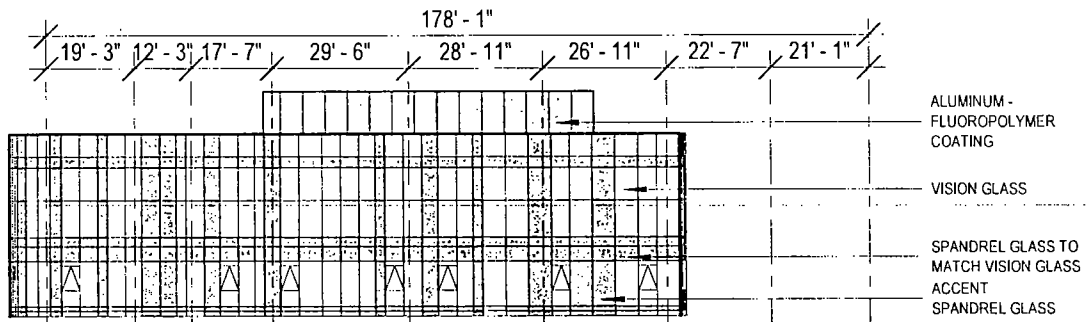
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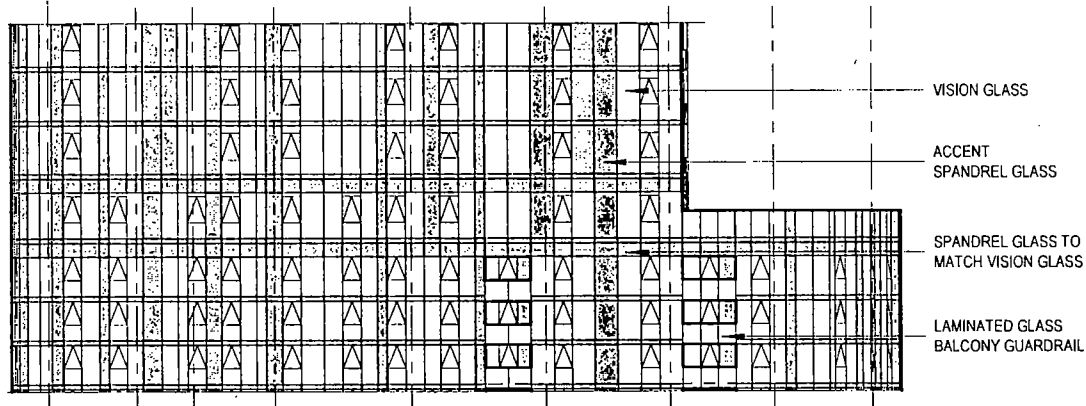
BUILDING J- SOUTH AND WEST ELEVATIONS
SCALE: 1" = 100'-0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
Applicant • IJKL, LLC and Lakeshore East LLC
Introduced • September 13, 2018
Plan Commission • October 18, 2018

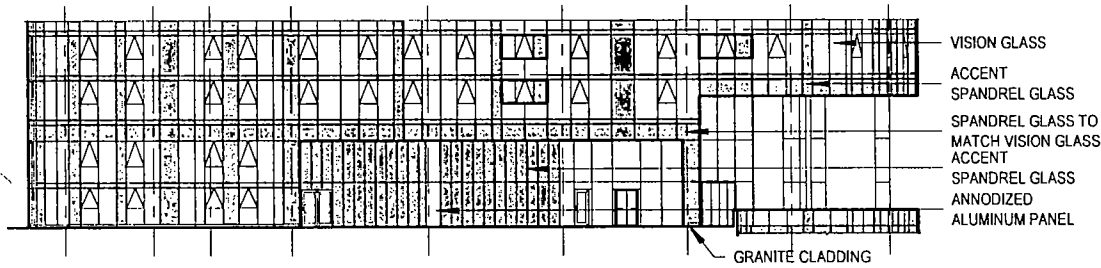
Final for Publication



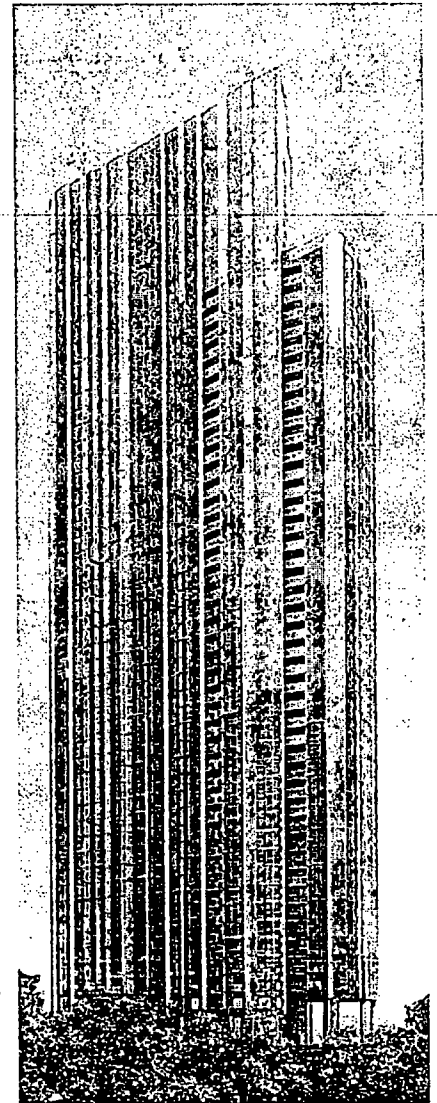
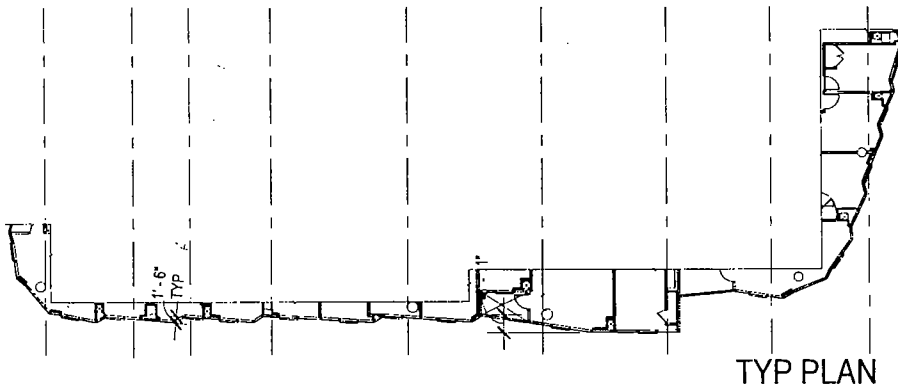
TOP



MIDDLE



BASE



BUILDING J - ENLARGED WEST ELEVATION

SCALE: 1" = 40'-0"

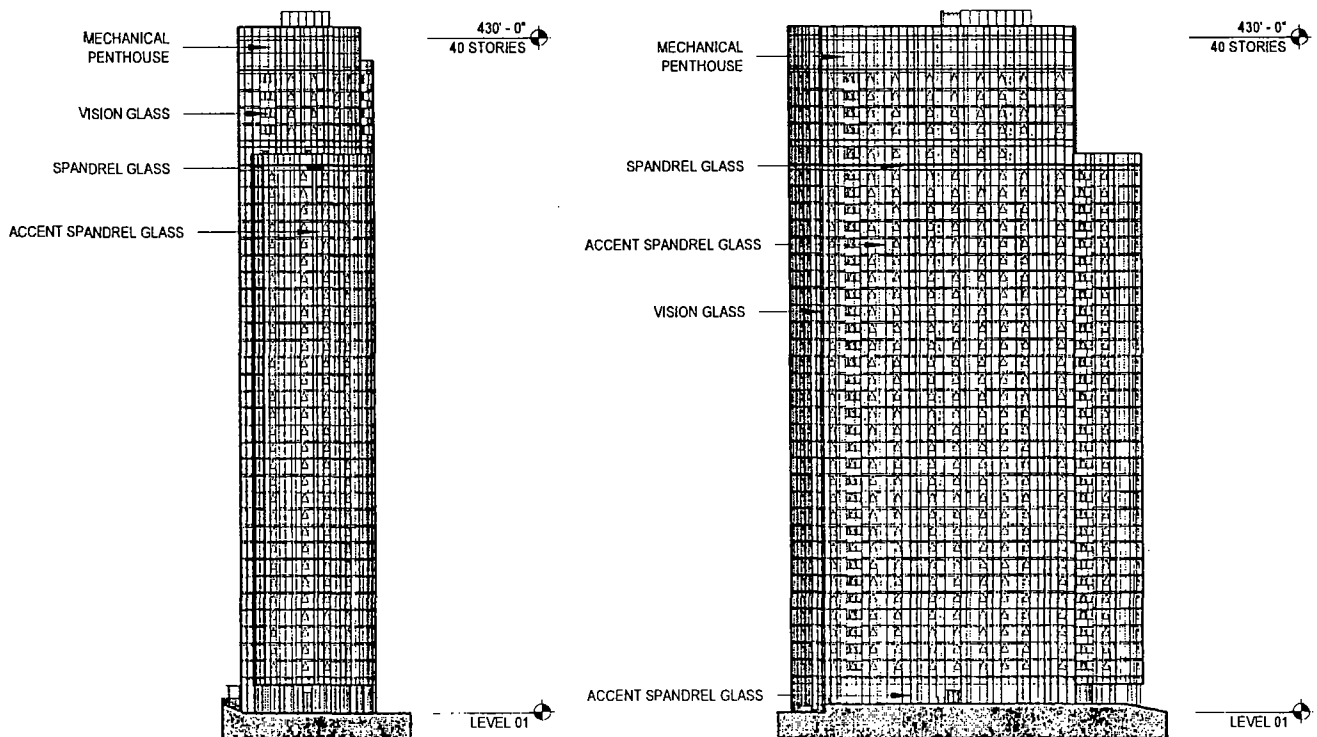
Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive

Applicant • IJKL, LLC and Lakeshore East LLC

Introduced • September 13, 2018

Plan Commission • October 18, 2018

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BUILDING KL - NORTH AND EAST ELEVATIONS

SCALE: 1" = 100'-0"

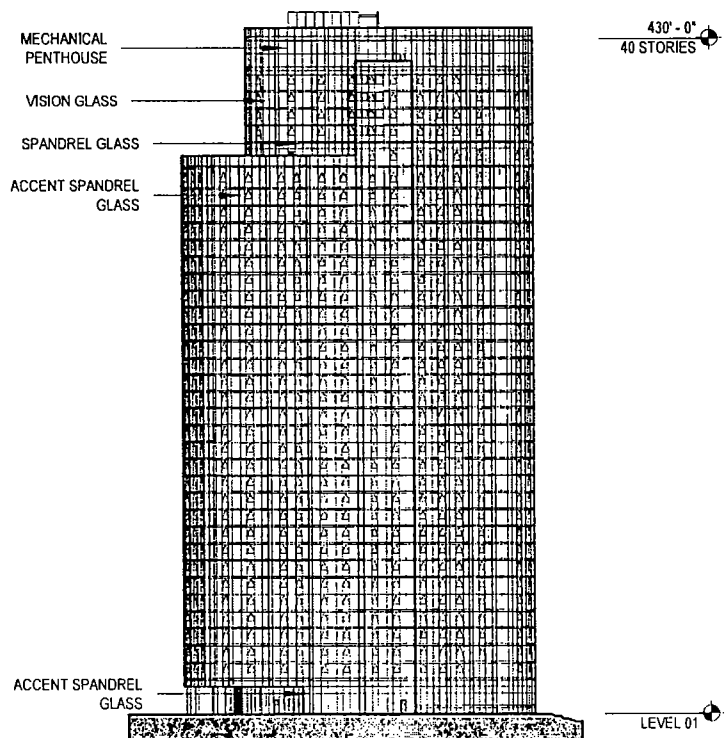
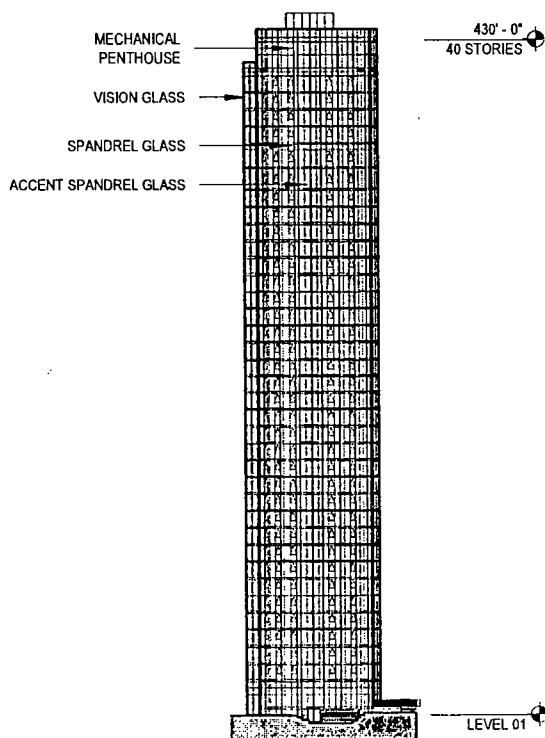
Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive

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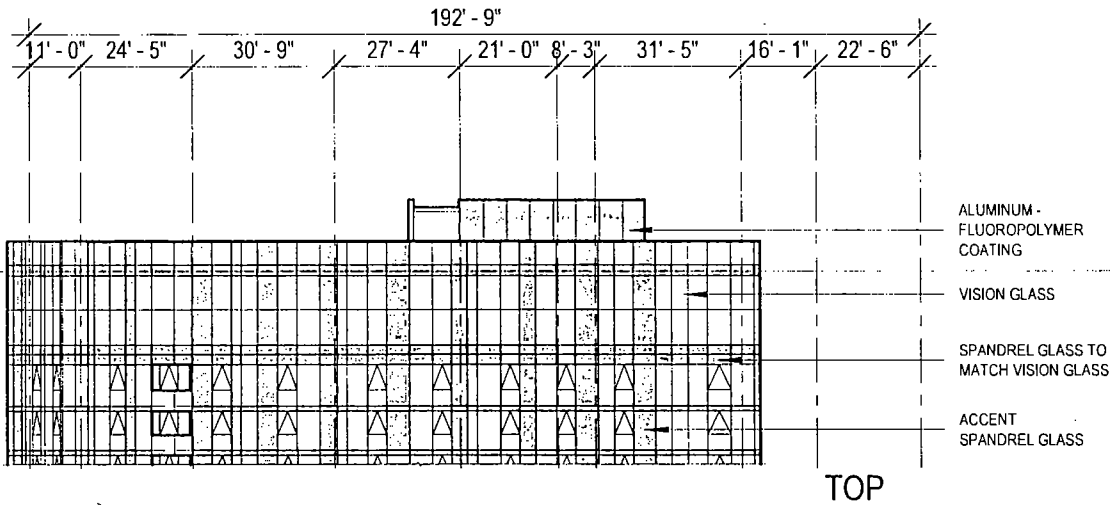
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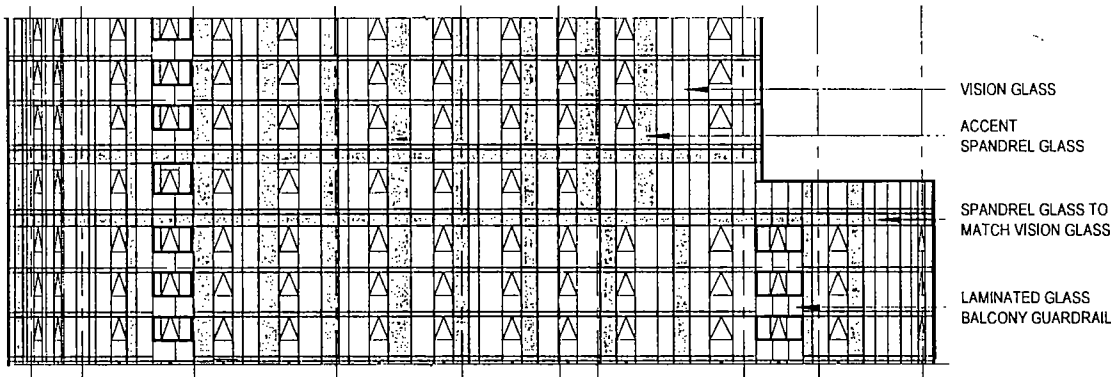
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SCALE: 1" = 100'-0"

Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive
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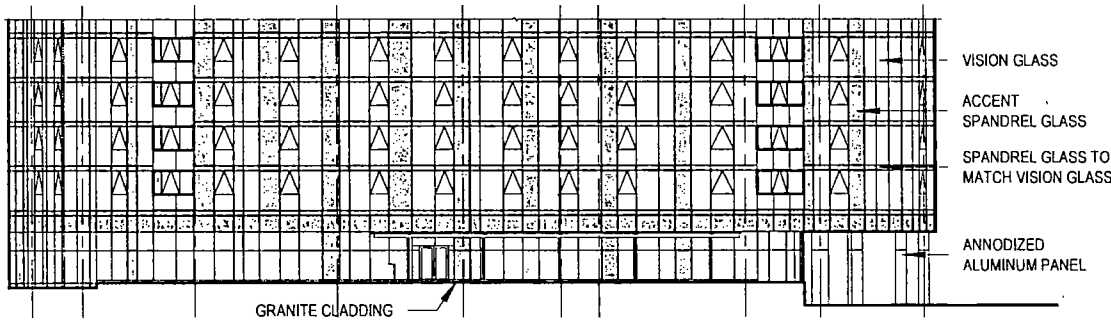
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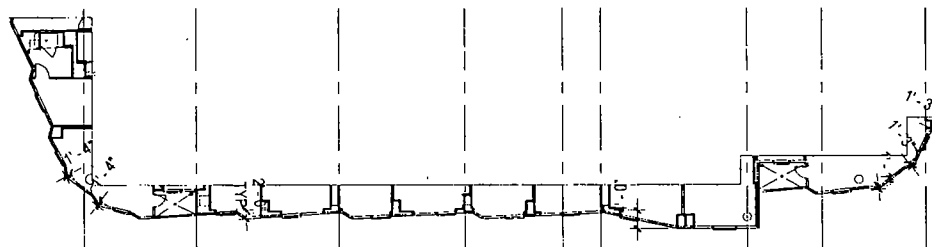
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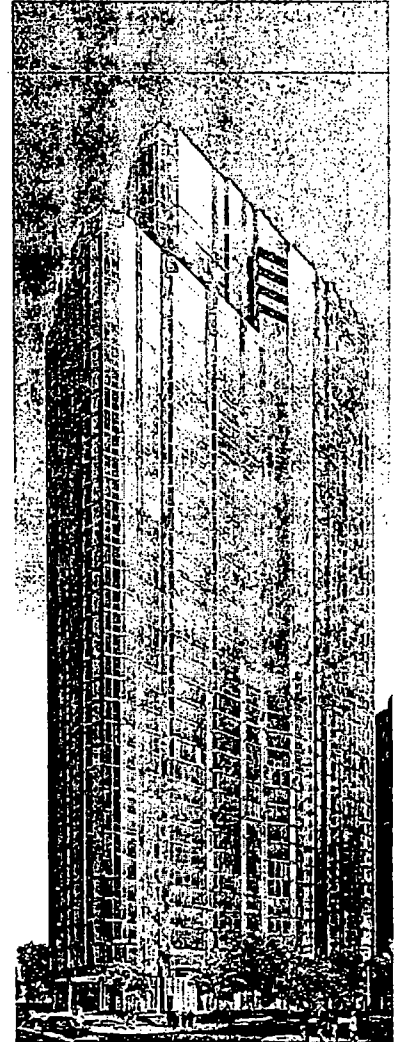
MIDDLE



BASE



TYP PLAN



BUILDING K/L - ENLARGED EAST ELEVATION

SCALE: 1" = 40'-0"

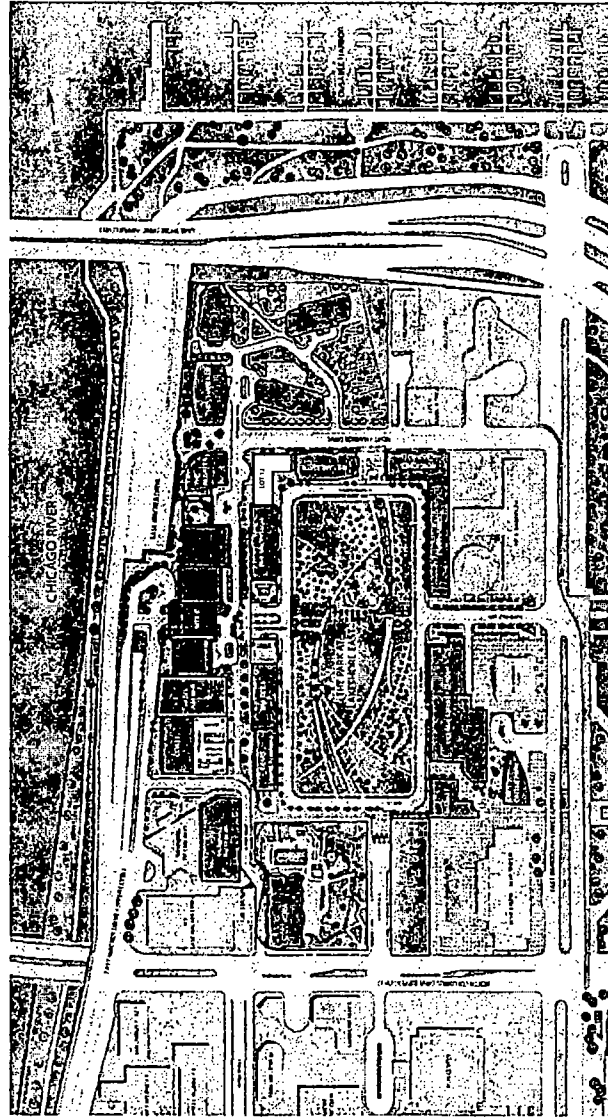
Address • 197-301 North Harbor Drive and 425-500 East Waterside Drive

Applicant • IJKL, LLC and Lakeshore East LLC

Introduced • September 13, 2018

Plan Commission • October 18, 2018

MASTER PLAN & DESIGN STANDARDS
LAKESHORE EAST
Chicago, Illinois



Prepared for
Lakeshore East LLC
originally prepared by
SKIDMORE, OWINGS & MERRILL, LLP
with revisions prepared by
BKL ARCHITECTURE LLC.

UPDATED TO INCLUDE MINOR CHANGES AND MODIFICATIONS
BY THE CITY OF CHICAGO DEPARTMENT OF PLANNING AND DEVELOPMENT
THROUGH OCTOBER 18, 2018

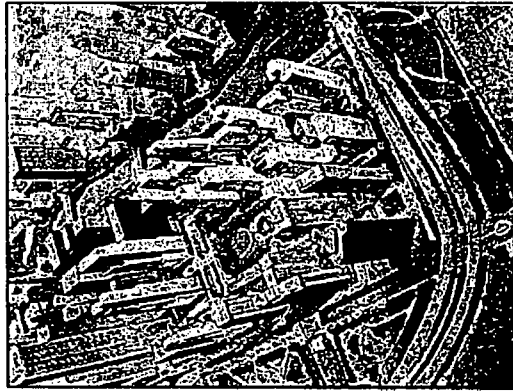
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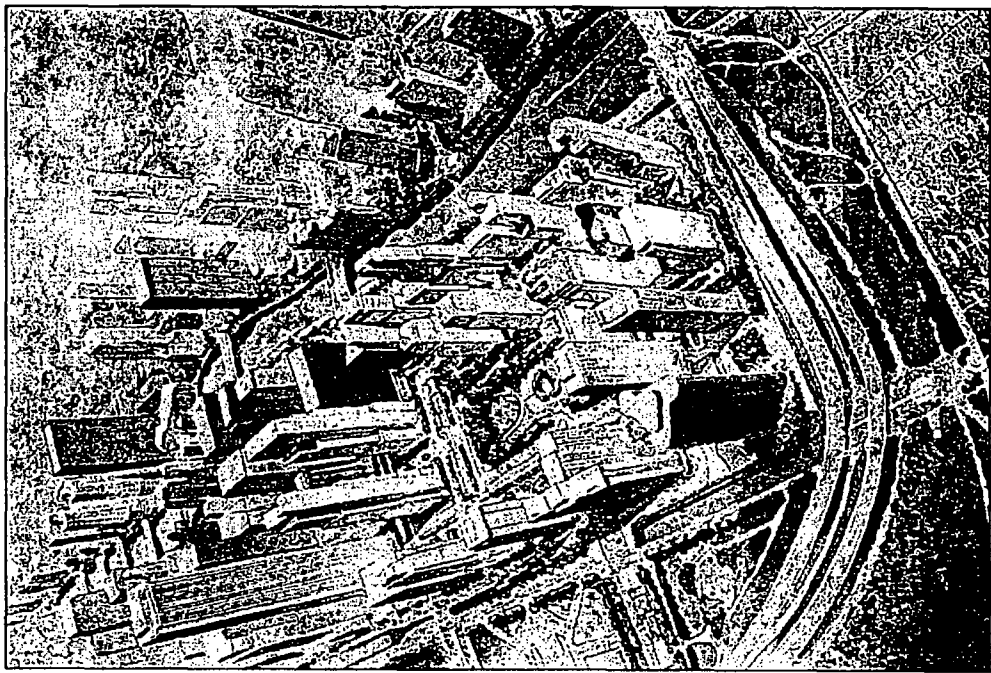
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Document Intent		
Existing Zoning Map		
Planned Development Boundary		
Sub Area Map		
2.0 Urban Design Principles		
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Urban Framework		
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Open Space		
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Bulk Regulations and Data Table		
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Parcelization Plan Upper Level		
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Existing Utilities Easement		
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Highrise Building Design Criteria		
Lowrise Building Design Criteria		
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Building Materials		
Environmental Considerations		
Maximum Building Envelopes		
Illustrative Elevations		
Parcel Criteria / Maximum Building Envelopes		

Lakeshore East Master Plan • Chicago, Illinois
Lakeshore East LLC

-
- The map illustrates the layout of the Lakeside East development. Key areas include:
- Buildings:** Labeled A through O, representing various structures such as hotels, clubs, and commercial buildings.
 - Streets:** Major thoroughfares like Wacker Drive, North Field Blvd, and South Field Blvd are shown.
 - Parks and Open Spaces:** Areas like 'The Park at Lakeside East' and 'The Shoreham' provide green space within the development.
 - Infrastructure:** Features like parking lots, walkways, and transit stations are indicated.
- A compass rose in the upper right corner points North, and a scale bar indicates distances within the development.

1.0 Introduction





LAKESHORE EAST
MASTER PLAN AND DESIGN STANDARDS

Introduction

Lakeshore East is a new mixed-use community that will be developed on Illinois Center railroad land between Lake Shore Drive on the east, Michigan Avenue on the west, Randolph Street on the south and Wacker Drive on the north. Lakeshore East, located within Illinois Center east of Columbus Drive, will include residential, hotel, office, mixed-use development and a new public park.

Development on the approximately 52 acres of land within these Illinois Center boundaries has been regulated by a number of Planned Developments approved by the City of Chicago since 1969. Illinois Center is the subject of the existing Planned Development No. 70. This Planned Development was originally approved in 1969. It was subsequently amended in 1979, and then again in 1993.

Most of the Illinois Center property west of Columbus Drive has now been developed. East of Columbus Drive, there are also a number of existing buildings, including: an office building at Three Illinois Center; the Blue Cross-Blue Shield building; the Swiss Grand Hotel and Five high rise residential buildings. In addition, west of Columbus Drive, the residential Park Millennium is currently under construction.

Lakeshore East will be created on approximately 28 acres of land that remain between Columbus Drive and Lake Shore Drive. The development of Lakeshore east will be guided by a new amendment

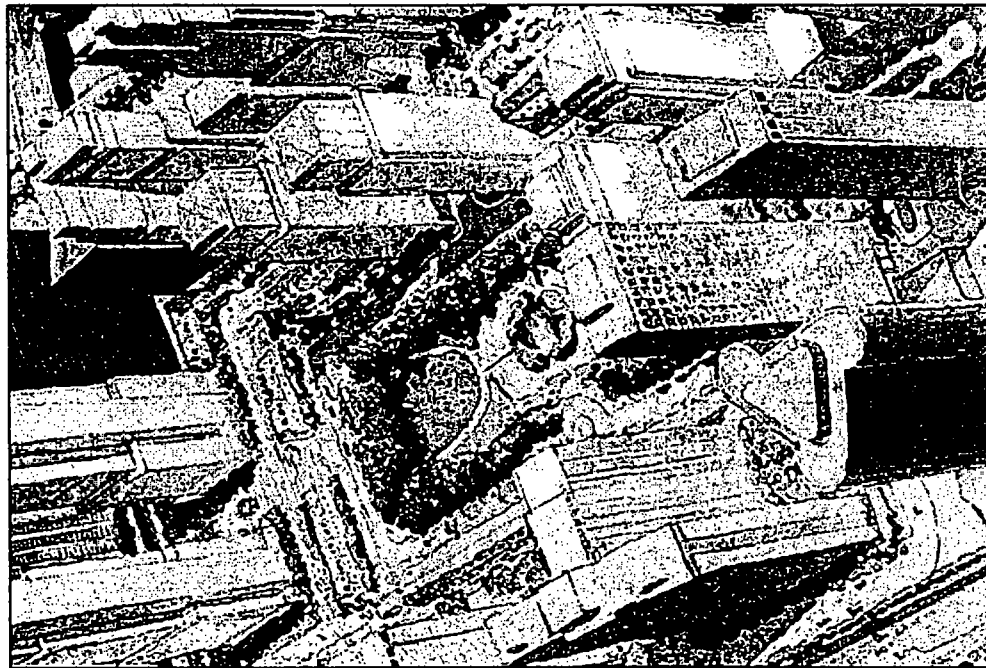
to Planned Development No. 70. This area is identified as Sub-Area E within Planned Development No. 70, and is currently occupied by a golf course facility and other open space located on the property east of Columbus Drive. Lakeshore East will be located within this area.

The 1993 Amendment to Planned Development No. 70 permitted approximately 14.5 million square feet to be constructed on a net developable area of 1.1 million square feet (28 acres) within Sub-Area E

Under the new amendment to Planned Development No. 70 The Lakeshore East development will construct 9.7 million square feet of buildable area, which is approximately 2/3 of what was previously allowed. The net developable area has been reduced to under 700,000 square feet (14 acres) as a result of a significant increase in the land dedicated to the public for parks, open spaces and road rights of way

Lakeshore East will create a new urban neighborhood including residential condominiums, apartments and townhomes, hotel, office commercial, retail and mixed-use development and public open space including a new public park. A combined public school / park district facility is also anticipated to be included within the development.

This Lakeshore East Master Plan and Design Guidelines Standards both illustrates and will guide the organization, scale, form and character of new buildings, streets and open spaces over time

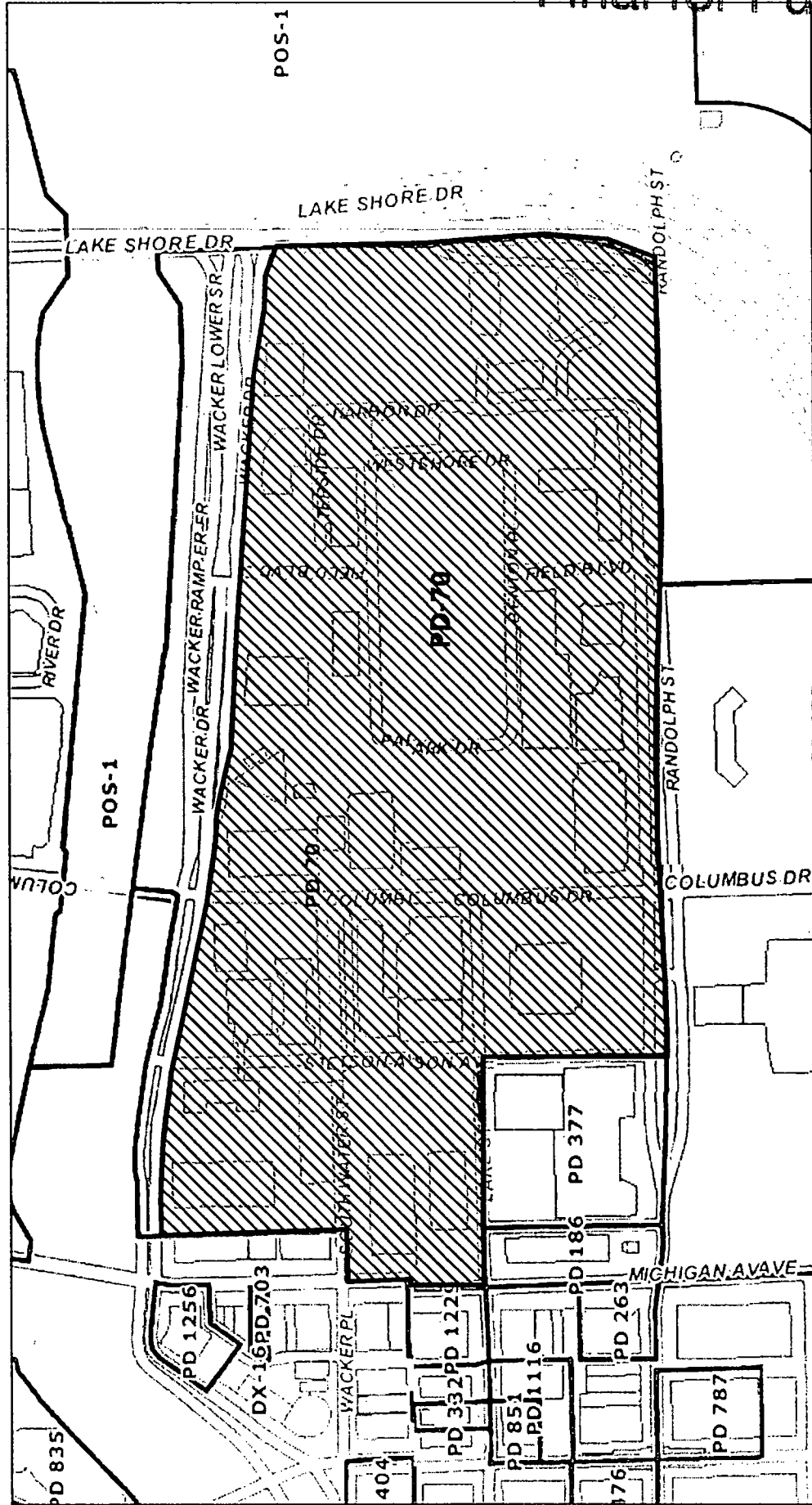


This document is intended to ensure that the future development of Lakeshore East is consistent with the vision and principles laid out in the *Lakeshore East Master Plan*.

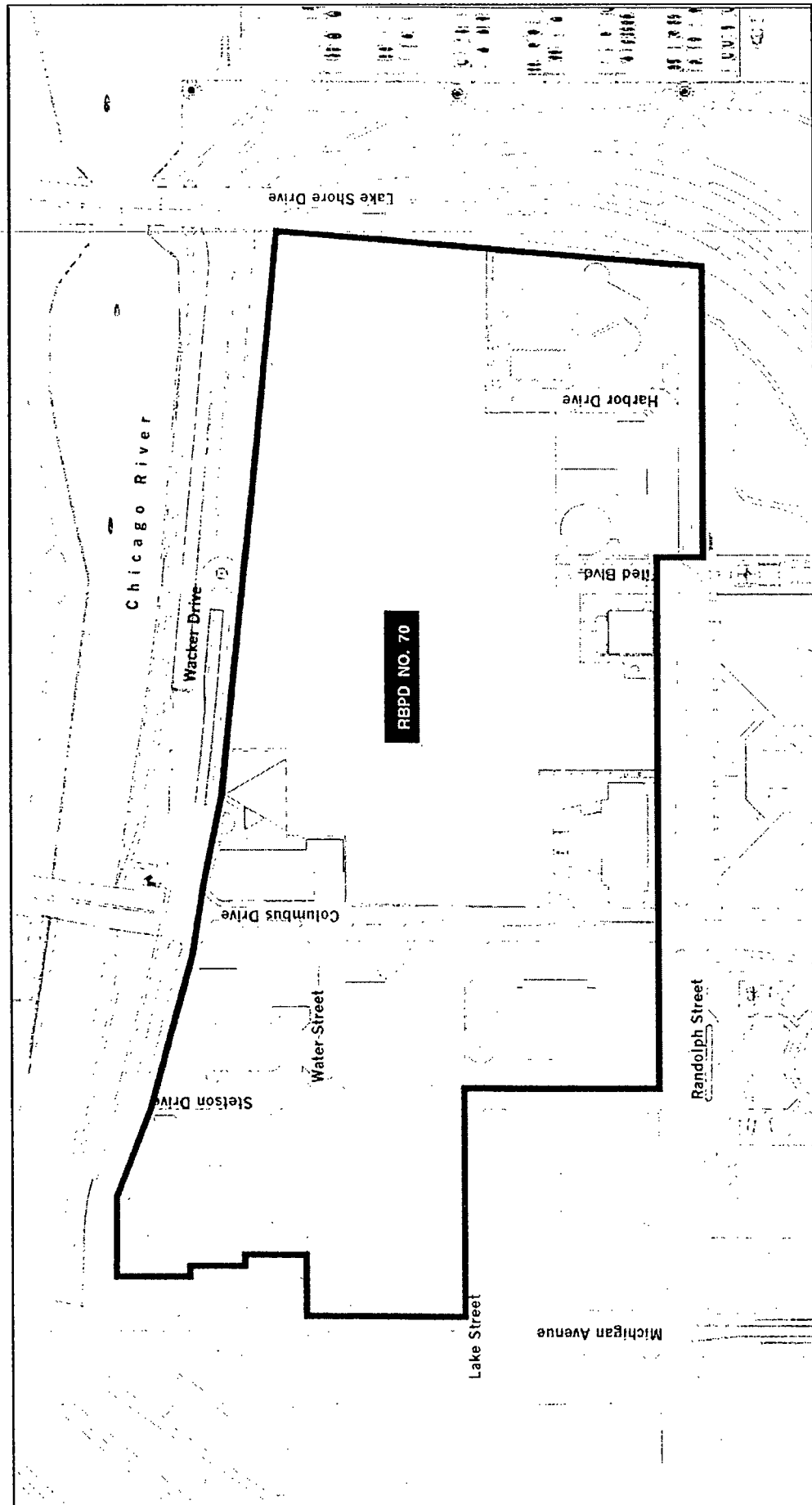
The document is organized into five sections:

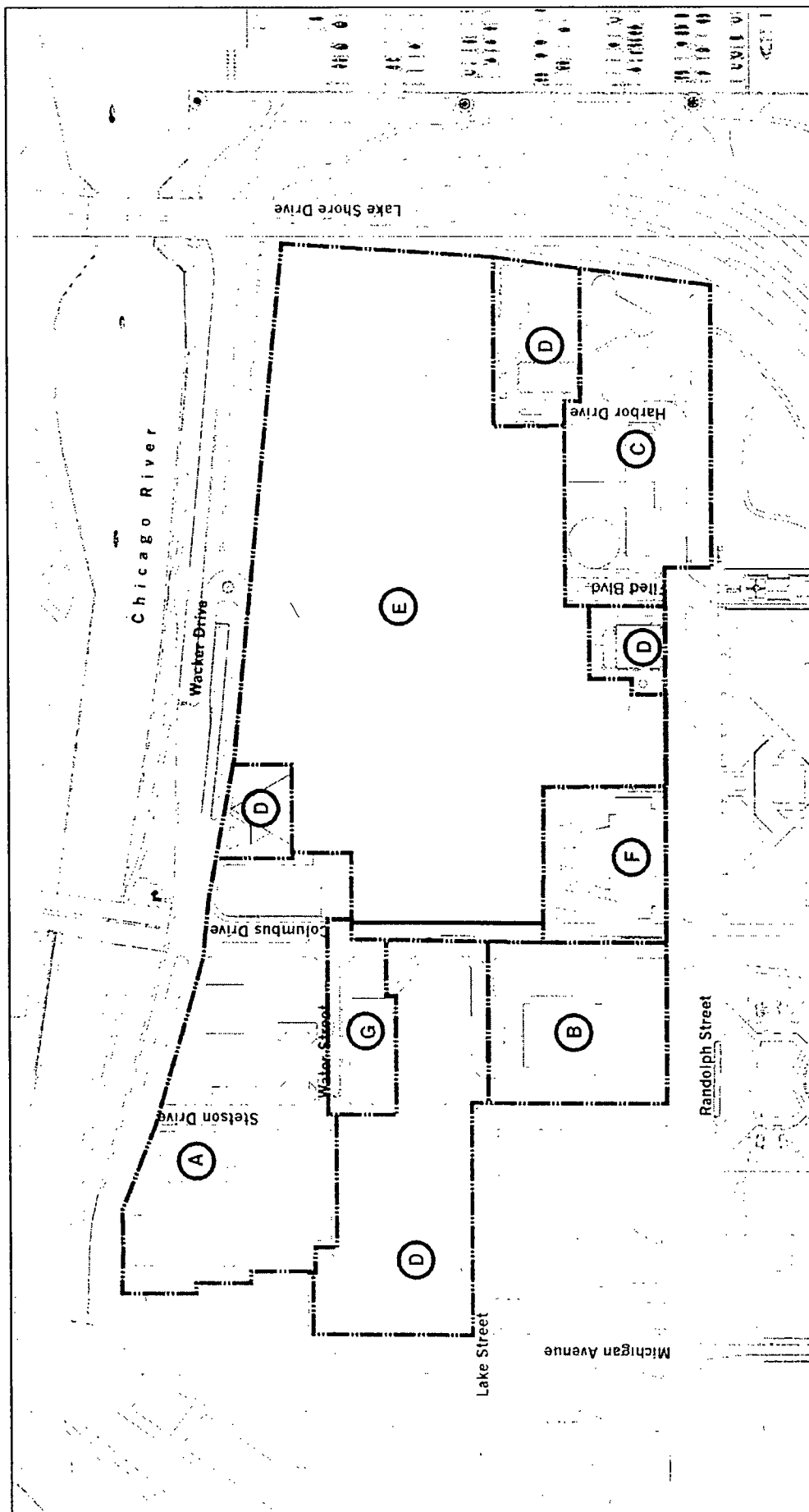
- **Urban Design Principles**
Basic design principles that will guide future development
- **Development Summary**
The framework for development
- **Access and Circulation**
Connections for cars and people
- **Open Space**
Parks and courtyards to create a green neighborhood
- **Design Standards**
Guidelines that will shape the character of buildings, and the public realm

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Residential Business Planned Development No. 70

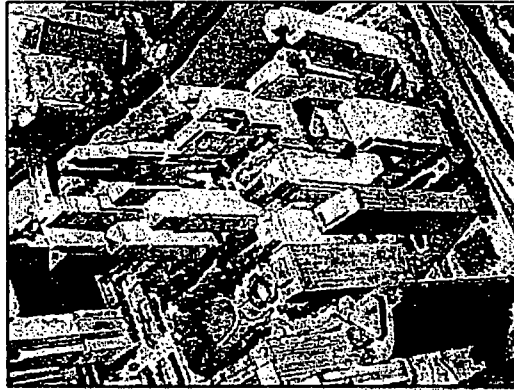


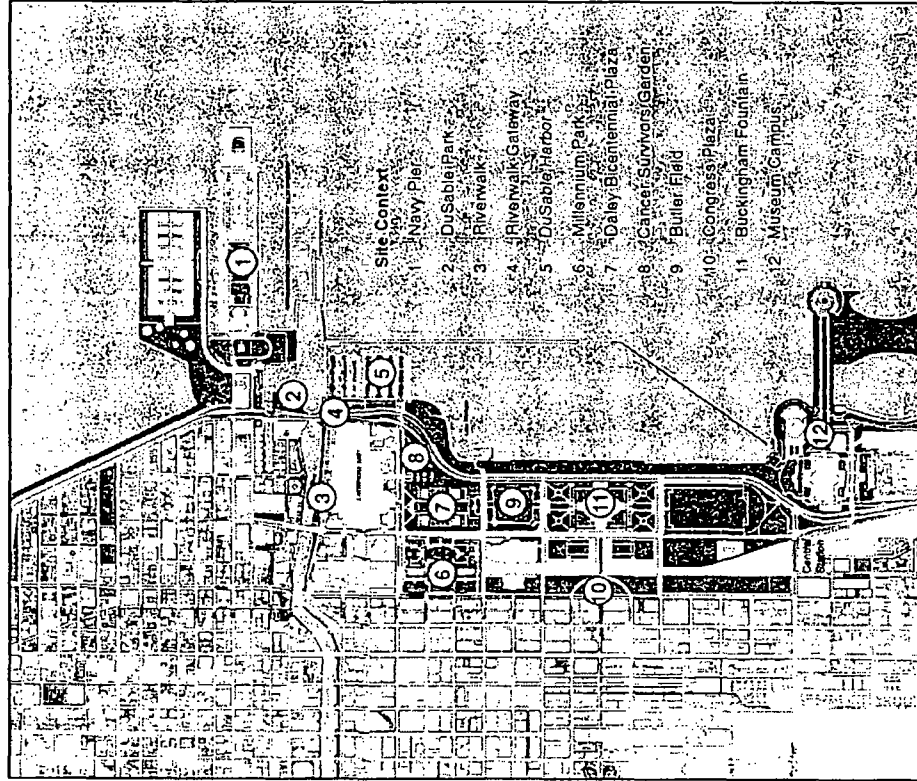


(A) Subarea Identification

2.0 Urban Design Principles

General Intent / Site Context and Location
Urban Framework
Building Height and Massing
Access and Circulation
Open Space
View Corridors





Site Context and Location

The Lakeshore East site is one of the largest remaining development parcels in the center of Chicago. Located at the mouth of the Chicago River at Lake Michigan, the site offers tremendous opportunities for new development and open space. The remarkable assets of this site include:

- Proximity to the Loop, the East Loop, the Magnificent Mile, Navy Pier, DuSable Harbor, Grant Park, the Lakefront and the Chicago River
- Spectacular views of the city and the waterfront
- Diverse uses on surrounding blocks, including offices, hotels, residential
- Proximity to transit including the METRA, CTA, Rail and the new busway to McCormick Place
- Proximity to Grant Park Parking Garages

To respond to the unique context of the site, a set of urban design principles have been developed that address the following issues

- *The Urban Framework*
- *Building height and massing*
- *Access and Circulation*
- *Open Space*
- *View Corridors*

General Intent / Site Context and Location

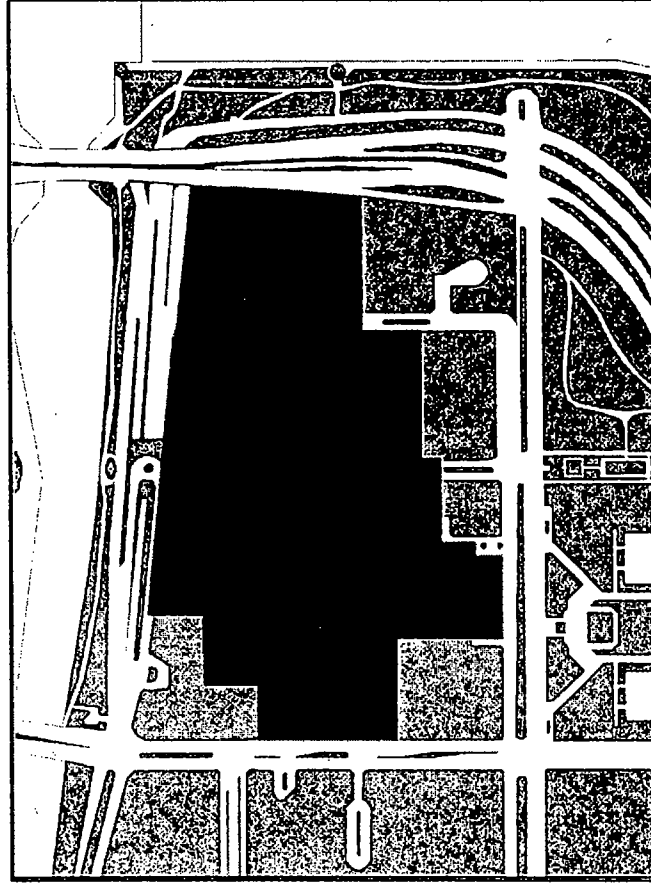
The Master Plan sets forth the basic principles and controls needed to guide the development of the +/-28 acres Lakeshore East development. The Master Plan Standards will guide the development of Lakeshore East as it becomes a vibrant urban district supporting 9.7 million square feet of new residential, hotel, office and mixed use development.

It is also the intent of the Master Plan to enhance the urban fabric and character of Chicago's central area, while also achieving the goals of the City of Chicago and the developer criteria set forth for the Lakeshore East area by a 1993 Planned Development amendment.

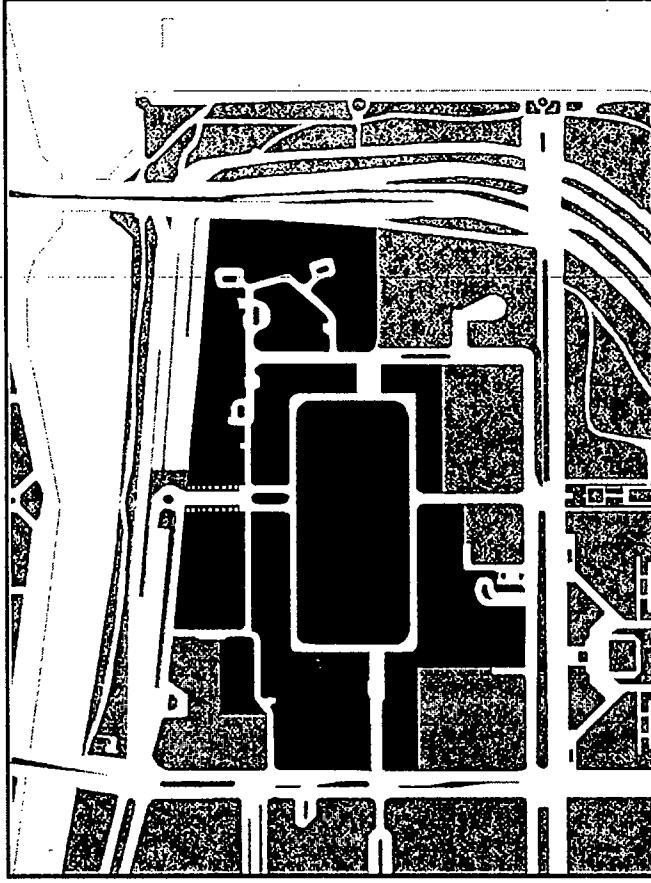
The Master plan offers a development framework intended to ensure that as Lakeshore East evolves, it will provide a high quality of life for its residents and neighbors alike. The pattern of development blocks, streets and open spaces will connect Lakeshore East and adjacent developments and amenities. This framework will also strengthen the areas adjacent to the site, offering access to Lakeshore East's amenities and connections through this new district to other neighborhoods, the Chicago River and the lakefront

Urban Framework

- Establish urban blocks which are similar in size to typical downtown Chicago blocks
- Create urban streets in character with those of a residential neighborhood
- Create addressing opportunities for multiple buildings per block



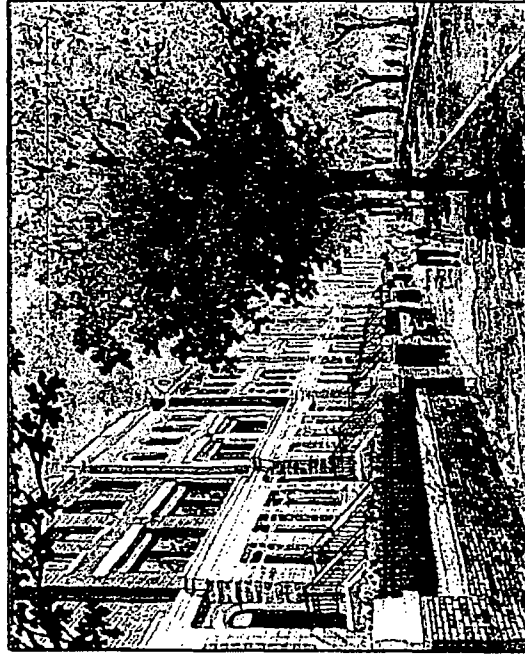
Subarea E
A single, contiguous development site.



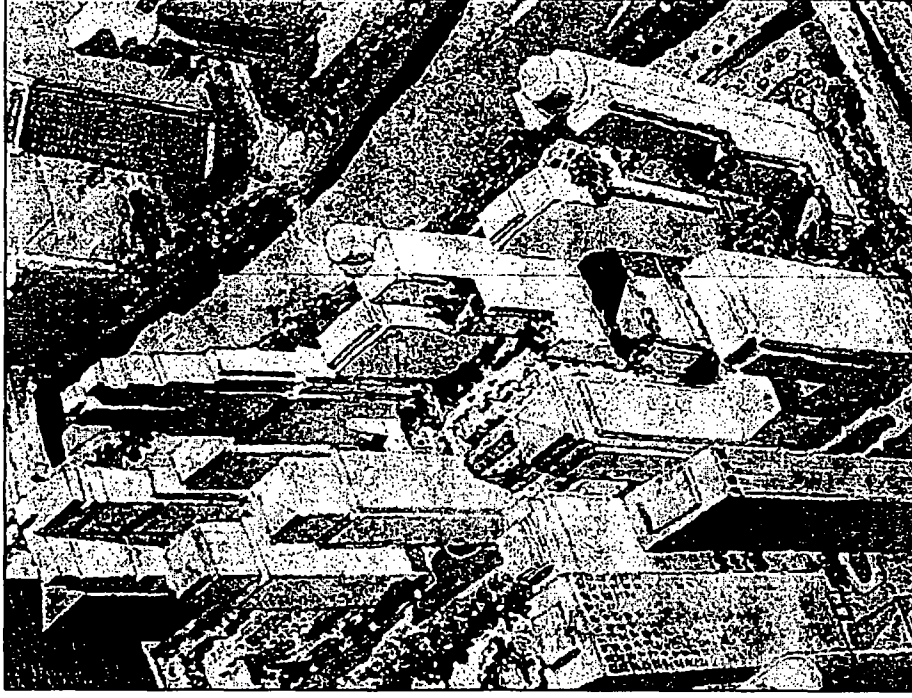
Proposed development plan
Establish urban-scaled blocks and extend the local street system into the development
to avoid the potential perception of a 'super-block'

Building Height and Massing

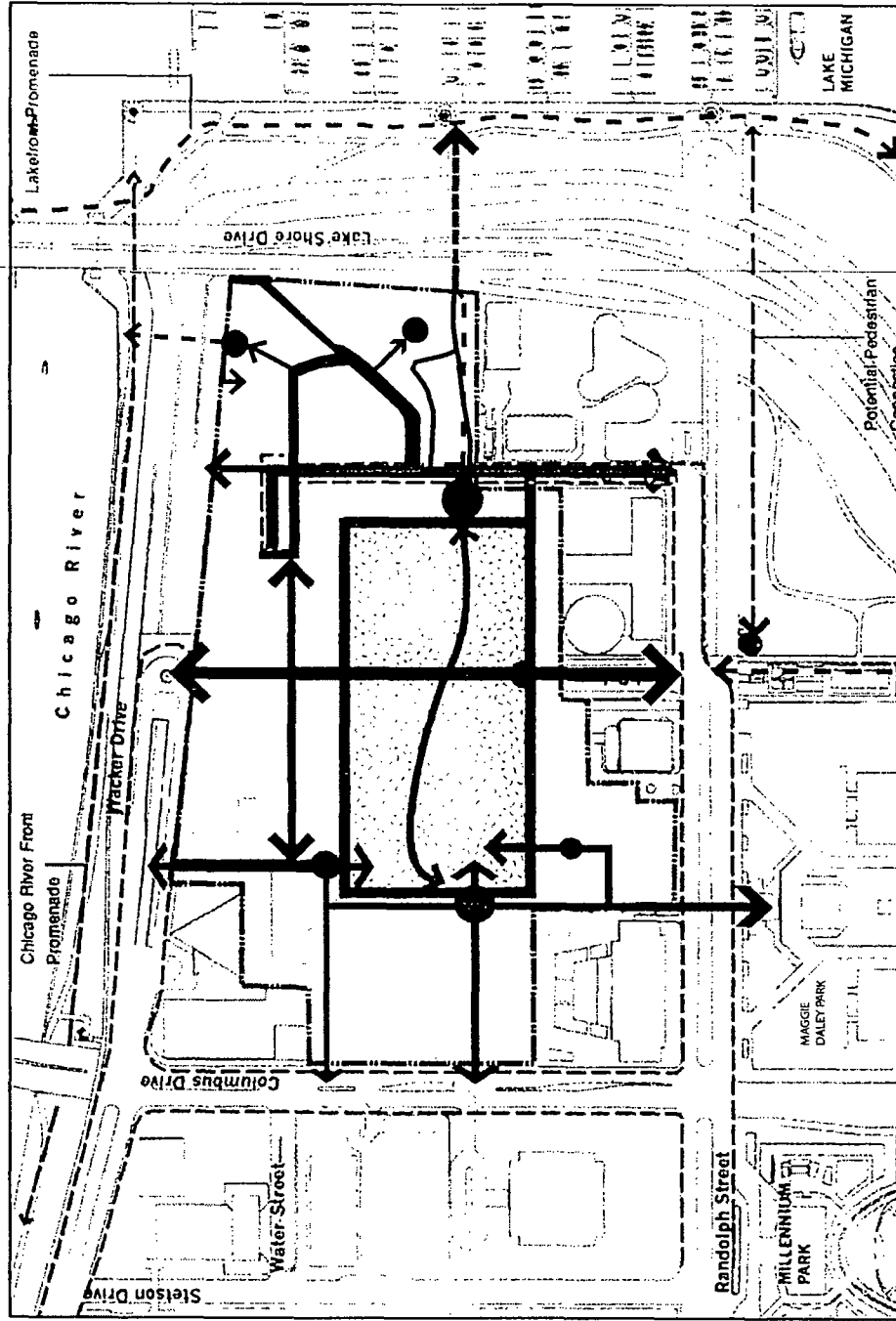
- Place tall buildings with a sensitivity towards existing residential towers
- Frame the park with low-rise development and townhouses which can screen all existing and future parking podiums from view
- Place the tallest buildings along Coumbus Drive and at key prominent locations
- Strengthen the character of the urban street wall, or facades, on Wacker Drive along the Chicago River
- "Stagger" high-rise buildings to maximize views throughout the future development



Low-rise townhouses can screen parking podiums from the street



Building massing and height should be sensitive to existing towers, street walls and view corridors



Maximize connectivity to surrounding development and create a permeable neighborhood

NOTE: These plans are diagrammatic in nature and are only intended to show approximate dimensions and locations.

Access & Circulation

- Maximize connection to surrounding development
- Create a permeable neighborhood
- Weave the different levels together
- Strengthen pedestrian connections to the river and lakefront
- Provide convenient parking, drop-off, and service access without negatively impacting the existing streets

Diagram Legend:

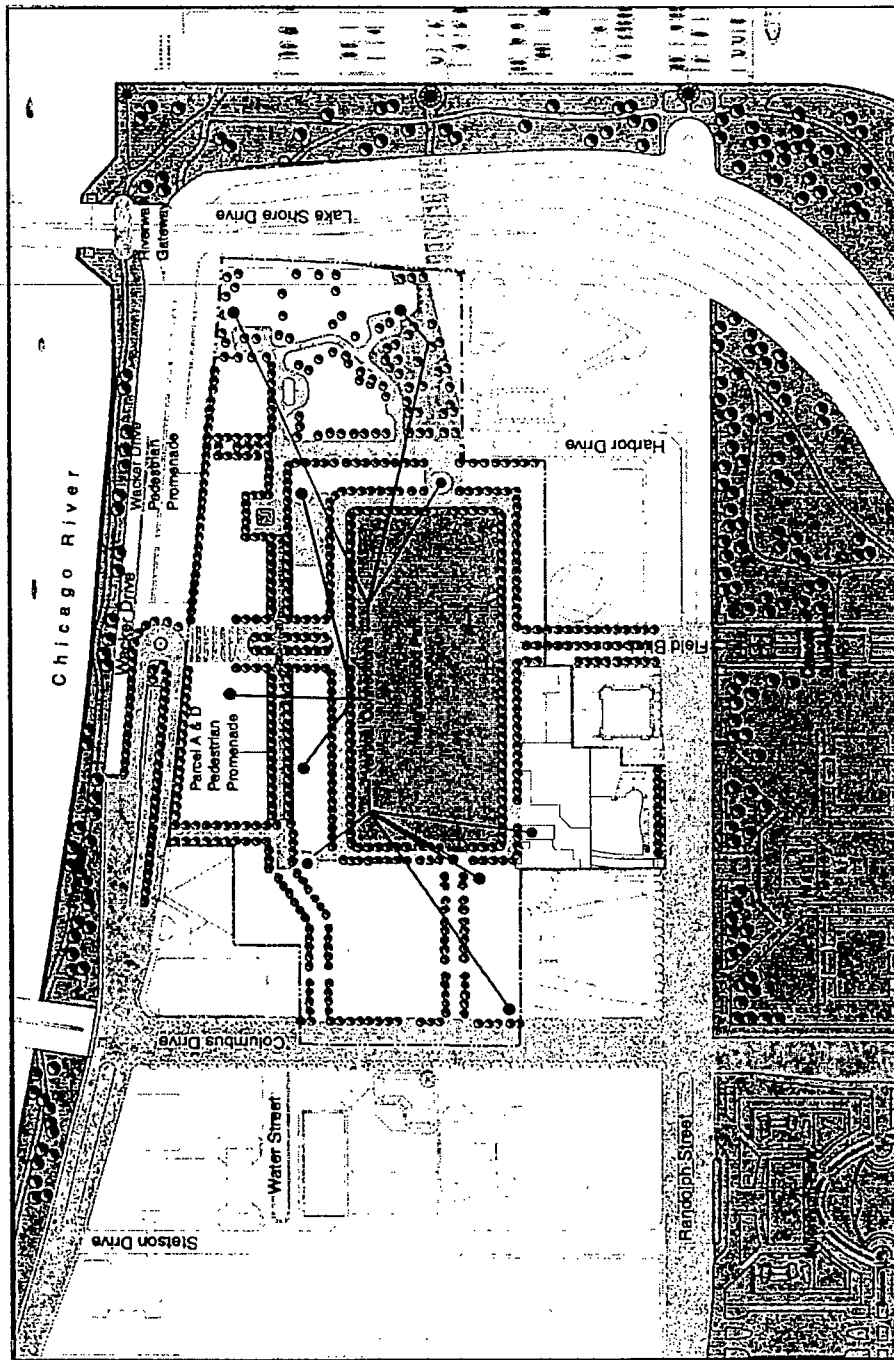
The Diagram to the right represents a hierarchy of connectivity. The large arrows represent the major or primary access corridors of the site. They represent both pedestrian or vehicular traffic depending on their location. The smaller arrows indicate the secondary corridors of access.

The circles represent the major vertical connections of the site which allows the pedestrian to access the different levels of the development

The dark dotted arrows indicate existing connections along the Riverfront, Lakefront and to Grant Park at the lower level. The light dotted arrows identify existing pedestrian movement along the major streets at the upper level.

Open Space

- Create a large neighborhood park as the focus of Lakeshore East
- Create smaller courtyards between buildings.
- Connect the open spaces in Lakeshore East to Grant Park, the Chicago Riverwalk and the lakefront park system
- Create well landscaped 'green' streets



Develop a new public park, smaller courtyards between buildings and create landscaped green streets.

NOTE: These plans are diagrammatic in nature and are only intended to show approximate dimensions and locations.

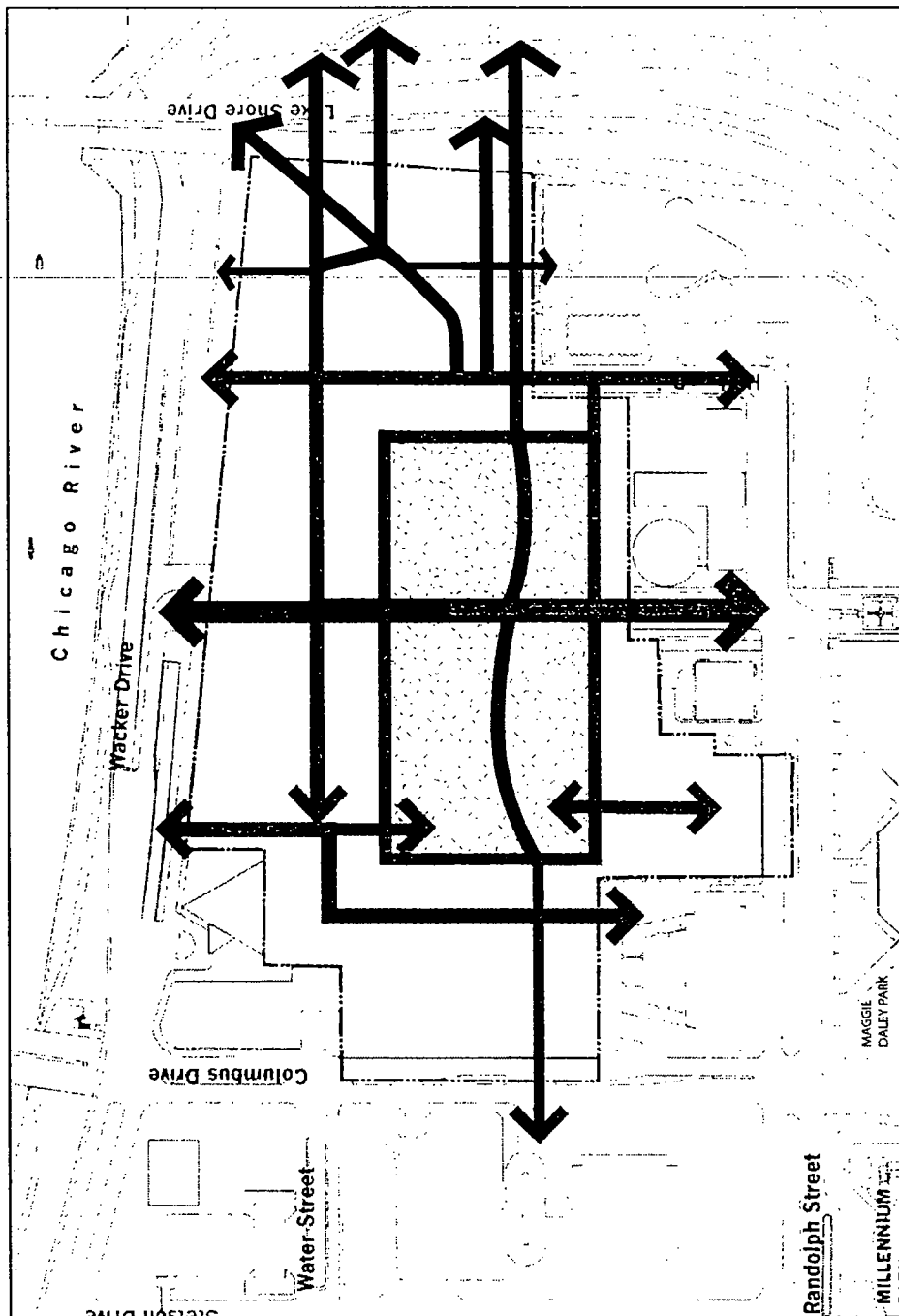
View Corridors

- Ensure a visually permeable neighborhood
- Create east / west and north / south view corridors
- Maximize views from existing residences
- Place tall buildings at the northern edge of Lakeshore East
- Maximize views to the central park
- Create appropriate building separation to allow for views through the development

Diagram Legend:

The large arrows and thicker lines represent the major view corridors of the site

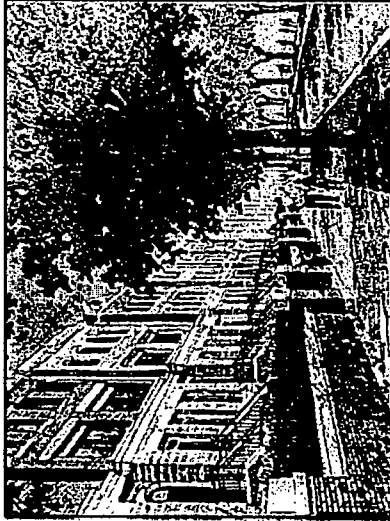
The smaller arrows represent the secondary view corridors of the site.



NOTE: These plans are diagrammatic in nature and are only intended to show approximate dimensions and locations.

3.0 Development Summary

Development Framework
Bulk Regulations and Data Table
Proposed Parcelization Plan - Upper Level Plan
Proposed Parcelization Plan - Intermediate Level
Proposed Parcelization Plan - Lower Level
Generalized Land Use Plan
Utility Easements
Easements





Introduction

The master plan for Lakeshore East establishes a framework of 7 development parcels. These development parcels are defined by a network of addressing streets at various levels. Parcel dimensions have been established that will allow for flexibility in accepting a variety of land uses and building types.

The intent of the master plan is to develop a new, vibrant urban district that promotes a collective image for Lakeshore East. This may be achieved through a consensus that is based on the following principles:

Land Use

- New development should be compatible with neighboring uses.
- Locate Office uses at the western portion of the site, adjacent to existing office buildings. office could locate along Wacker Drive limited by the location of the upper level roadway of Wacker Drive
- Locate residential uses adjacent to and within views of the neighborhood park, Chicago River, and / or Lake Michigan
- Provide a future school site within the park

Parcelization

- Ensure the creation of an urban framework of streets and blocks
- Allow for future development flexibility

Development Plan

- Locate taller buildings with a sensitivity towards existing view corridors from neighboring buildings
- Frame the neighborhood park with townhomes or low-rise development with taller buildings beyond.

Parking

- Maximize on-street parking
- Screen all structured parking from view
- Develop below grade parking lined with townhomes which front onto the park

Final for Publication

Residential - Business Planned Development Number 70

Amended Planned Development Use and Bulk Regulations and Data

October 16, 2018

Subarea	Net Development Area (1)		Max Net F.A.R.	Building Area		Residential Dwelling Units		Office (s.f.)		Hotel Rooms (# of Keys)		Retail Sales/Service Area	
	sf	acres		Existing/Under Construction	Future Allowed	Existing/Under Construction	Future Allowed	Existing/Under Construction	Future Allowed	Existing/Under Construction	Future Allowed	Existing/Under Construction	Future Allowed
Subarea A													
One Illinois Center	66,104	1.52											
Hyatt West	52,256	1.20											
Two Illinois Center	80,085	1.84											
Hyatt East	73,000	1.68											
Columbus Plaza	38,154	0.88											
Three Illinois Center	52,560	1.21											
Subtotal	362,159	8.33	12.20	4,419,463	-	552	-	1,762,906	-	2,041	-	153,958	-
Subarea B													
ADN Building (Amoco)	136,819	3.14	19.72	2,698,308				2,698,308				84,000	
Subarea C													
Outer Drive East	88,463	2.03											
Harbor Point	79,950	1.84											
Subtotal	168,413	3.87	11.90	2,004,225	-	1,682	-	-	-	-	-	42,668	-
Subarea D													
Buckingham	34,825	0.80											
175 Harbor Dr.	50,249	1.15											
Park Shore	41,672	0.96											
Swishotel	34,000	0.78											
Fairmont	43,169	0.99											
Athletic Club	21,943	0.50											
BTE	44,091	1.01			900,671				900,671				
BTN	63,660	1.46											
BTS	53,778	1.23											
Subtotal	387,387	8.88	17.06	5,707,385	900,671	1,380	-	2,699,000	900,671	1,337	-	221,369	-
Subarea E													
Public School	31,583	0.73			53,000								
Private Development	825,455 *	18.95		7,126,103	2,573,897	4,029 (4)	3,167 (4)	286,567 (4)	76,159 (4)	544 (4)	597 (4)	185,162	587,843
Subtotal	857,038	19.67	11.38	7,126,103	2,626,897 (3)	4,029 (4)	3,167 (4)	286,567 (4)	76,159 (4)	544 (4)	597 (4)	185,162	587,843
Subarea F													
BCBS	100,905	2.32		1,813,819				1,813,819					
Subarea G													
Park Millennium	40,641	0.93		582,903		480						50,000	
SUBTOTAL	2,053,362 *	47.14 *	11.86	24,352,206	3,527,568	8,133	3,167	9,260,600	976,830	3,922	597	737,157	587,843
ORDINANCE MAXIMUMS (existing and future, prior to conversion):				Building Area		Residential Dwelling Units		Office (s.f.)		Hotel Rooms (# of Keys)		Retail Sales/Service Area	
				27,880,245		9,050		12,000,000		5,500		1,335,000	

(1) Net developable areas for individual parcel components (A, B, C, or D) are taken from original survey as prepared by Dale Weaver

(2) Areas outside of Subarea E provided by the City of Chicago & I E Consultants, Surveyors

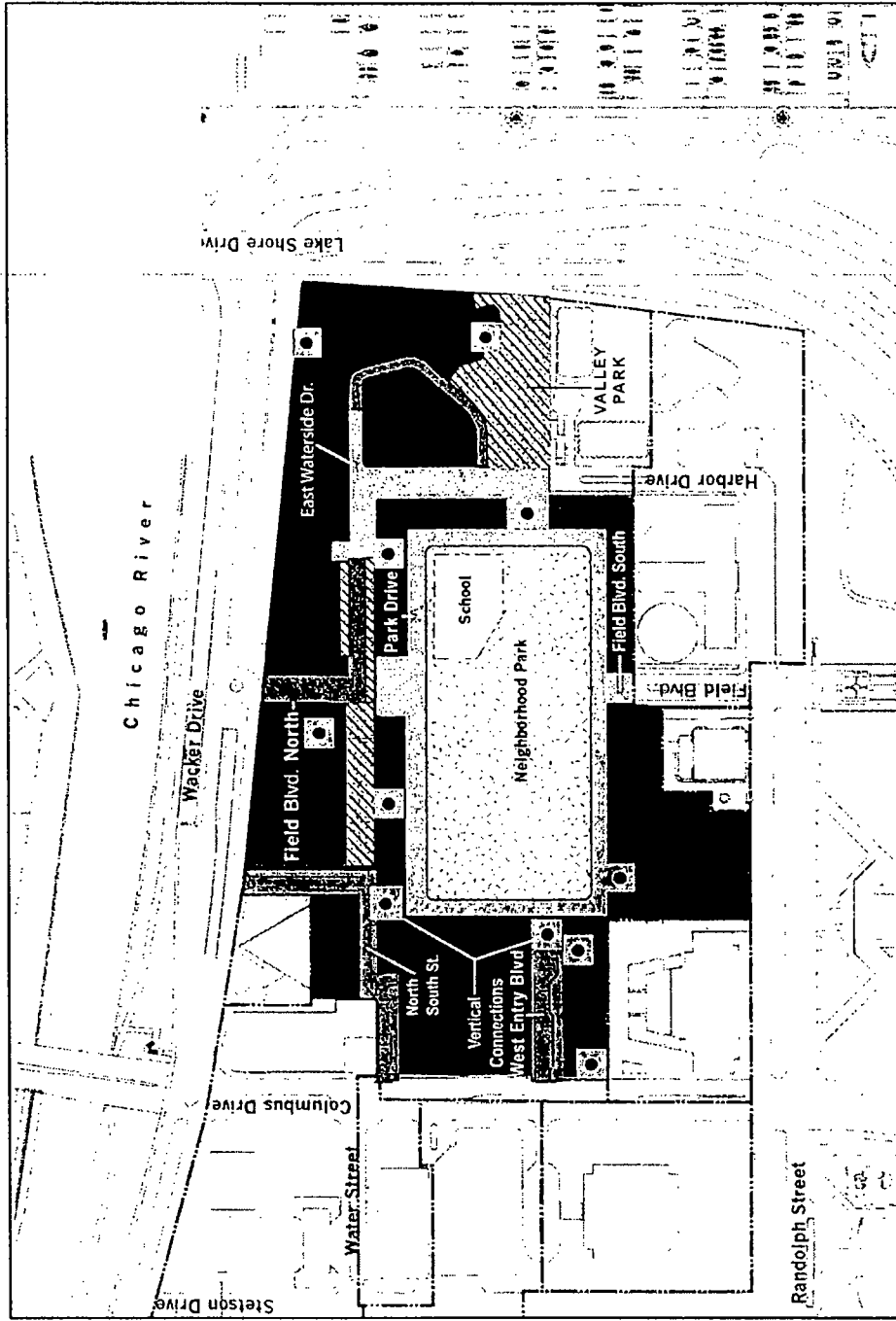
(3) Future Allowed figures are subject to potential modifications/transfers provided for in PD 70 and include I, J, K, L, Building O and other future development in Subarea E.

(4) Future Allowed figures have been adjusted to reflect an allowed PD conversion of keys and office space to dwelling units (1,000 s.f. office space = 1 dwelling unit and 2 hotel keys = 1 dwelling unit)

981 hotel keys converted

1,760,099 sf office space converted

* Private developable area include public spaces not dedicated. This PD eliminates 10,469 sf of dedicated ROW. Note that 17,898sf of public streets not dedicated has been added to the program (included in the Net Developable Area)



-  Public Open Space
-  R.O.W.
-  Net Developable Area (Upper Level)
-  Vertical Connection
-  Public Street Not Dedicated
-  Non-Dedicated Public Space

NOTE: These plans are diagrammatic in nature and are only intended to show approximate dimensions and locations.
 ** Does not include Parkway portion of Ring Road in R.O.W.

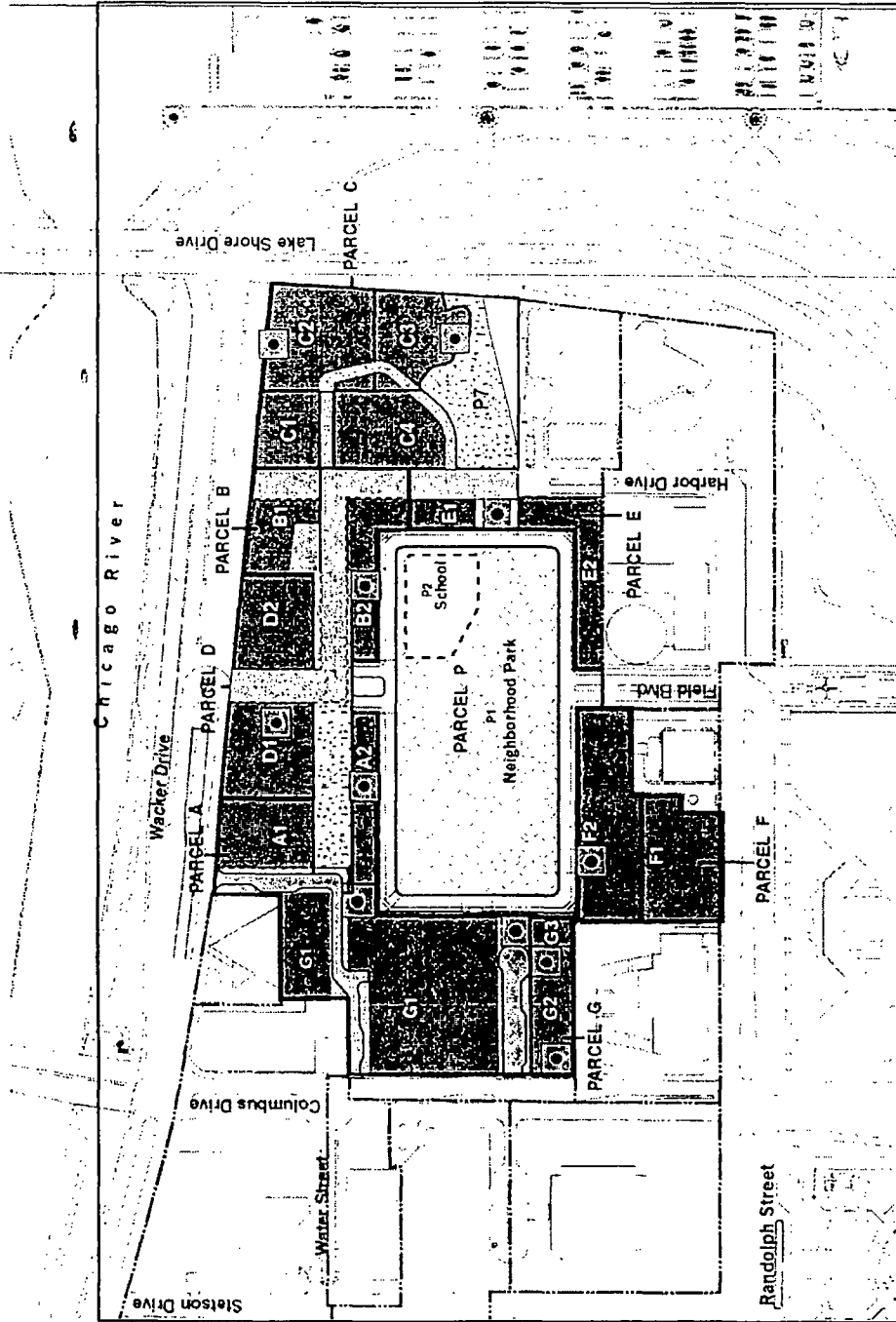


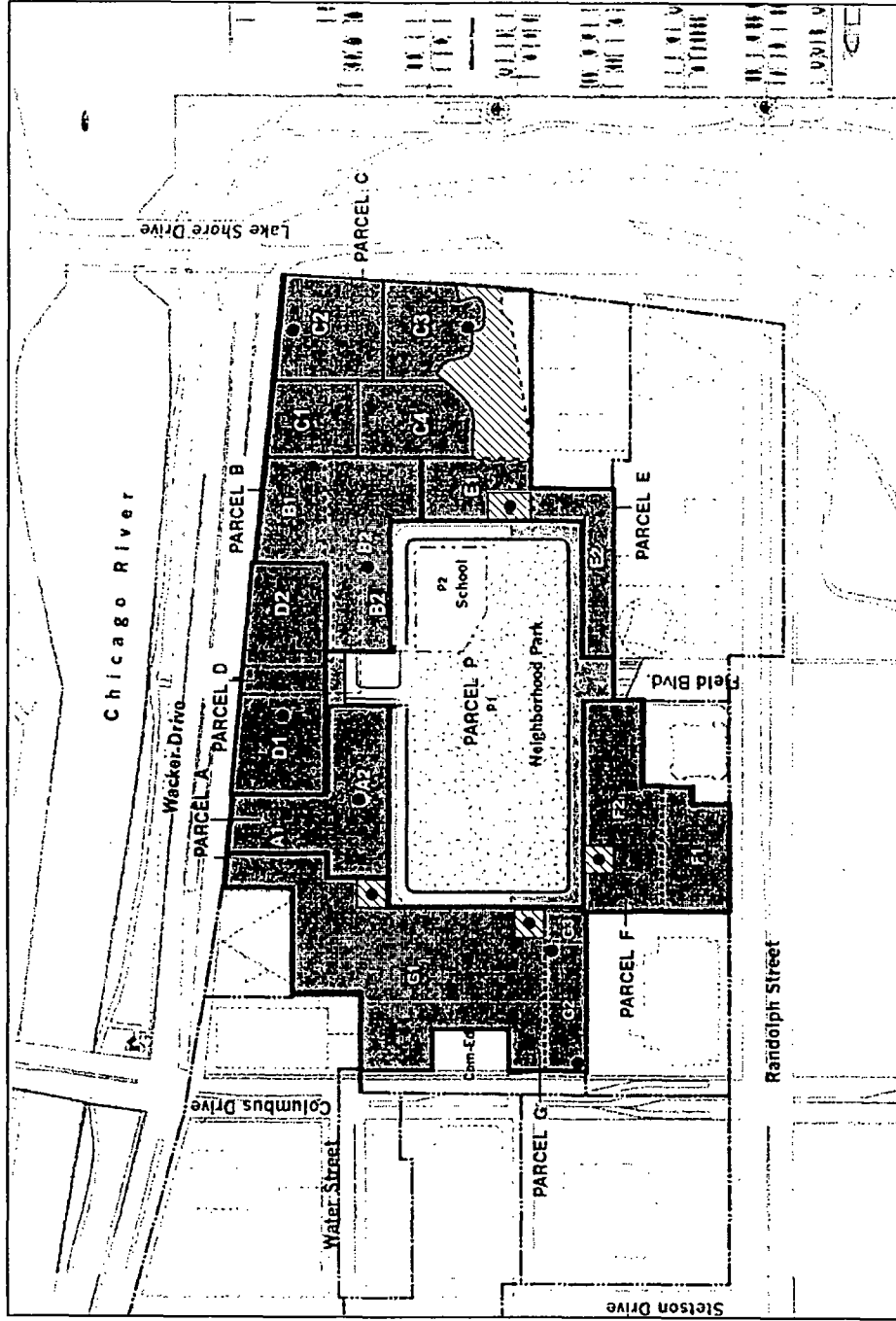
October, 2018

Subarea E Parcelization Plan - Upper Level
(+/-) elev. +47.00 to +58.68 CCD

Subarea E Parcelization Plan - Upper Level

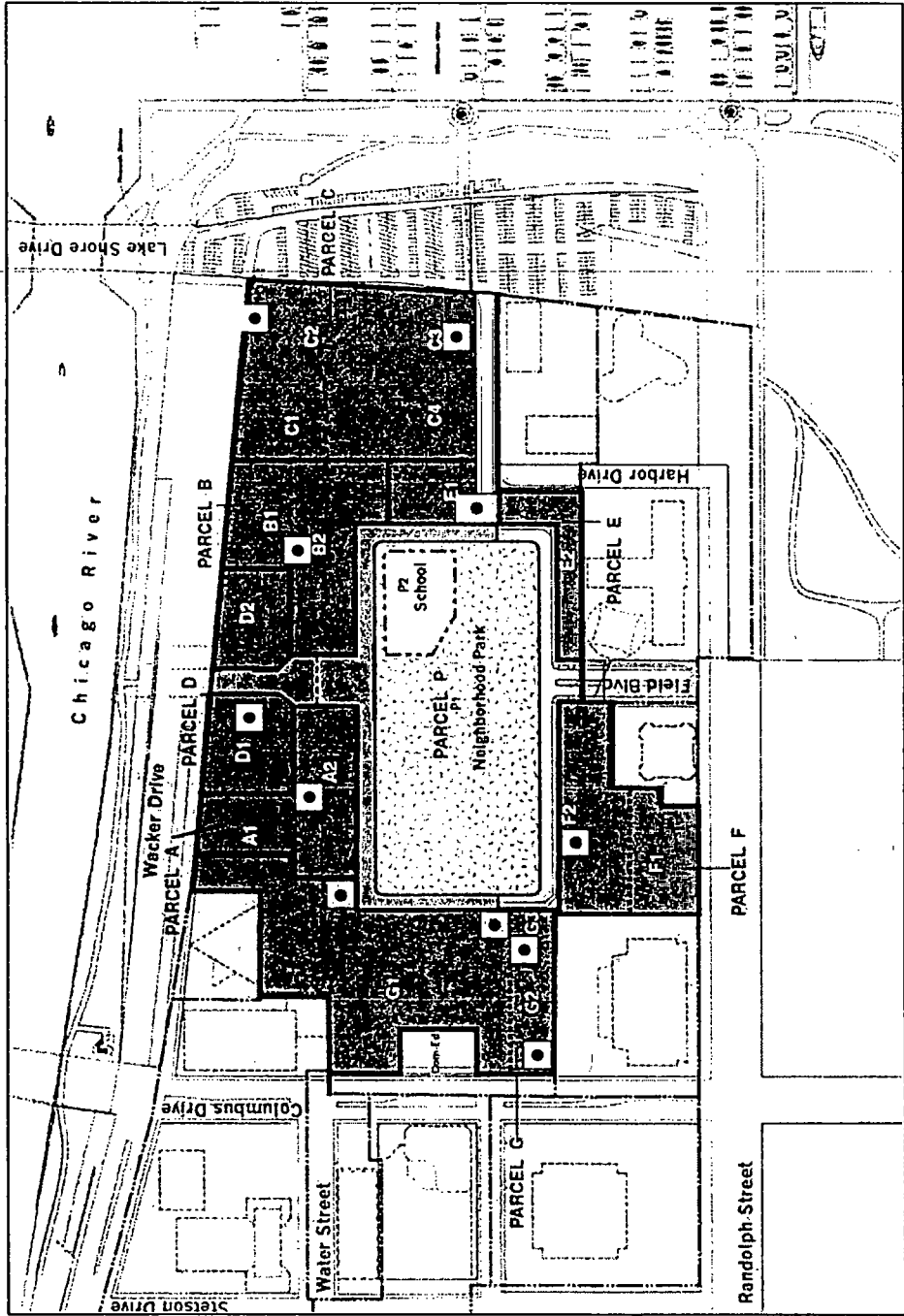
- Adjustments to the final location of parcel boundaries may occur as specific building designs are submitted for further review by the city.
- Sub-parcels may be combined into single parcels, or new sub-parcels may be created; however, the maximum building envelopes must be respected.
- All parcel or sub-parcel modifications are subject to administrative review and approval by the Department of Planning and Development.





- Public Open Space
- R.O.W.
- Development Parcel
- Non-Dedicated Public Space
- Vertical Connection

NOTE: These plans are diagrammatic in nature and are only intended to show approximate dimensions and locations.



Public Open Space

R.O.W.

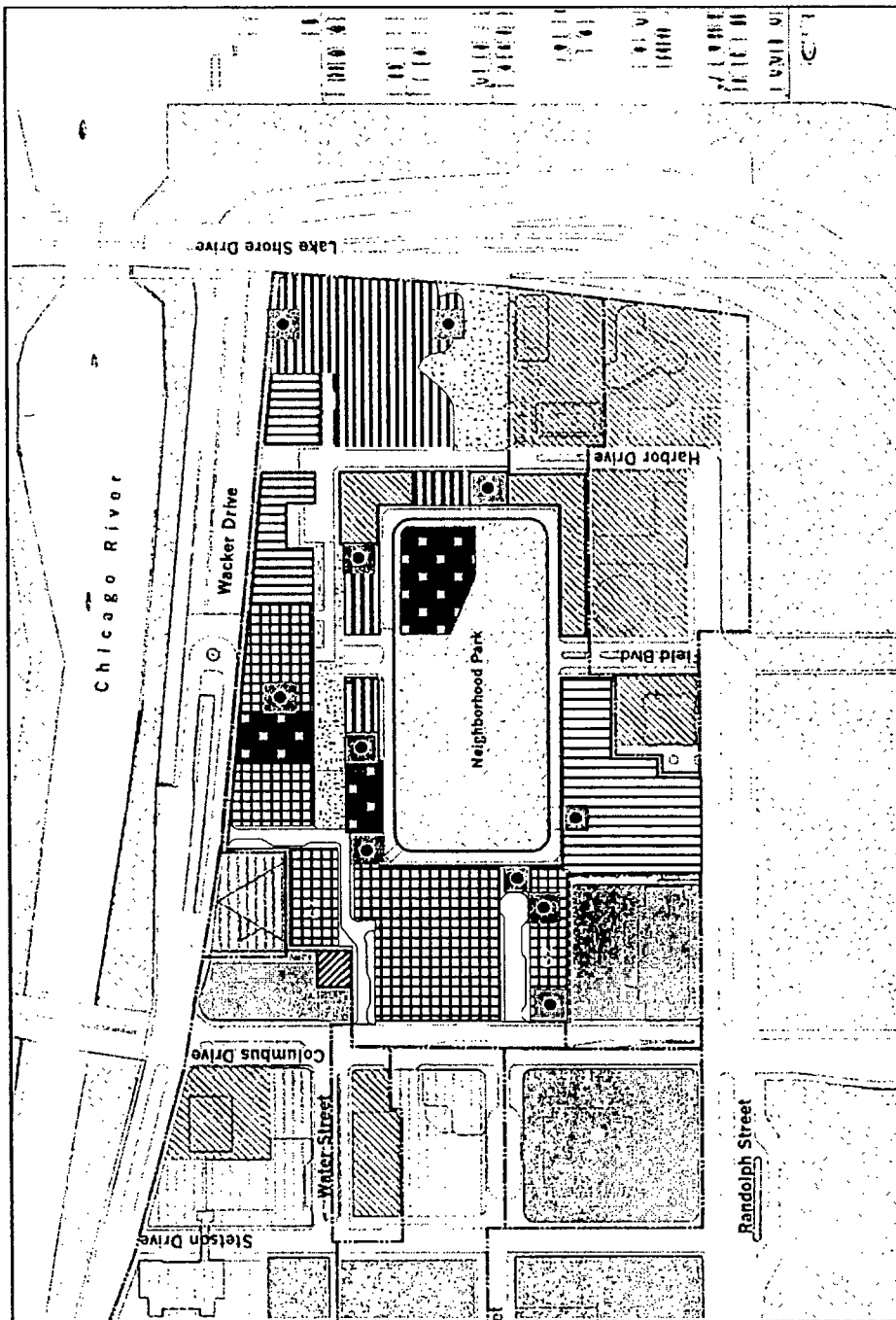
Development Parcel

Vertical Connection

NOTE: These plans are diagrammatic in nature and are only intended to show approximate dimensions and locations.

Land Use Plan

- Future Land uses shall be compatible with existing adjacent land uses



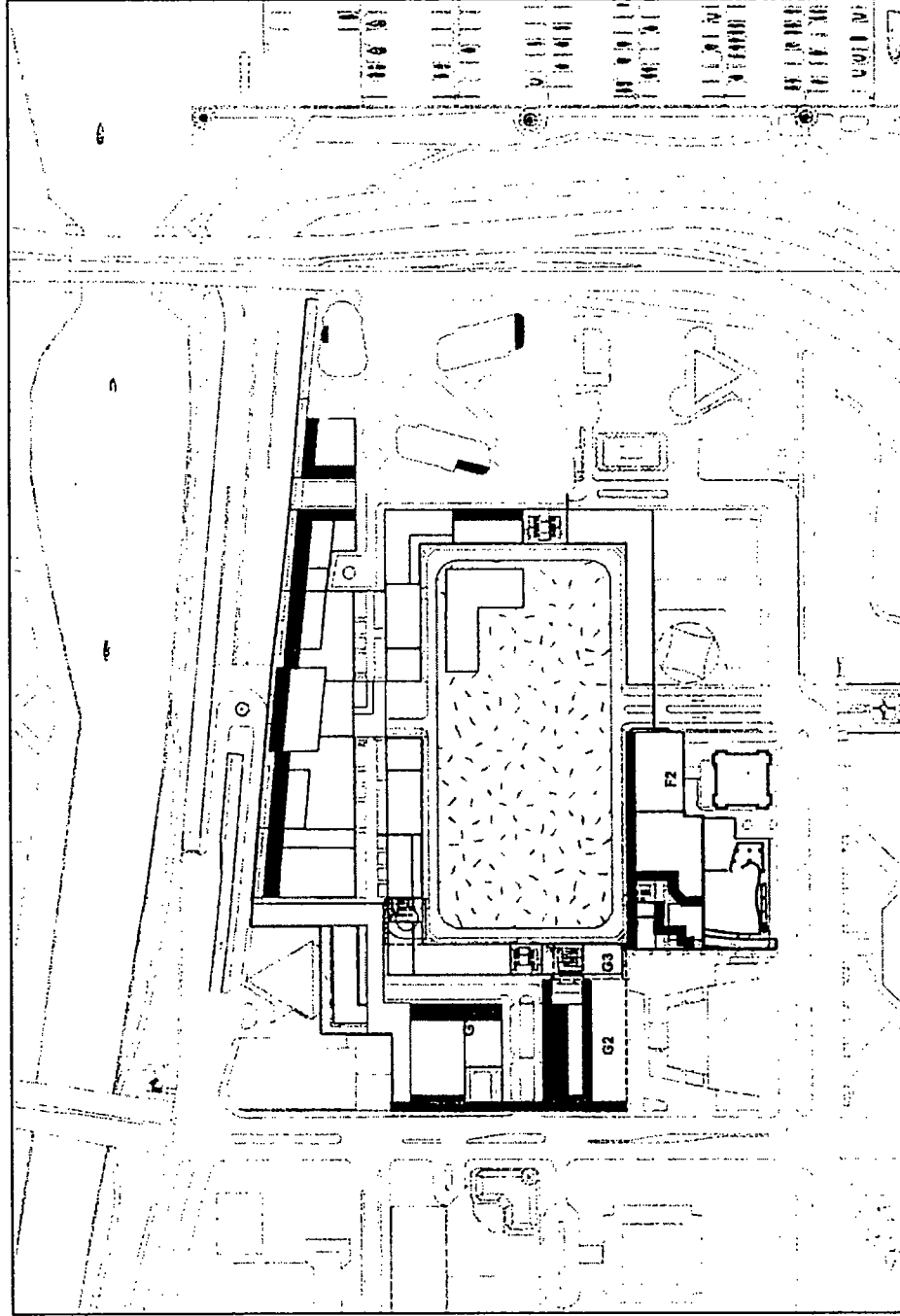
* per minor change letter dated 10/24/01, District Electrical Facilities will also be considered as an allowable use of developable land.

NOTE: These plans are diagrammatic in nature and are only intended to show approximate dimensions and locations.

October, 2018

Proposed Retail Frontage - Upper Level

- Non-residential or mixed-use buildings should maximize retail uses at the ground floor addressing the street or pedestrian access.
- For residential buildings, neighborhood-serving retail at the ground floor is strongly encouraged.
- For buildings along Upper Wacker Drive and Harbor Drive, neighborhood serving retail at the ground floor is strongly encouraged.

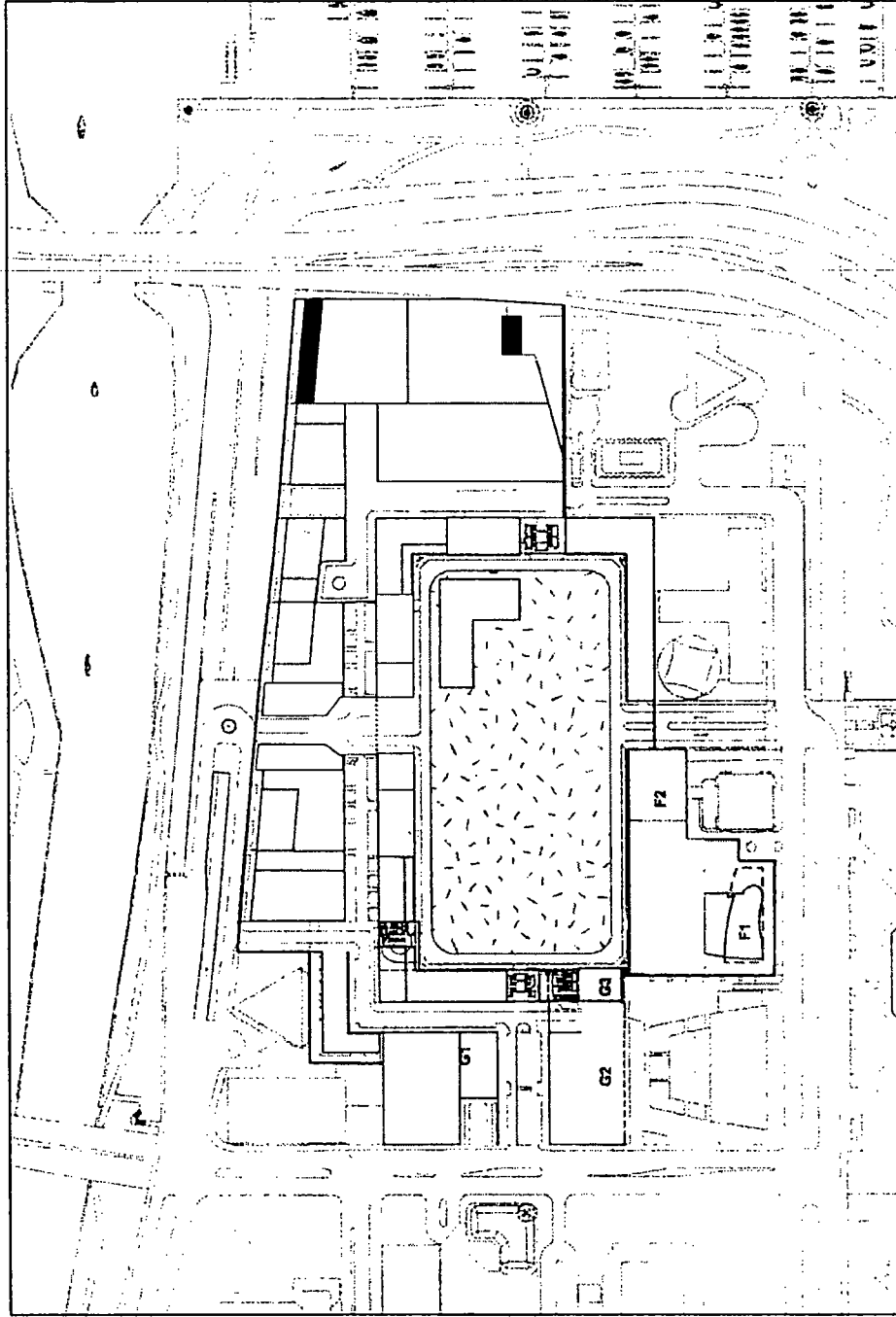


Note: These plans are diagrammatic in nature and are only intended to show suggested, not required, retail locations.



Proposed Retail Frontage - Lower Level

- For the taller buildings along the Park Drive, neighborhood serving retail at the ground floor is strongly encouraged.

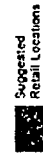
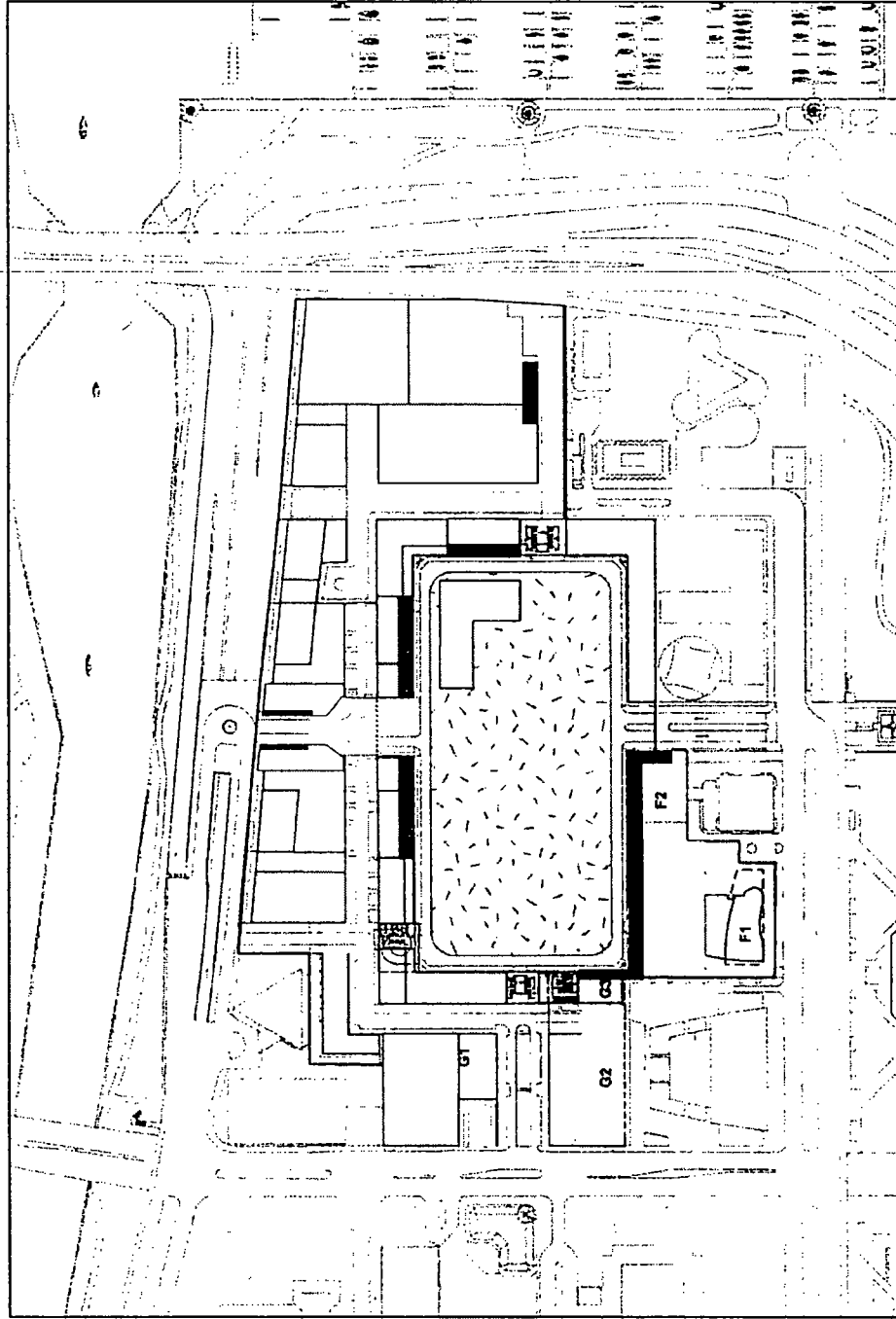


Suggested
Retail Locations

Note: These plans are diagrammatic in nature and are only intended to show suggested, not required, retail locations.

Proposed Retail Frontage - Lower Level

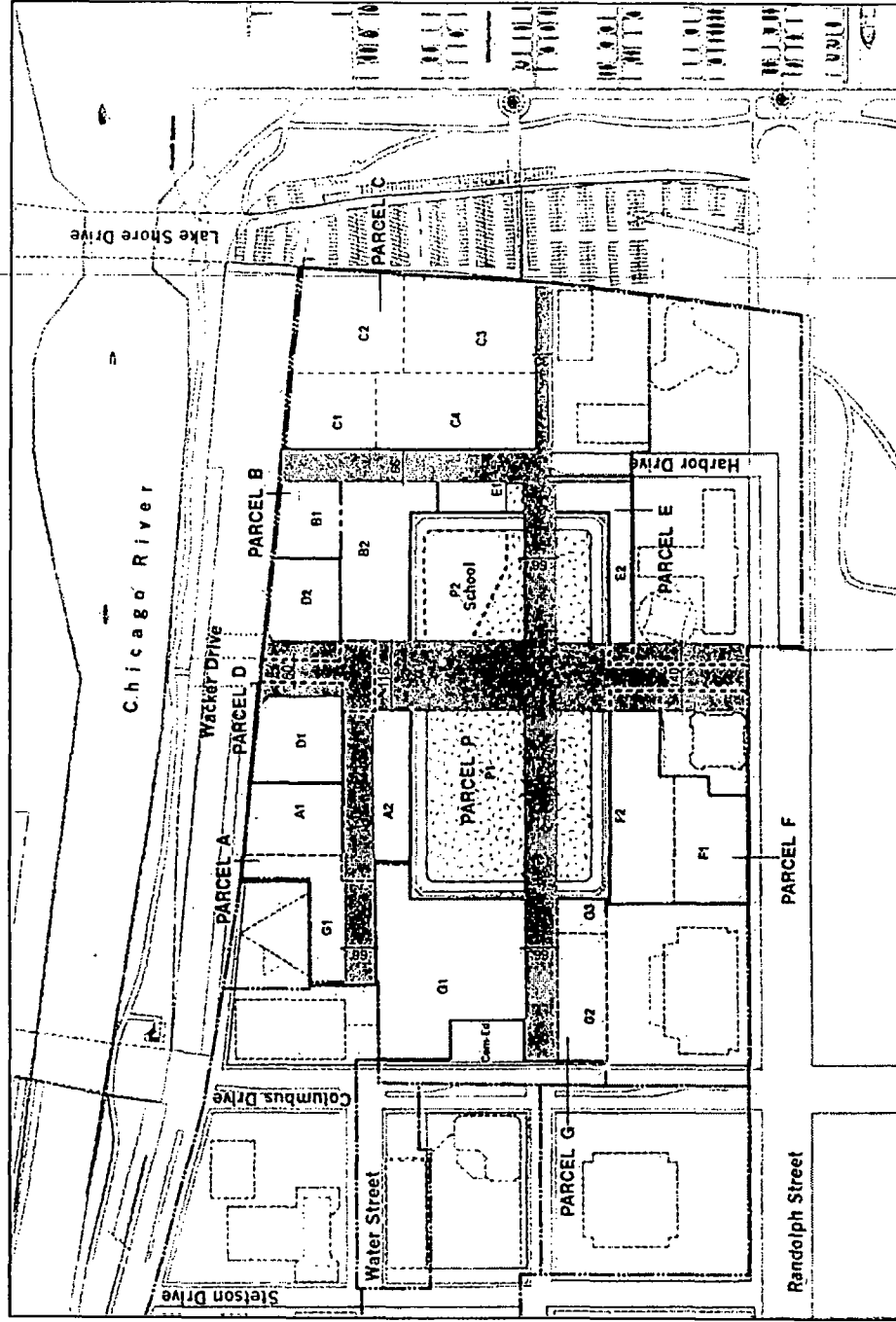
- For the taller buildings along the Park Drive, neighborhood serving retail at the ground floor is strongly encouraged.



Note: These plans are diagrammatic in nature and are only intended to show suggested, not required, retail locations.

Existing Utility Easements

- Some existing utilities and utility easements will be required to be relocated and / or vacated subject to administrative review and approval of the Department of Planning and Development.



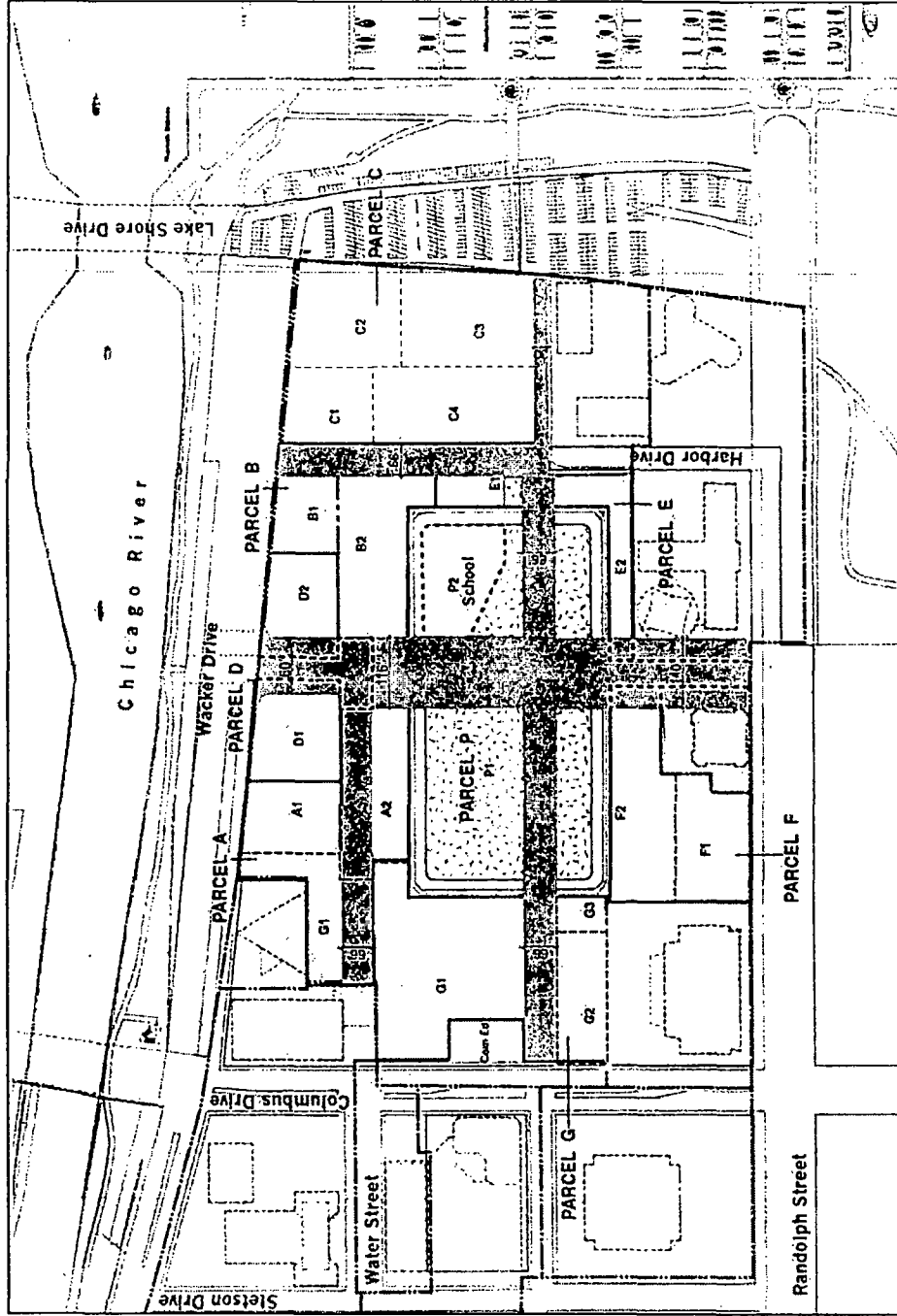
Utility Easements

NOTE: These plans are diagrammatic in nature and are only intended to show approximate dimensions and locations.

October, 2018

New Utility Easements

New Utility Easements

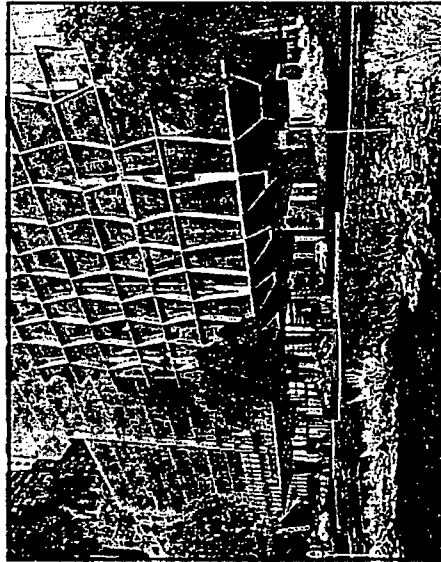


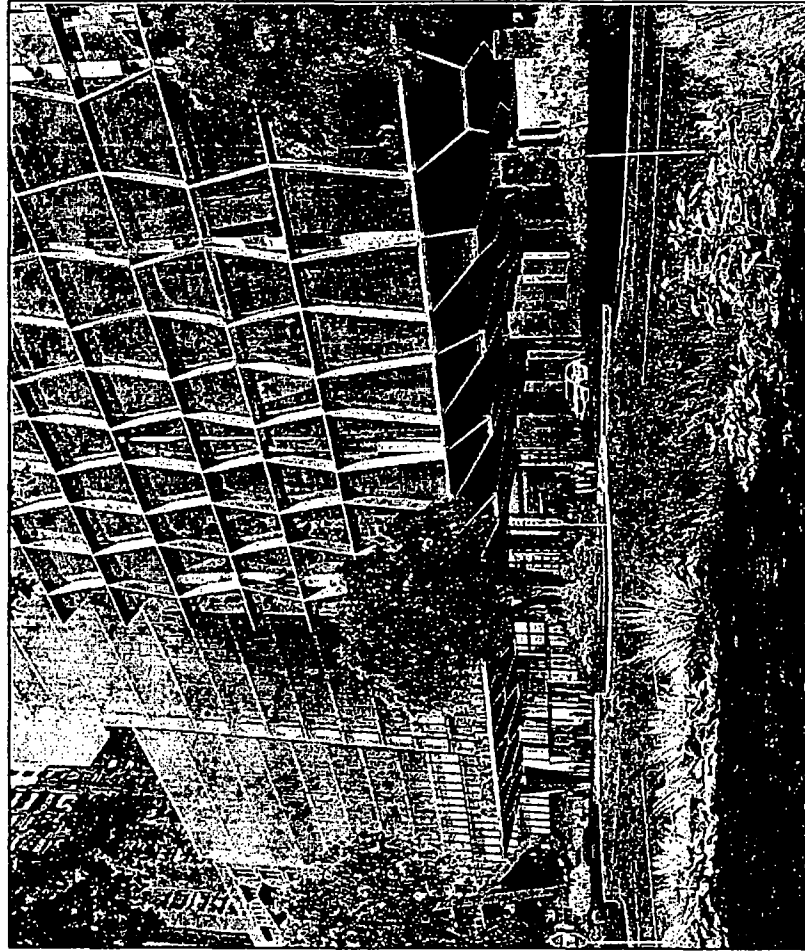
New Utility Easements
Common Easement Area

NOTE: These plans are diagrammatic in nature and are only intended to show approximate dimensions and locations.

4.0 Access and Circulation

Vehicular Access and Rights-of-Way
Automobile Entrance Zones / Parking Access
Pedestrian Access
Right of Way Criteria





Introduction

The family of streets identified within this master plan establishes an identity and character for this neighborhood. These street types address the complexities of the site and the different levels, while emphasizing the pedestrian character of the development. The framework of streets and sidewalks has been designed to provide separate vehicular and pedestrian paths, while ensuring an active and energetic street environment. Pedestrian safety and easy access between the different levels must also be ensured throughout the plan. Gracious sidewalk widths, ground level activities and distinctive landscape treatments will create a strong pedestrian environment for the neighborhood.

Each of the streets has been designed to serve equally as an open space amenity and a vehicular travel zone. The local streets lead to and link the new public park to each major block. This concept is strengthened by the provision for interior courtyards and other public spaces that directly link to the main pedestrian paths.

The following street types and sections respond to the anticipated levels of vehicular traffic within the neighborhood. They also establish a connected network of intimately-scaled, pedestrian oriented streets. These guidelines address the following principles

Streets

- Create well-scaled, tree-lined streets which strengthen the pedestrian environment
- Extend Harbor Drive north in order to complete the local residential street system
- Ramp Field Drive down from upper Randolph Street and connect it to a new Park Drive ringing the park at the natural grade level
- Provide a new internal street at the upper level to provide multiple access and address points for the western parcels

Parking Access

- Provide convenient access from the upper intermediate, and lower levels
- Locate parking access mainly within interior courts or side streets

Service Access

- Locate service access at the lower level
- Utilize utility easements for service access

Pedestrian Access

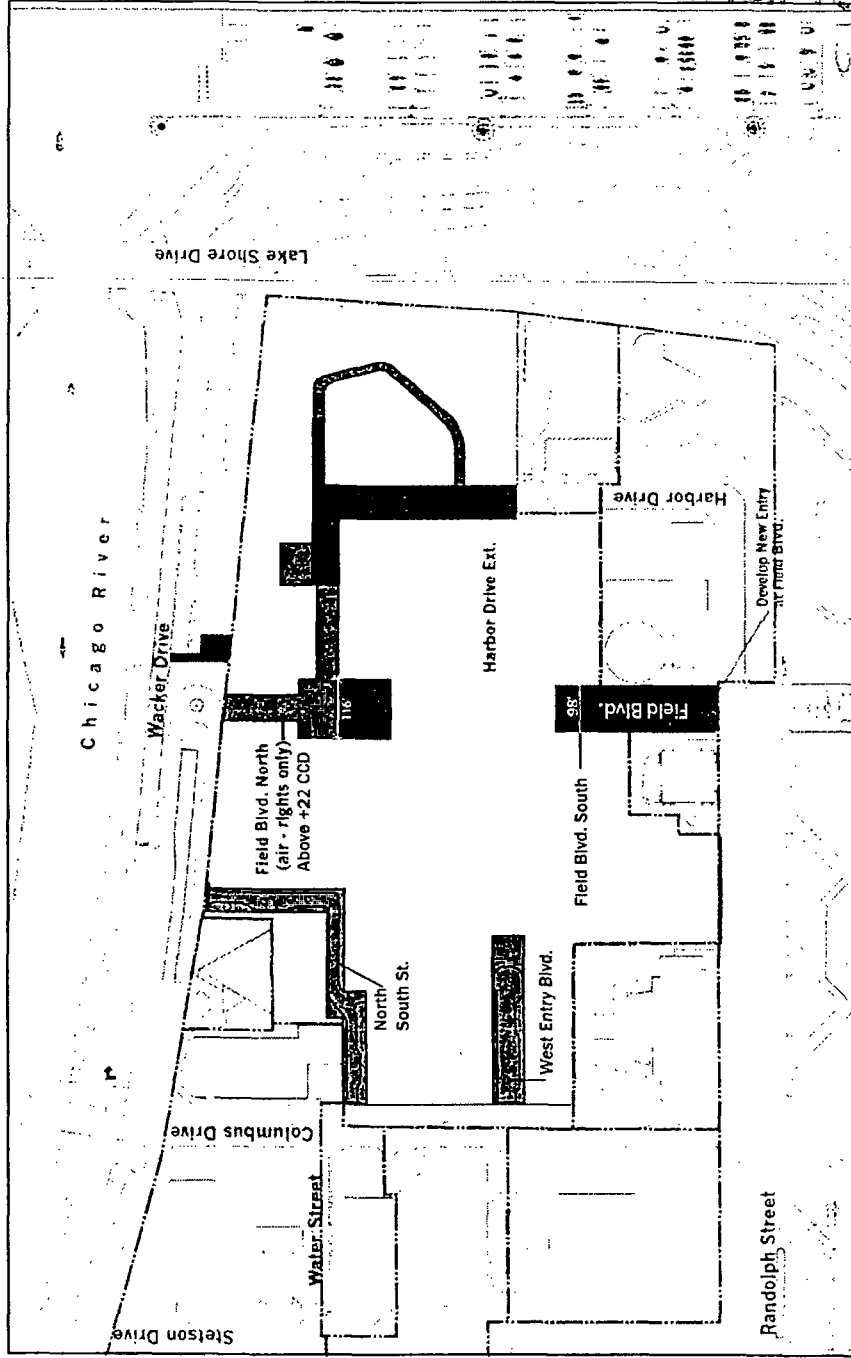
- Provide clear pedestrian links to the Chicago River and Lake Michigan
- Provide convenient access between the upper and lower levels
- Provide handicap access throughout the development
- Extend and connect the Pedway to the park

Emergency Access Easements

- Emergency Access easements will meet all Chicago Fire Department requirements

R.O.W. Adjustments - Upper Level

- The upper level is intended to serve local neighborhood traffic only
- Access to the Park Drive occurs from a connection to Upper Randolph via a sloped Field Boulevard.



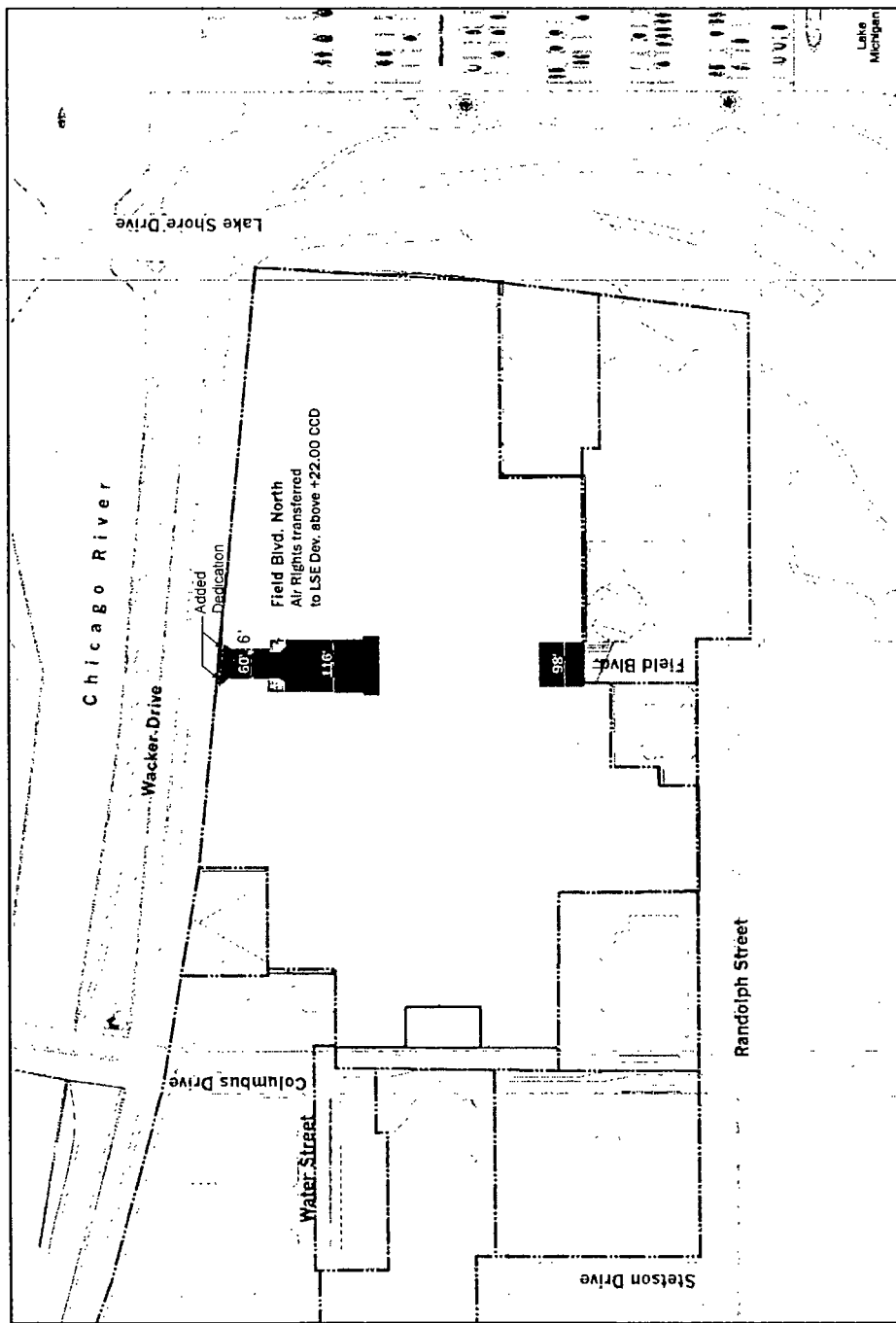
Note: Refer to Street Criteria Drawings for new R.O.W. dimensions & criteria

NOTE:

1. Various Non-Roadway Portions of Upper Level Right of Ways may contain ventilation grates and structures in order to accommodate Lower Level Uses (e.g., parking garages, public, quasi-public and private utilities, etc.)
2. These plans are diagrammatic in nature. The precise location and dimensions of future roadways will be subject to the approval of the city. The vacation and dedications will be accomplished through separate ordinances.

R.O.W. Adjustments - Intermediate Level

- No new roadway connections to Wacker Drive or Randolph St. are proposed at this level, in order to minimize thru-traffic.

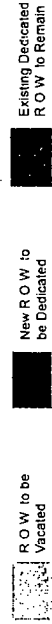
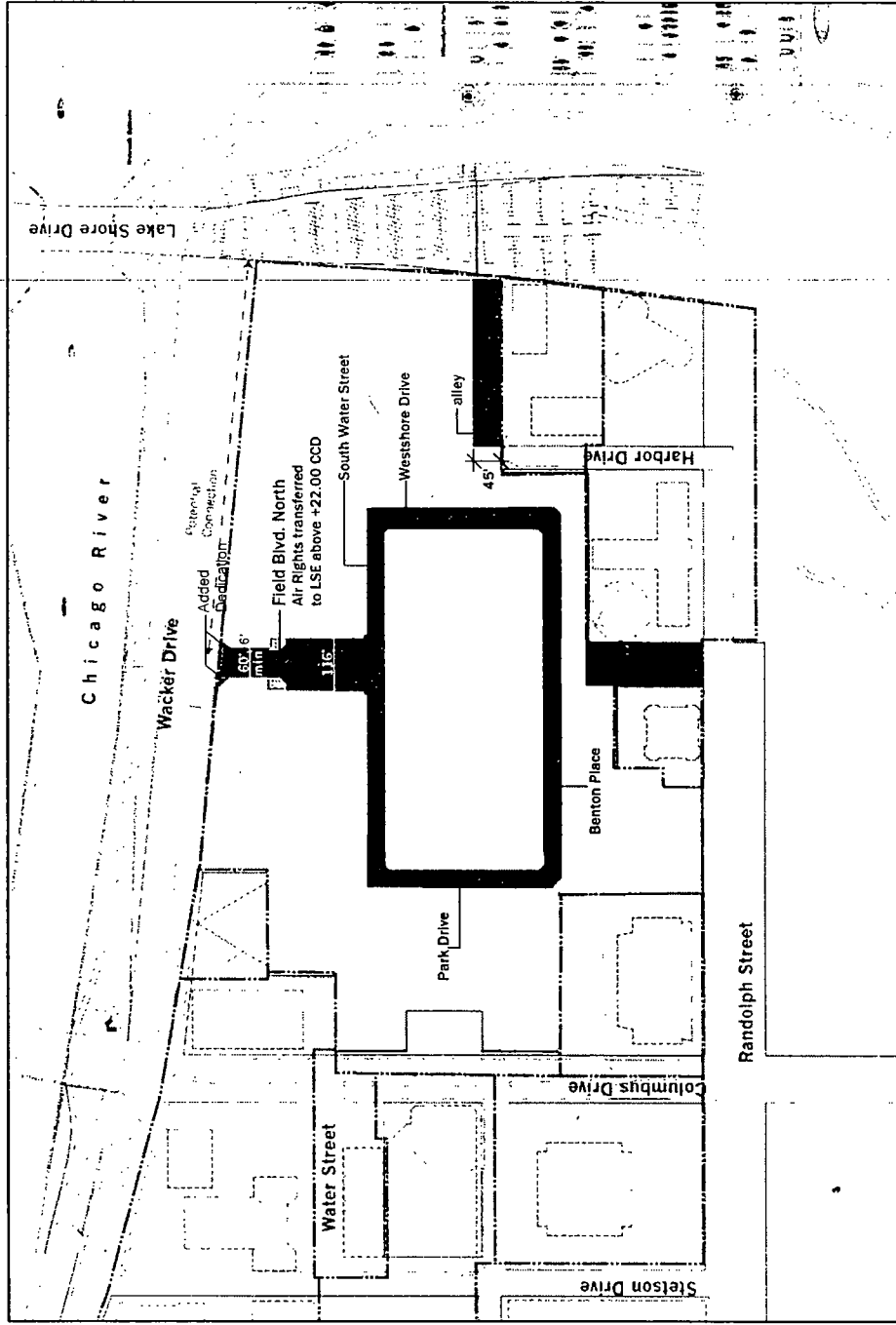


Legend:
 [White Box] R.O.W. to be Vacated
 [Black Box] New R.O.W. to be Dedicated
 [Grey Box] Existing Dedicated R.O.W. to Remain
 Note: Refer to Street Criteria Drawings for new R.O.W. dimensions & criteria

NOTE: These plans are diagrammatic in nature. The precise location and dimensions of future roadways will be subject to the approval of the city. The vacation and dedications will be accomplished through separate ordinances.

R.O.W. Adjustments - Lower Level

- The Park Drive will ring the neighborhood park with one way through traffic at the lower level
- A connection to Lower Wacker Drive via Field Drive North will allow a second means of access / egress to the lower level



Note. Refer to Street Criteria Drawings for new R.O.W. dimensions & criteria

NOTE: These plans are diagrammatic in nature. The precise location and dimensions of future roadways will be subject to the approval of the city. The vacation and dedications will be accomplished through separate ordinances.

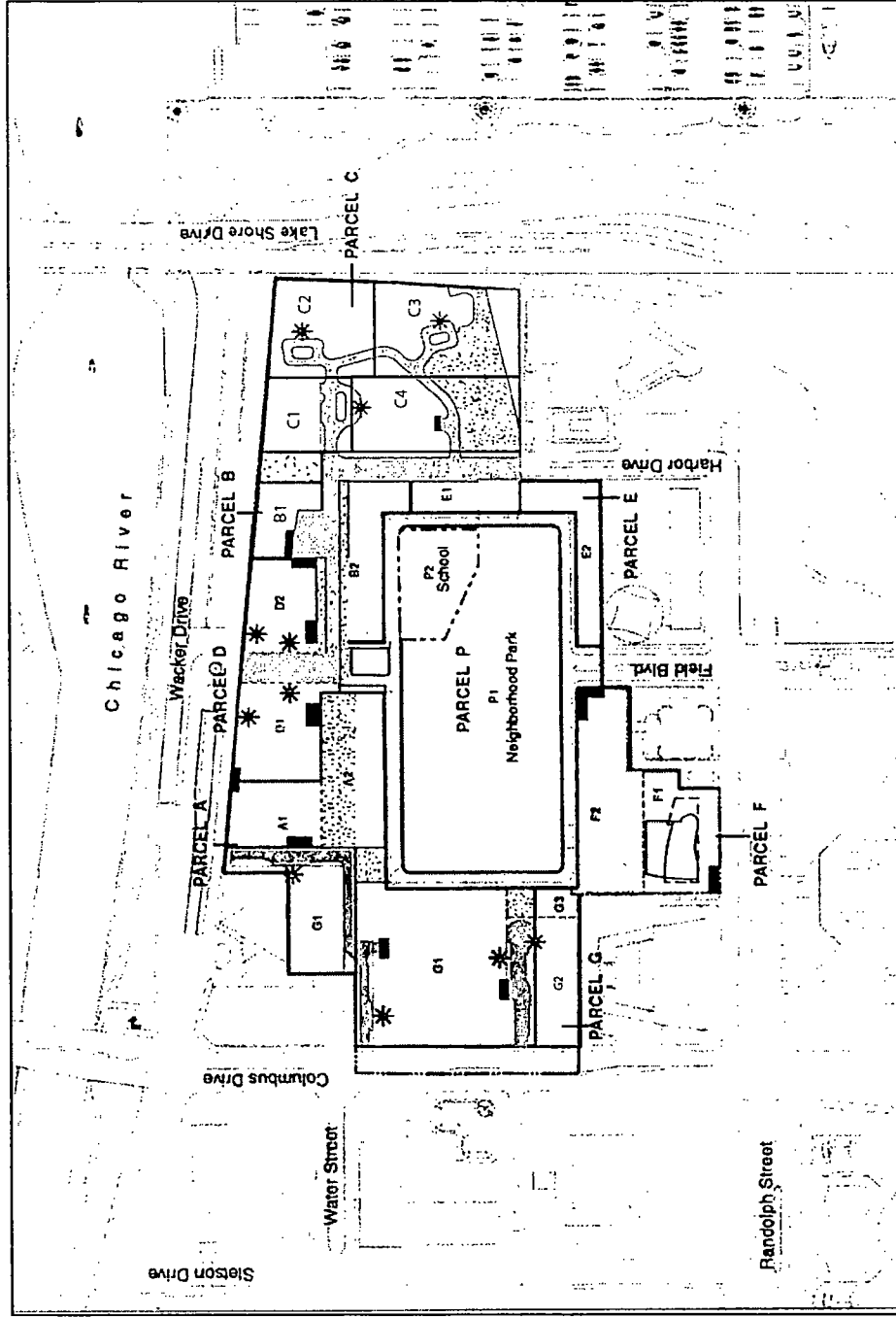


Automobile Drop-Off Zones

- Automobile Drop-off zones at building entries should be conveniently located, yet discreet.
- Curb-side drop-off zones are preferred. Large vehicular turn-around interrupting pedestrian sidewalks should be avoided.

Parking Entries

- Entries to parking garages should be incorporated & integrated into the facade of a building.
- Large uncovered ramps within the sidewalk zone or open spaces should be avoided.
- Where parking access traverses a pedestrian access, the paving material should be consistent w/ the adjacent pedestrian sidewalk paving.



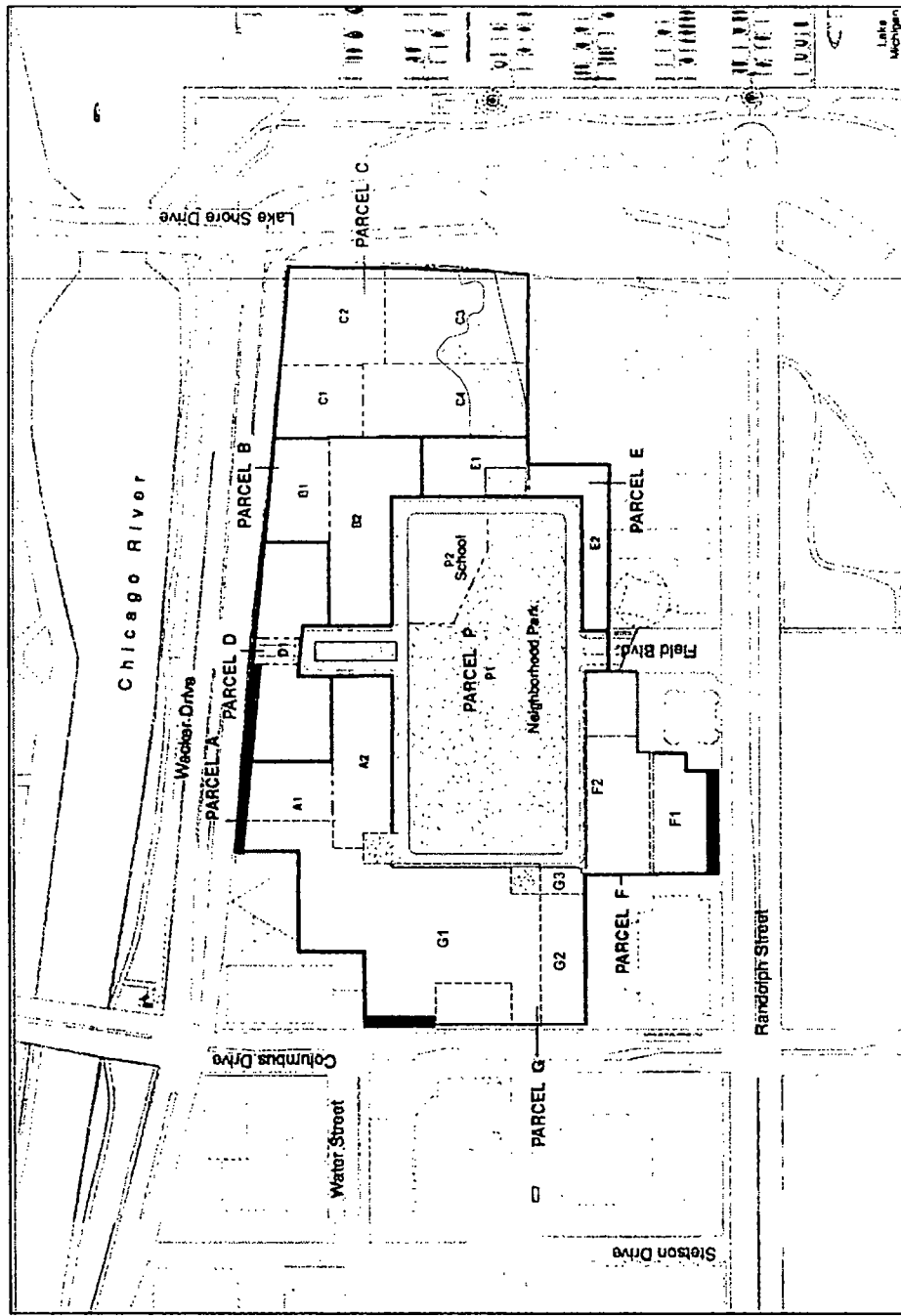
Allowed automobile entrance zones

* Drop Off Zones

Privately Held Public Open Space

NOTE: These plans are diagrammatic in nature and are only intended to show suggested, not required automobile entrance zones.

Automobile Entrance Zones - Intermediate Level



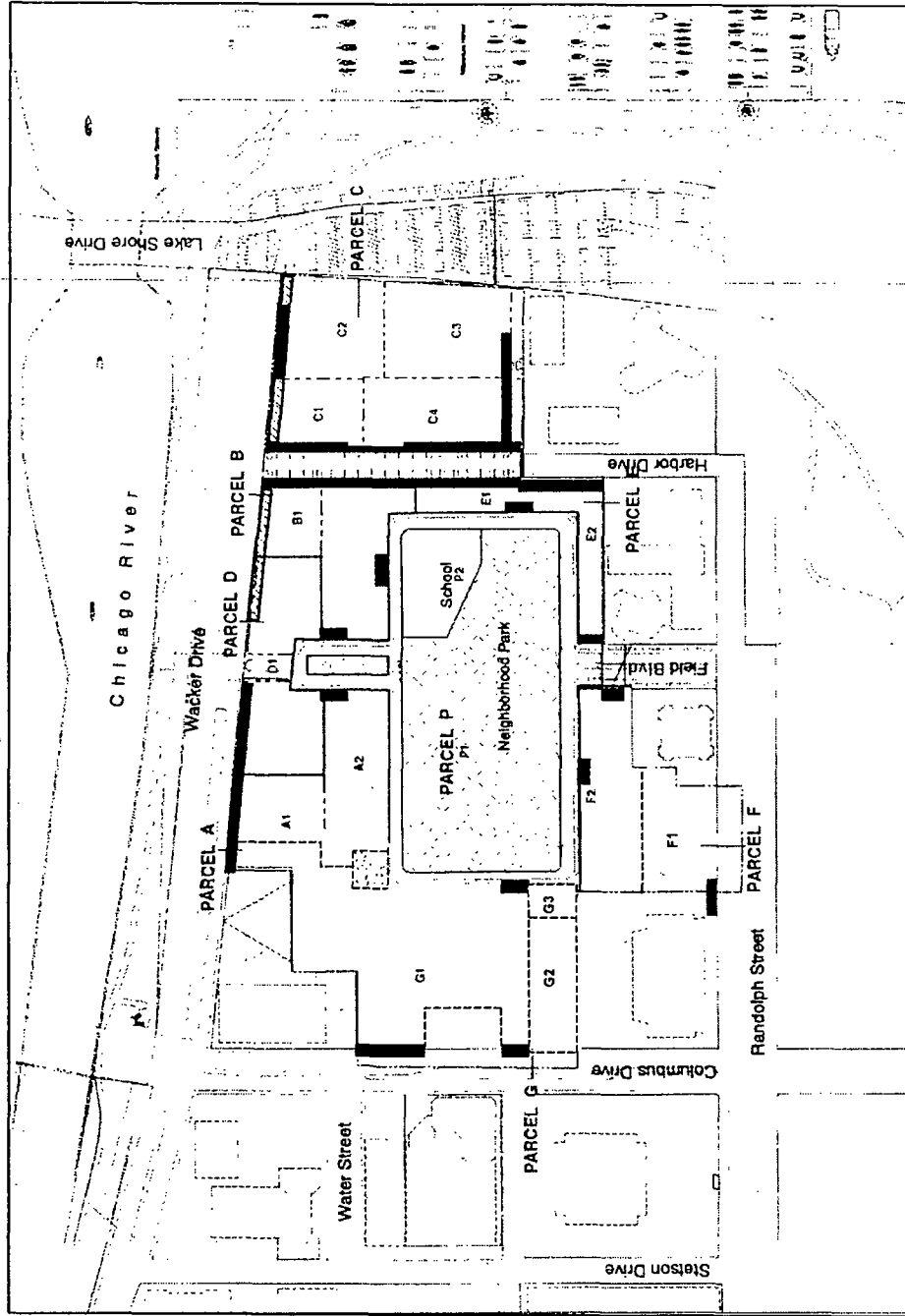
- Allowed automobile entrance zones
- Privately Held Public Open Space

NOTE: These plans are diagrammatic in nature and are only intended to show suggested, not required automobile entrance zones.

Automobile Entrance Zones - Lower Level

Service Vehicular Access

- The lower level is where all service activity will take place. Both access to and from the site will take place on this level.
- Service access can feed off the existing utility easements at this level.



- ▨ Potential Driveway entry zone (requires extension of a portion of Lower Wacker Drive)
- ▬ Allowed automobile entrance zones

NOTE: These plans are diagrammatic in nature and are only intended to show suggested, not required automobile entrance zones.



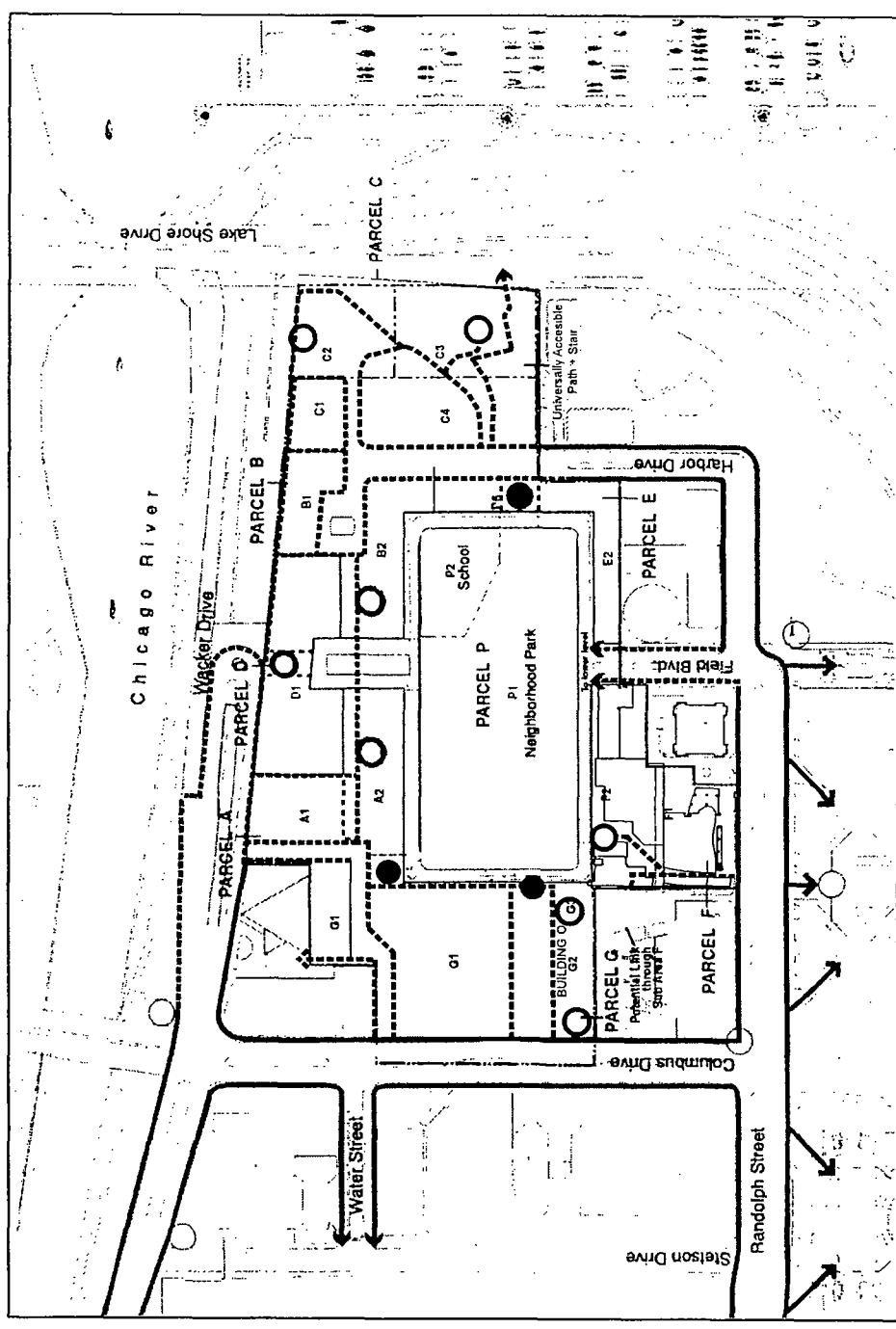
Pedestrian Walkway System - Upper Level

Connections to Grant Park

- Pedestrian Connections will exist on the upper level connecting to existing Grant Park Features such as the Daley Bicentennial Garage, Cancer Survivor's Park, and Millennium Gardens. These connections will adhere to the guidelines of both The City of Chicago Landscape Ordinance, and also those provided by the Chicago Park District

Vertical Connections

- Vertical connections will also be provided, and will be handicap accessible.
- Private developable spaces may be integrated in and around the vertical connections, as long as public access is maintained to the elevators and/or stairs.

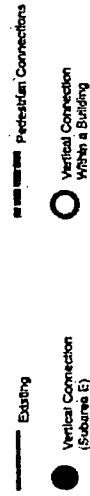
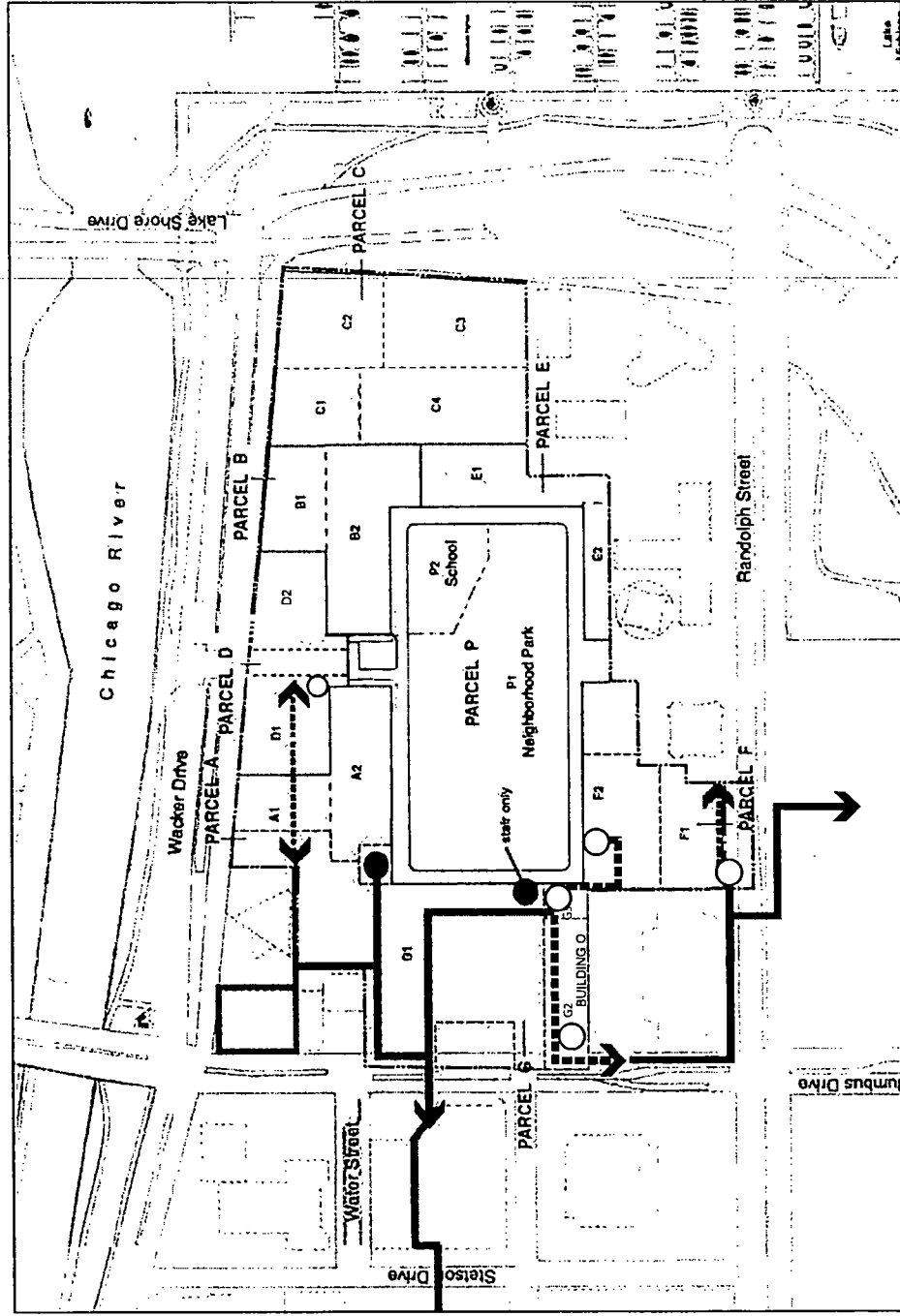


NOTE These plans are diagrammatic in nature and they indicate the approximate location and the intent of the vertical connection, not the precise locations.

Pedestrian Walkway System - The Pedway System

Connections to existing pedway system

- Within Subarea E, connections to the existing pedway system will be provided to sub-parcels A1, D1, F1, G1 and G2, as shown on the adjacent diagram. In general, the new connections maintain or exceed minimum requirements, and provide a minimum / maximum of 8-25' walkway widths.
- Within other subarea, potential connections could feed into these proposed for Subarea E.

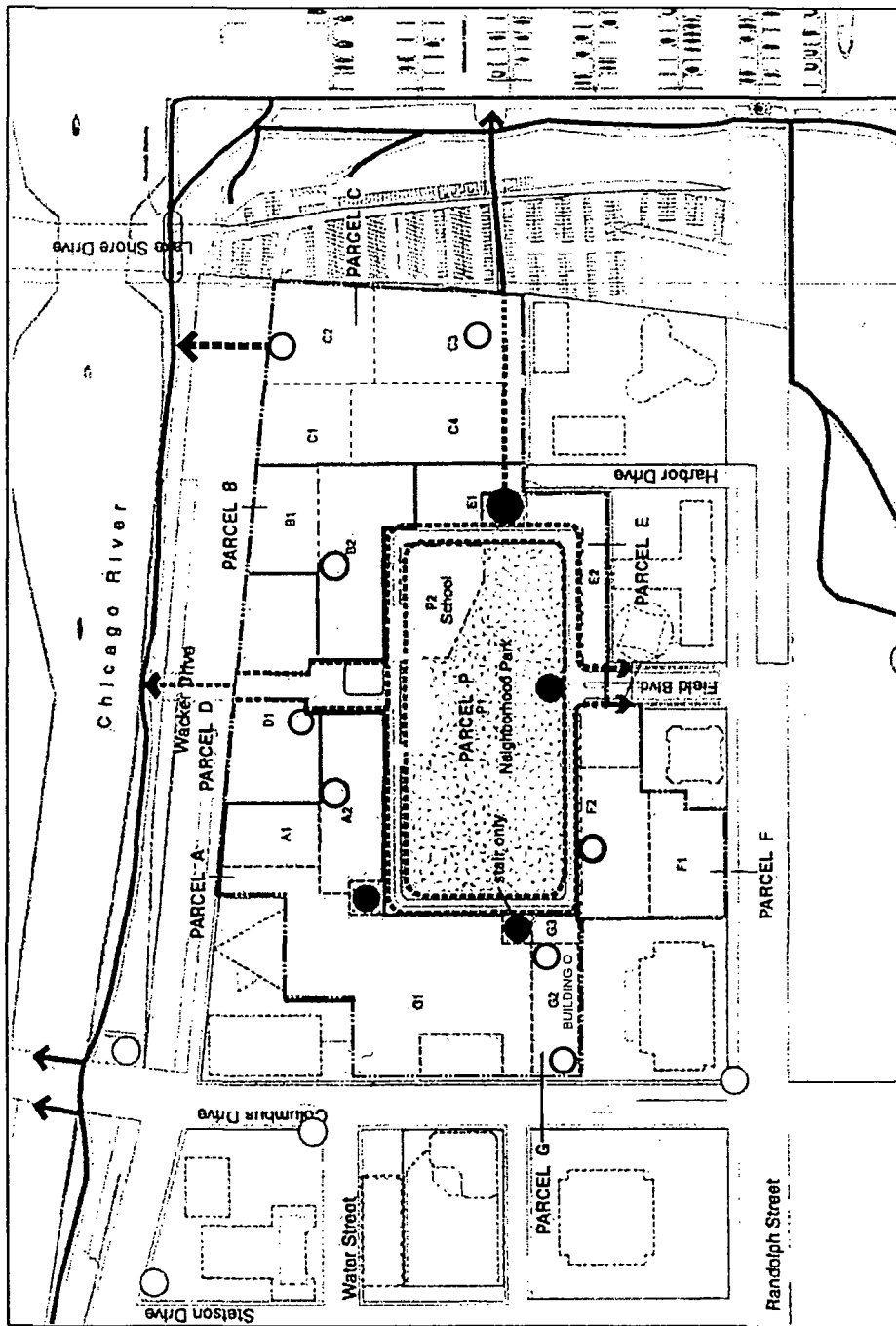


NOTE: This plan is diagrammatic in nature and is intended to show which parcels / sub-parcels should be connected to the parkway system, not the actual route for the connections.

Pedestrian Walkway System - Lower Level

Connections to the River and Lakefront

- Pedestrian Connections will be provided to both the lakefront and river at the lower level. These connections will be coordinated with The Chicago Park District.
- The character of the connections to the river and lakefront should be pedestrian friendly. Ornamental pedestrian fixtures should be provided. Adequate lighting levels for safety should also be provided. A safe marked zone for pedestrians should be created.
- Parking and sound should be screened from view of these pedestrian corridors to the greatest extent possible.
- Long large walls at the proposed underground pedestrian connection to the Lake and River should provide openings to active spaces when possible or be well-fit with articulated architectural details and potentially public art displays such as the existing Riverwalk Gateway at the southwest corner of the River and the Lake.



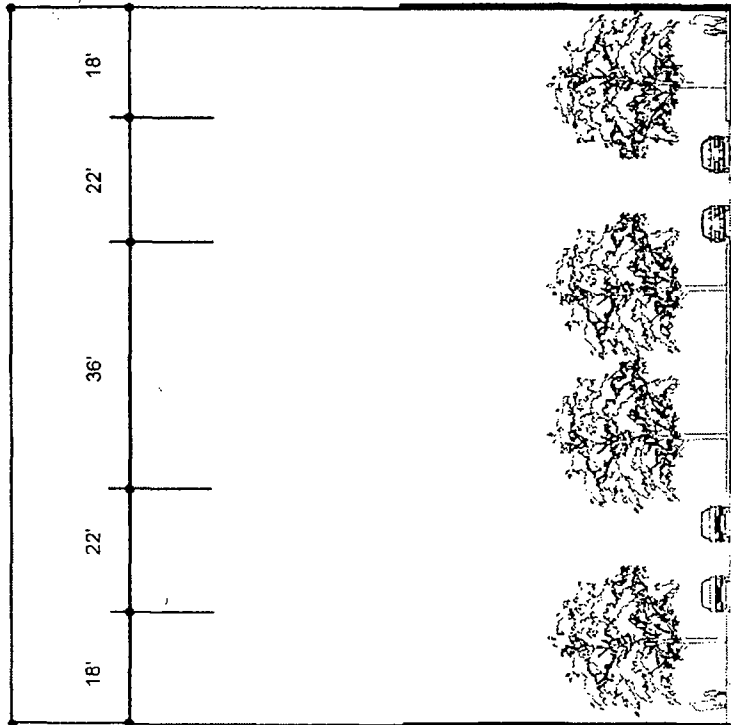
NOTE: This plan is diagrammatic in nature and is intended to show which parcels / sub-parcels should be connected to the parkway system, not the actual route for the connections.

VERTICAL CONNECTIONS: Private Developable spaces may be integrated in and around the vertical connections, as long as public access is maintained to the elevators and/or stairs.

FIELD BLVD. NORTH

Field Blvd. North serves as a major entry into the new development from the north. Field Blvd. shall be a divided boulevard oriented north and south, connecting to the Park Drive. It will feature a consistent and attractive streetscape to include canopy trees, street lighting and pedestrian walk (per City ordinance). These walks will serve as the primary link for pedestrians walking to and from the Neighborhood Park and the Chicago Riverwalk.

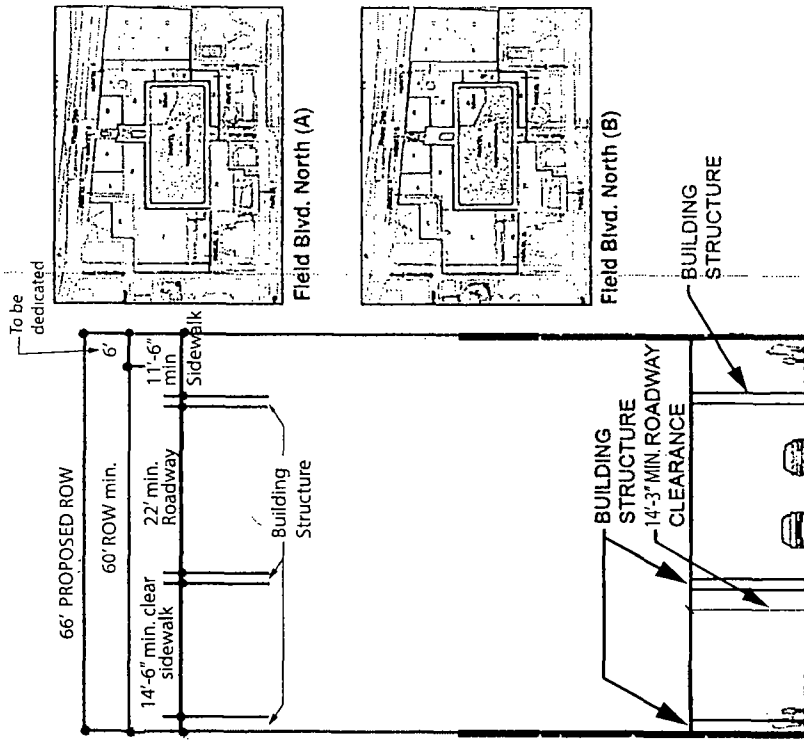
116' R.O.W.



TYPE

R.O.W. WIDTH	116'
FACE OF CURB TO FACE OF CURB	22' / 22'
TRAFFIC LANES	ONE WAY / ONE WAY
TRAFFIC LANES WIDTH	22' / 22'
PARKING LANES	NO PARKING
PARKING LANE WIDTH	N/A
DESIGN SPEED	20 MPH
PARKWAY WIDTH	18'
SIDEWALK WIDTH	30'
CURB RADIUS	25' O.C.
STREET TREE SPACING (MIN.)	

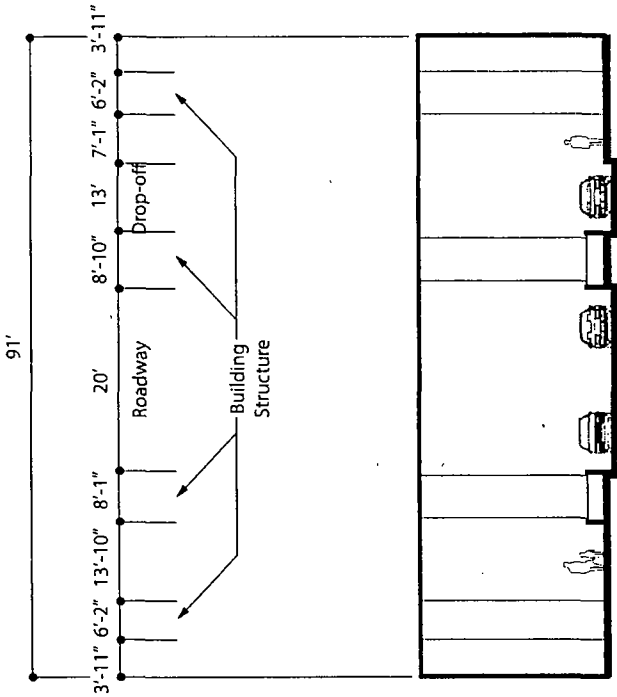
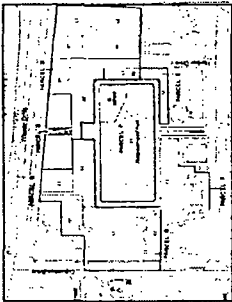
FIELD BLVD. NORTH (A)



TYPE

R.O.W. WIDTH	66' MIN. (66' PROPOSED)
FACE OF CURB TO FACE OF CURB	22' / 22'
TRAFFIC LANES	ONE-WAY EACH SIDE
TRAFFIC LANES WIDTH	11' / 11'
DESIGN SPEED	20 MPH
PARKWAY WIDTH	N/A
MEDIAN WIDTH	N/A
SIDEWALK WIDTH	11'-6" MIN.
CURB RADIUS	30'

FIELD BLVD. NORTH (B)

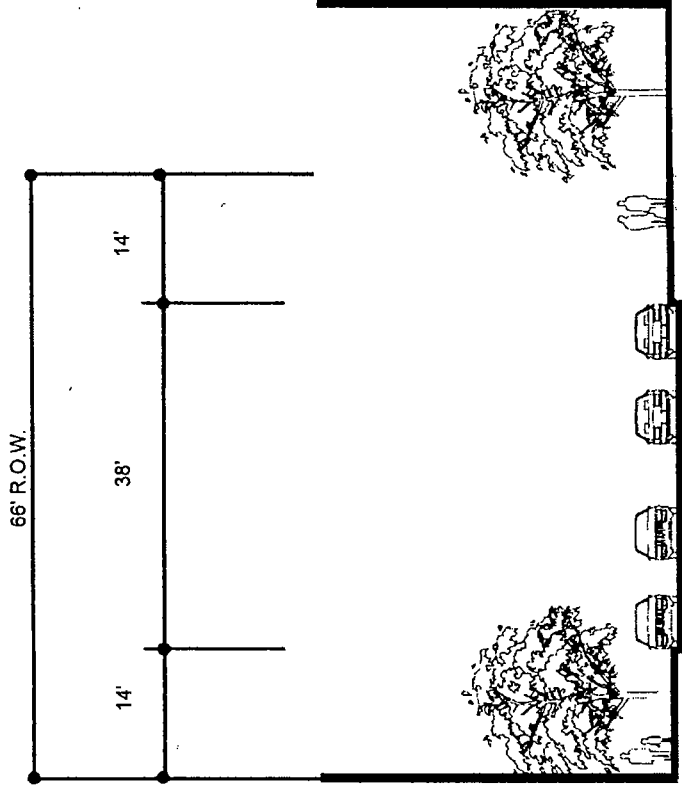
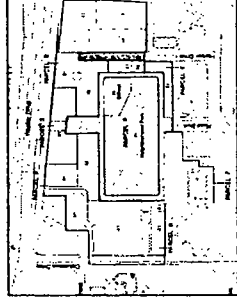


TYPE	UPPER FIELD BLVD. (NORTH)	
	N/A	20'
R.O.W. WIDTH	N/A	20'
FACE OF CURB TO FACE OF CURB	N/A	20'
TRAFFIC LANES	N/A	20'
TRAFFIC LANES WIDTH	N/A	20'
PARKING LANES	N/A	20'
PARKING LANE WIDTH	N/A	20'
DESIGN SPEED	N/A	20 MPH
MINIMUM PARKWAY WIDTH	N/A	20'
MINIMUM SIDEWALK WIDTH	N/A	3'-11"
CURB RADIUS	N/A	20'

HARBOR DRIVE EXTENSION

The Harbor Drive Extension shall continue existing Harbor Drive north, from Randolph Street at the south to access auto courts on the north. An interim green space shall be provided along Wacker Drive (at the upper level). This allows the opportunity for Harbor Drive to connect to Upper Wacker Drive at a later date.

The Harbor Drive Extension shall feature a consistent and attractive streetscape to include, street lighting and pedestrian walks (per City ordinance). Canopy trees as well as other plant material shall be provided to the greatest extent possible.



TYPE HARBOR DRIVE EXTENSION

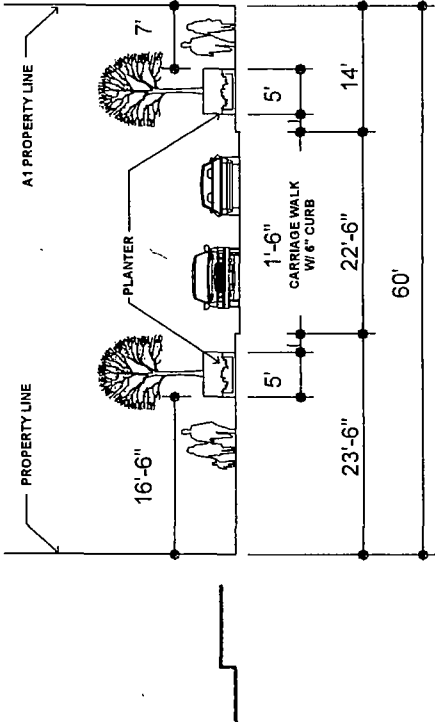
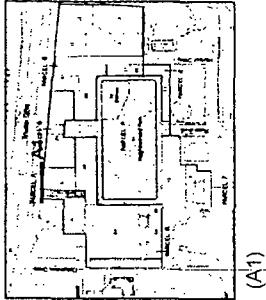
R.O.W. WIDTH	66'
FACE OF CURB TO FACE OF CURB	38'
TRAFFIC LANES	TWO-WAY
TRAFFIC LANES WIDTH	11' / 11'
PARKING LANES	BOTH SIDES
PARKING LANE WIDTH	8'
DESIGN SPEED	20 MPH
SIDEWALK WIDTH	14'
CURB RADIUS	15'
STREET TREE SPACING (MIN)	25' O.C.

NORTH/SOUTH STREET

The North/South Street serves as additional internal access for new development on the northwestern edge of the development. It will feature an attractive streetscape including street lighting and pedestrian walks (per City ordinance). Canopy Trees as well as other plant material should be provided to the greatest extent possible.

As described by the following diagrams, the 60' R.O.W. remains consistent throughout its length, however, the street section (paved areas vs sidewalk areas) varies within the 60' width

For this particular (C) section of the North/South St. Section. Either (C) or (B) may be used as alternatives for this section.



**COLUMBUS WACKER THRU DRIVE PLANTING MEDIAN
NORTH-SOUTH STREET (A1)**

Refer to Sidewalk Design Criteria 4.20 "Sidewalk on structure with low planters"

NOTE: These plans are diagrammatic in nature and are only intended to show approximate locations and dimensions.

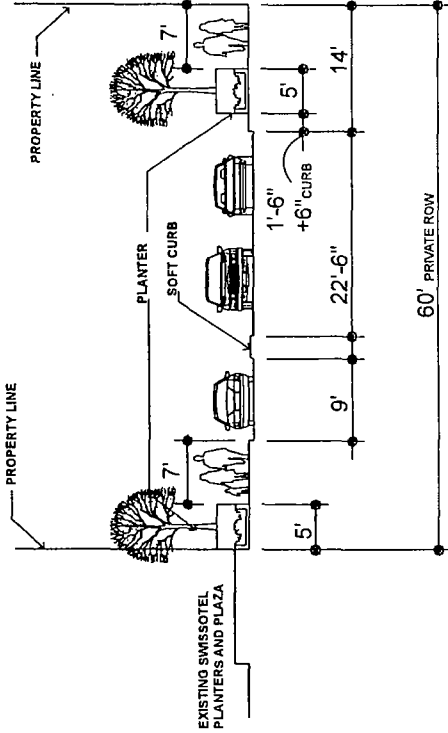
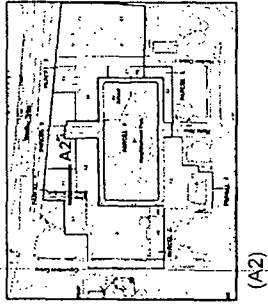
R.O.W. WIDTH	60'
FACE OF CURB TO FACE OF CURB	22'-6"
TRAFFIC LANES	TWO - WAY
TRAFFIC LANES WIDTH	11' / 11' MIN.
PARKING LANES	N/A
PARKING LANE WIDTH	N/A
DESIGN SPEED	20 MPH
SIDEWALK WIDTH	AS SHOWN ABOVE
CURB RADIUS	15'
STREET TREE SPACING (MIN)	25' O.C.

NORTH/SOUTH STREET

The North/South Street serves as additional internal access for new development on the northwestern edge of the development. It will feature an attractive streetscape including street lighting and pedestrian walks (per City ordinance). Canopy Trees as well as other plant material should be provided to the greatest extent possible.

As described by the following diagrams, the 60' R.O.W. remains consistent throughout its length, however, the street section (paved areas vs. sidewalk areas) varies within the 60' width.

For this particular (C) section of the North/South St. Section. Either (C) or (B) may be used as alternatives for this section

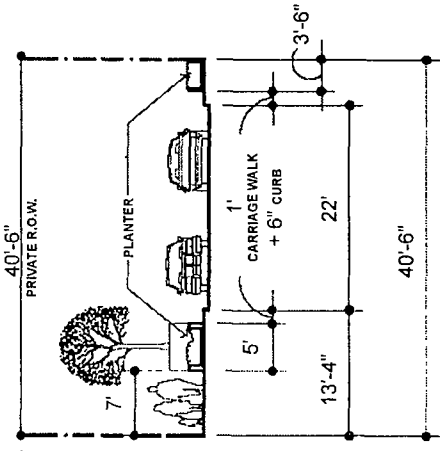


**COLUMBUS WACKER THRU DRIVE PLANTING MEDIAN
NORTH-SOUTH STREET (A2)**

Refer to Sidewalk Design Criteria 4.20 "Sidewalk on structure with raised planters"

NOTE: These plans are diagrammatic in nature and are only intended to show approximate locations and dimensions

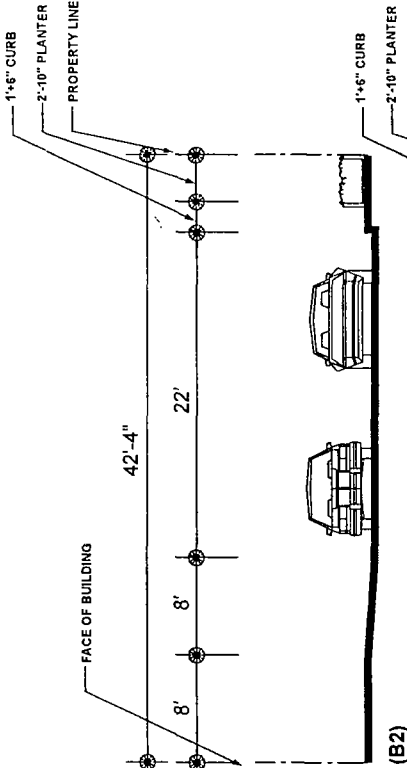
R.O.W. WIDTH	60'
FACE OF CURB TO FACE OF CURB	22'-5"
TRAFFIC LANES	TWO - WAY
TRAFFIC LANES WIDTH	11' / 11' MIN.
DROP OFF LANES	1
DROP OFF LANE WIDTH	9'
DESIGN SPEED	20 MPH
SIDEWALK WIDTH	7' MIN. (INCLUDING 6" CURB)
CURB RADIUS	15'
STREET TREE SPACING (MIN)	25' O.C.



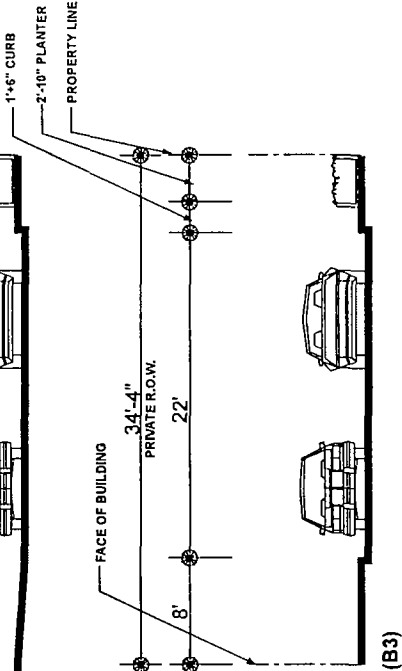
COLUMBUS WACKER E-W THRU DRIVE
NORTH-SOUTH STREET (B)

Refer to Sidewalk Design Criteria 4.20
"Sidewalk on structure with low planters"

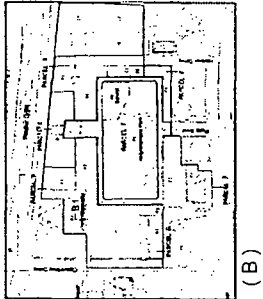
NOTE: These plans are diagrammatic in nature and are only intended
to show approximate locations and dimensions.



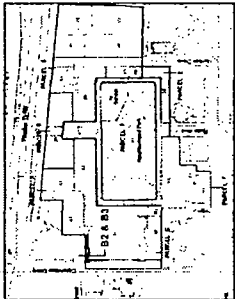
(B2)



(B3)



(B)



(B2 & B3)

NORTH-SOUTH (B)

NORTH-SOUTH STREET (B2)

R.O.W. WIDTH	40'-6"
FACE OF CURB TO FACE OF CURB	22'
TRAFFIC LANES	TWO - WAY
TRAFFIC LANES WIDTH	11' / 11'
PARKING LANES	N/A
PARKING LANE WIDTH	N/A
DESIGN SPEED	20 MPH
SIDEWALK WIDTH	7' MIN.
CURB RADIUS	15'
STREET TREE SPACING (MIN)	25' O.C.

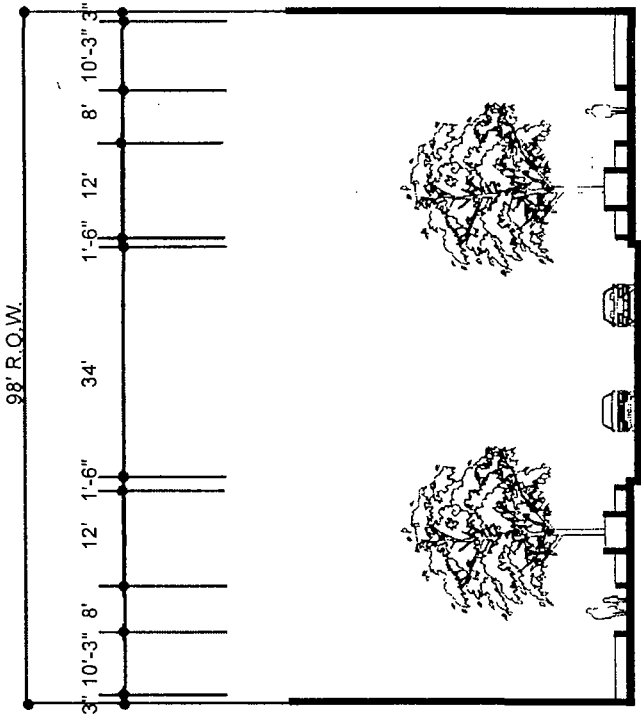
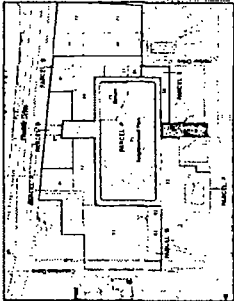
R.O.W. WIDTH	22'
FACE OF CURB TO FACE OF CURB	TWO - WAY
TRAFFIC LANES	11' / 11'
TRAFFIC LANES WIDTH	8' ONE SIDE
DROP OFF LANE	N/A
PARKING LANE WIDTH	20 MPH
DESIGN SPEED	8' (INCLUDING 6" CURB)
SIDEWALK WIDTH	15'
CURB RADIUS	N/A
STREET TREE SPACING (MIN)	N/A

FIELD BLVD. SOUTH

Field Blvd. South serves as a major entry into the new development from the south. Field Blvd. South shall be oriented north and south, connecting Randolph Street to the Park Drive. It will lead directly to the main overlook terrace at the Public Park.

The drive will feature a consistent and attractive streetscape to include canopy trees, street lighting and pedestrian walks (per City ordinance). These walks will serve as the primary link for pedestrians walking to and from Grant Park and the Neighborhood Park.

The street may slope down at a maximum rate of 1/12, provided that handicap accessible walks are provided along the edges with appropriate landings provided every 30" with vertical rise. The 23' wide sidewalk zone must provide adequate landscaping to screen the adjacent buildings' parking podiums. Air intakes and exhaust from lower level uses may be located within the median but must be screened with landscaping.



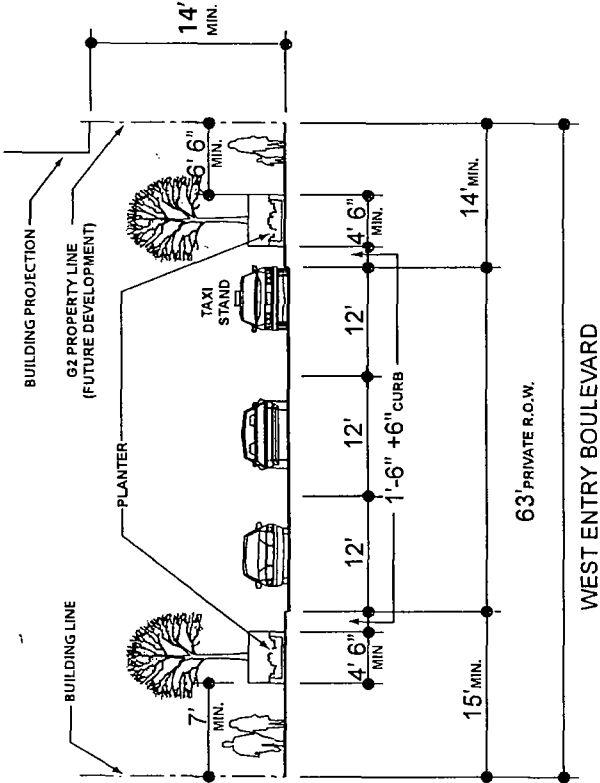
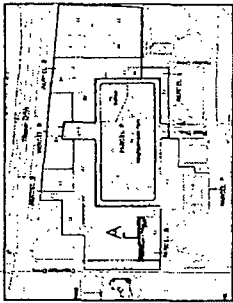
FIELD BLVD. (SOUTH)

TYPE	
R.O.W. WIDTH	98'
FACE OF CURB TO FACE OF CURB	34'
TRAFFIC LANES	TWO WAY
TRAFFIC LANES WIDTH	17' / 17'
PARKING LANES	NO PARKING
PARKING LANE WIDTH	N/A
DESIGN SPEED	20 MPH
MINIMUM PARKWAY WIDTH	7'
MINIMUM SIDEWALK WIDTH	6'
CURB RADIUS	
STREET TREE SPACING (MIN)	25' O.C.

WEST ENTRY BOULEVARD

The West Entry Boulevard serves as a major entry into the new development from the west. The West Entry Boulevard connects to Columbus St and the new North South Street, which will also enhance the connectivity to the park from the west.

This street will feature a consistent and attractive streetscape including canopy trees, shrubs, groundcover, perennials, street lighting and pedestrian walks (per City ordinance). These walks will serve as the primary link for pedestrians walking to and from the west and the Neighborhood Park.

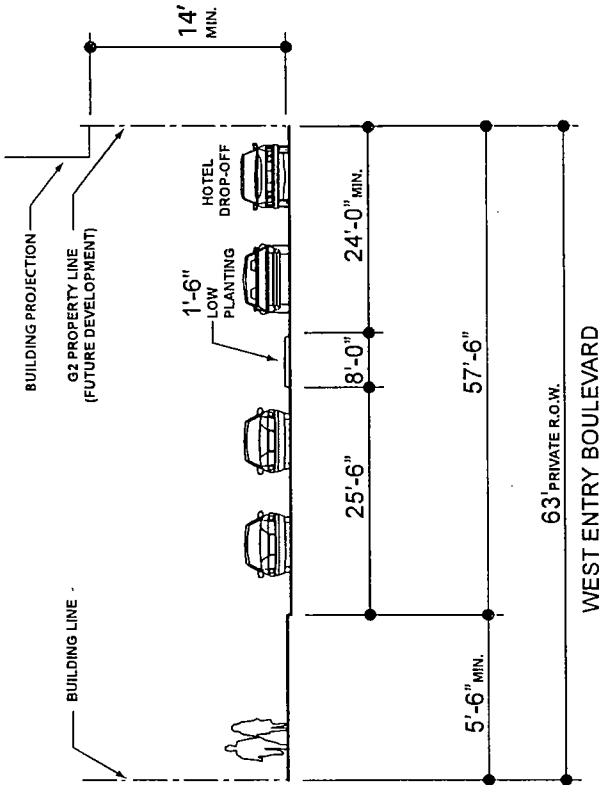
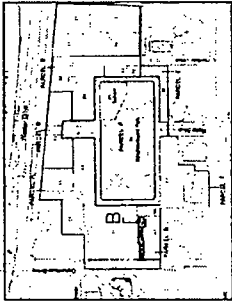


R.O.W. WIDTH	63'
FACE OF CURB TO FACE OF CURB	32'-6"
TRAFFIC LANES	TWO WAY
TRAFFIC LANES WIDTH	12' / 12'
PARKING LANES (TAXI)	ONE SIDE
PARKING LANE WIDTH (TAXI)	8'
DESIGN SPEED	20 MPH
SIDEWALK WIDTH	7' MIN.
CURB RADIUS	15'
STREET TREE SPACING (MIN)	25' O.C.

WEST ENTRY BOULEVARD

The West Entry Boulevard serves as a major entry into the new development from the west. The West Entry Boulevard connects to Columbus St. and the new North South Street, which will also enhance the connectivity to the park from the west.

This street will feature a consistent and attractive streetscape including canopy trees, shrubs, groundcover, perennials, street lighting and pedestrian walks (per City ordinance). These walks will serve as the primary link for pedestrians walking to and from the west and the Neighborhood Park.



R.O.W. WIDTH	63'
FACE OF CURB TO FACE OF CURB	57'-6"
TRAFFIC LANES	TWO WAY
TRAFFIC LANES WIDTH	AS SHOWN
PARKING LANES (TAXI)	ONE SIDE
PARKING LANE WIDTH (TAXI)	8'
DESIGN SPEED	20 MPH
SIDEWALK WIDTH	5'-6" MIN.
CURB RADIUS	15'
STREET TREE SPACING (MIN)	N/A

THE PARK DRIVE

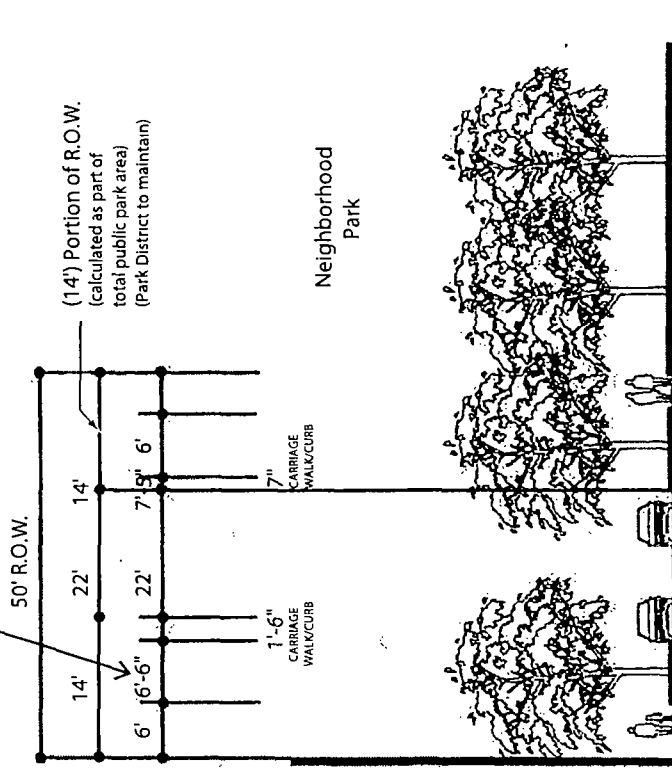
The Park Drive shall be a pedestrian-friendly neighborhood drive encircling the Neighborhood Park. It will feature a consistent and attractive streetscape to include canopy trees and other plant material in a parkway setting. It is recommended that traffic circulate in a direction which allows curbside drop-off adjacent to the school. Street lighting and pedestrian walks (per City ordinance) shall provide a pedestrian-friendly experience along the drive

From the intersection at Field Blvd. South, the Park Drive shall slope at a maximum rate of 1:20 in order to ensure handicap accessibility along the sidewalks.

The 15' wide zone at the perimeter of the park includes sidewalk & parkway which will be maintained by the city of Chicago.

NOTE: The Chicago Park District has agreed to maintain the landscaped parkway and sidewalk located on the park side.

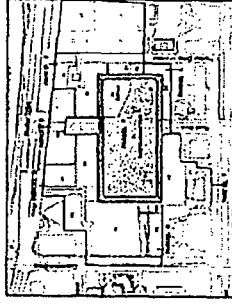
Should existing R.O.W not accommodate required widths, dimension of planter should be reduced (min. 5' - 0")



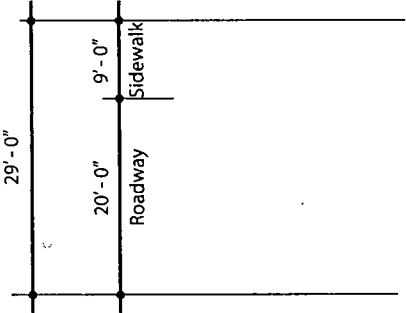
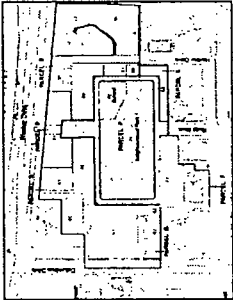
TYPE

R.O.W. WIDTH	50'
FACE OF CURB TO FACE OF CURB	22'
TRAFFIC LANES	ONE - WAY
TRAFFIC LANES WIDTH	12'
PARKING LANES	NO PARKING
PARKING LANE WIDTH	N/A
DESIGN SPEED	20 MPH
SIDEWALK WIDTH	6'
CURB RADIUS	15'
STREET TREE SPACING (MIN)	25' O.C.

PARK DRIVE, E. BENTON PLACE,
N. WESTSHORE DRIVE, E. SOUTH WATER,
& N. PARK DRIVE



Right-of-Way Criteria
Parcel C Internal Road
(Private Street)



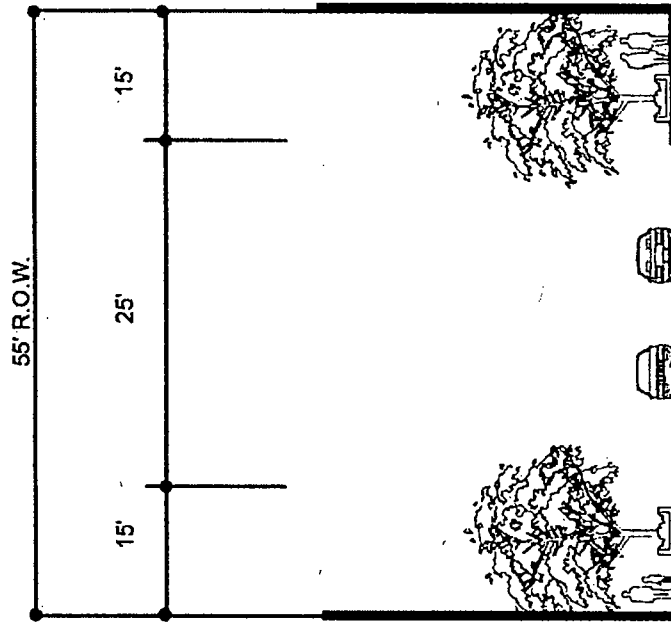
TYPE

PARCEL C INTERNAL ROAD

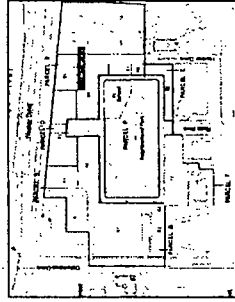
R.O.W. WIDTH	N/A
FACE OF CURB TO FACE OF CURB	20'-0"
TRAFFIC LANES	ONE-WAY
TRAFFIC LANES WIDTH	20'
PARKING LANES	N/A
PARKING LANE WIDTH	N/A
DESIGN SPEED	20 MPH
MINIMUM PARKWAY WIDTH	N/A
MINIMUM SIDEWALK WIDTH	9'-0"
CURB RADIUS	15'

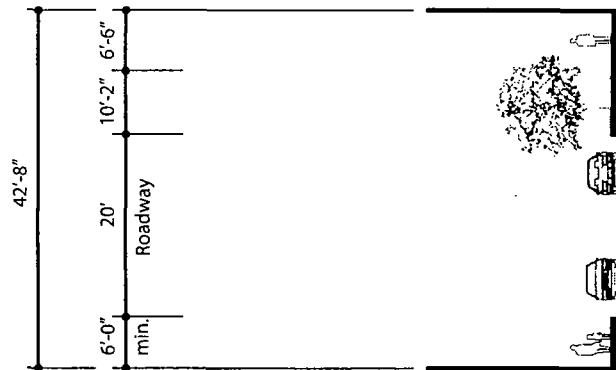
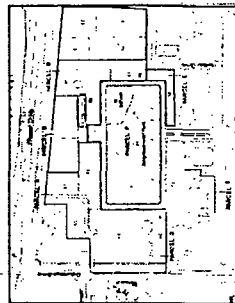
WATERSIDE DRIVE

Waterside Drive shall feature a consistent and attractive streetscape to include, street lighting and pedestrian walks (per City ordinance). Canopy trees as well as other plant material shall be provided to the greatest extent possible.



TYPE	WATERSIDE DRIVE
R.O.W. WIDTH	55'
FACE OF CURB TO FACE OF CURB	25'
TRAFFIC LANES	TWO WAY
TRAFFIC LANES WIDTH	11' 11" MIN.
PARKING LANES	N/A
PARKING LANE WIDTH	N/A
DESIGN SPEED	20 MPH
SIDEWALK WIDTH	7' MIN
CURB RADIUS	15'
STREET TREE SPACING	25' O.C.

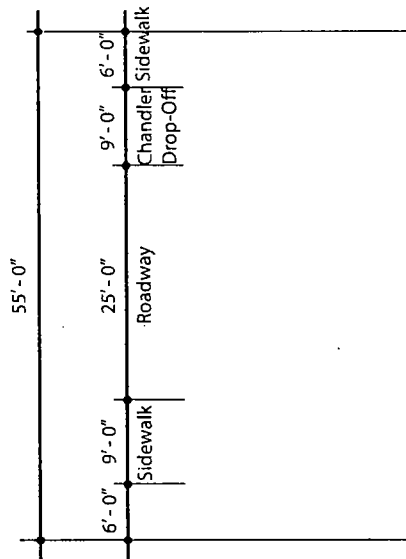
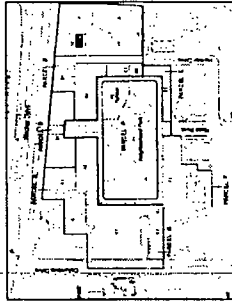




TYPE WATERSIDE DRIVE EXTENSION

R.O.W. WIDTH	N/A
FACE OF CURB TO FACE OF CURB	20'
TRAFFIC LANES	TWO-WAY
TRAFFIC LANES WIDTH	10'
PARKING LANES	N/A
PARKING LANE WIDTH	N/A
DESIGN SPEED	20 MPH
MINIMUM PARKWAY WIDTH	N/A
MINIMUM SIDEWALK WIDTH	6'-0"
CURB RADIUS	15'

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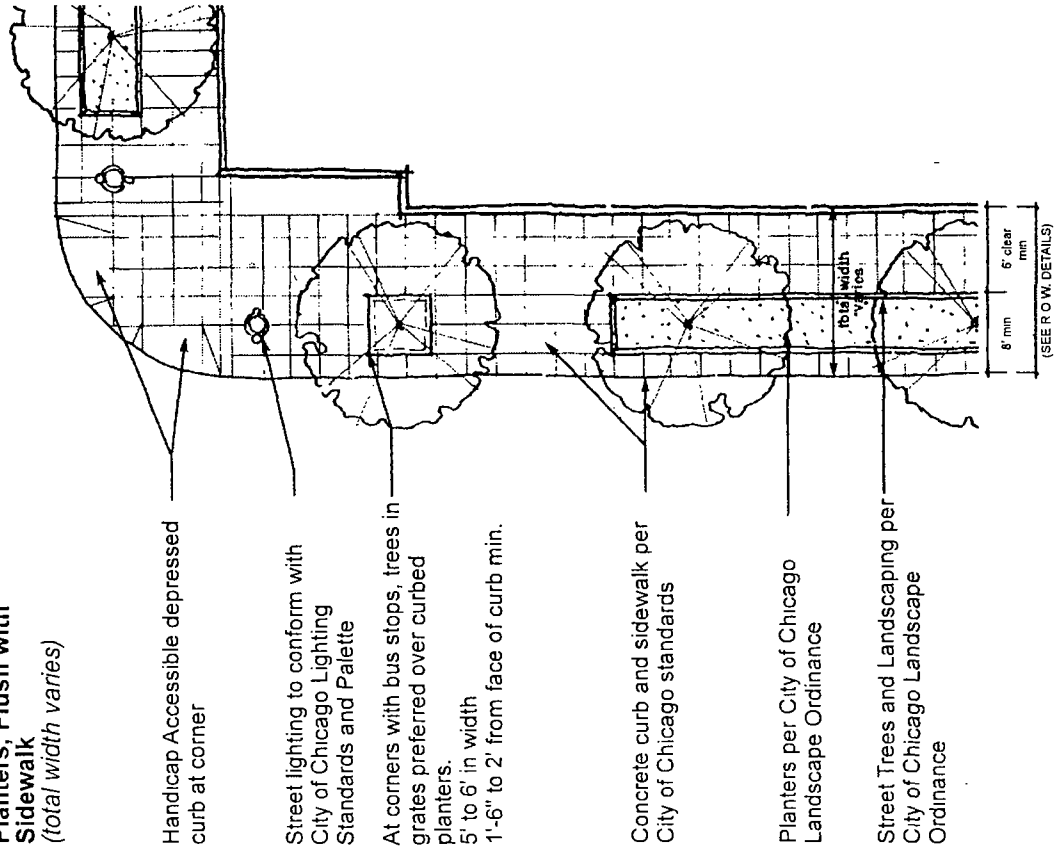


TYPE

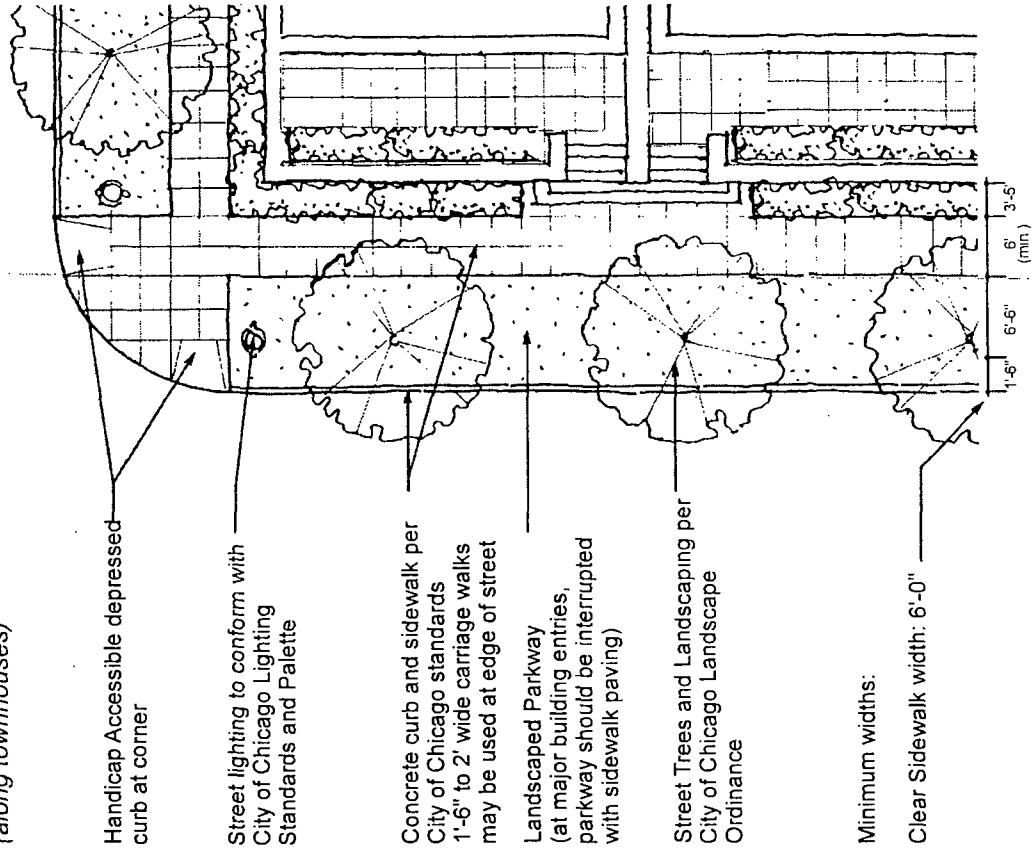
WATERSIDE DRIVE EXTENSION

R.O.W. WIDTH	55'-0"
FACE OF CURB TO FACE OF CURB	25'-0"
TRAFFIC LANES	TWO-WAY
TRAFFIC LANES WIDTH	AS SHOWN
PARKING LANES	N/A
PARKING LANE WIDTH	N/A
DESIGN SPEED	20 MPH
MINIMUM PARKWAY WIDTH	6'-0" MIN
MINIMUM SIDEWALK WIDTH	9'
CURB RADIUS	N/A

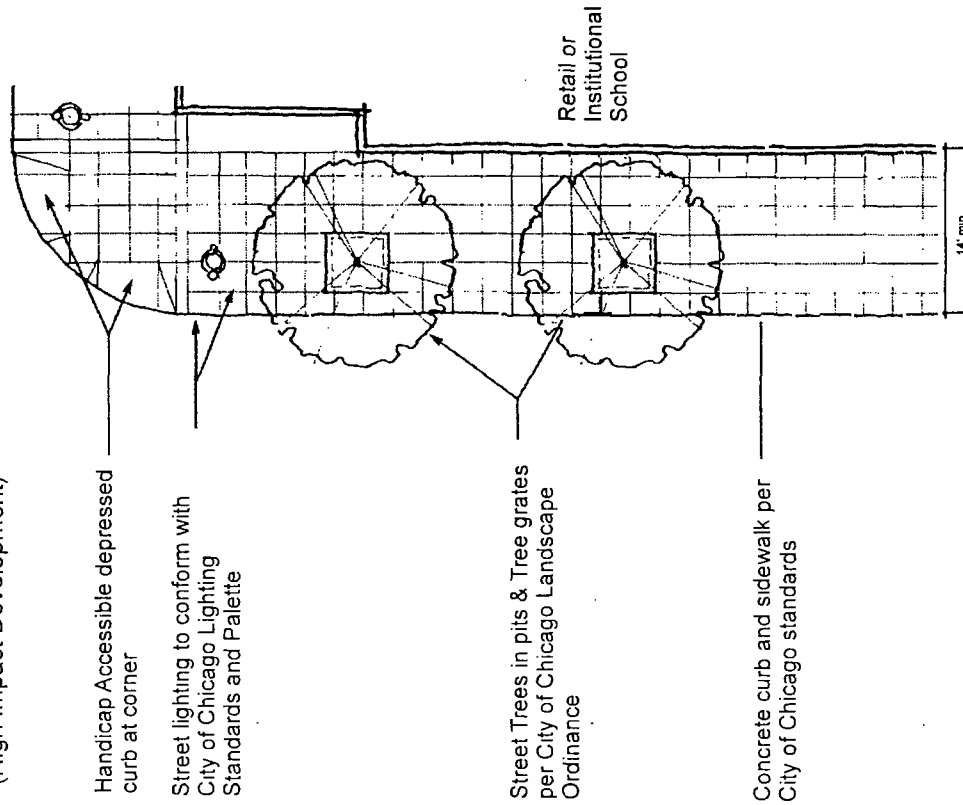
Sidewalk with in - Ground Planters, Flush with Sidewalk (total width varies)



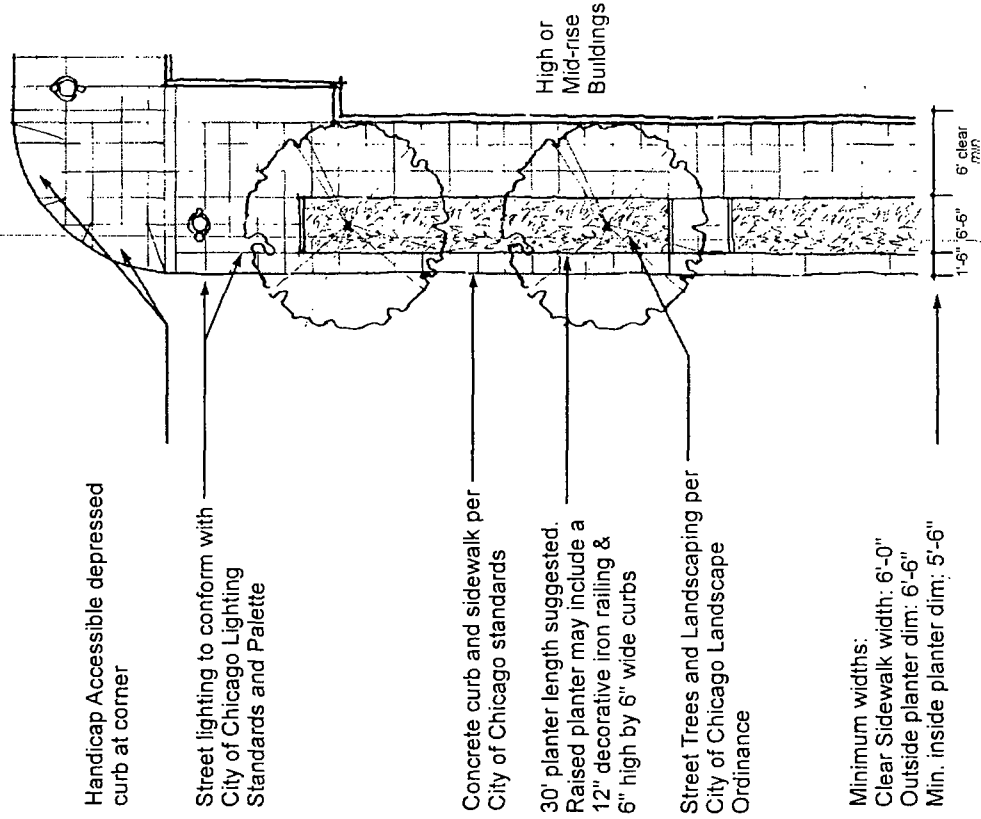
Park Drive Sidewalk (along townhouses)



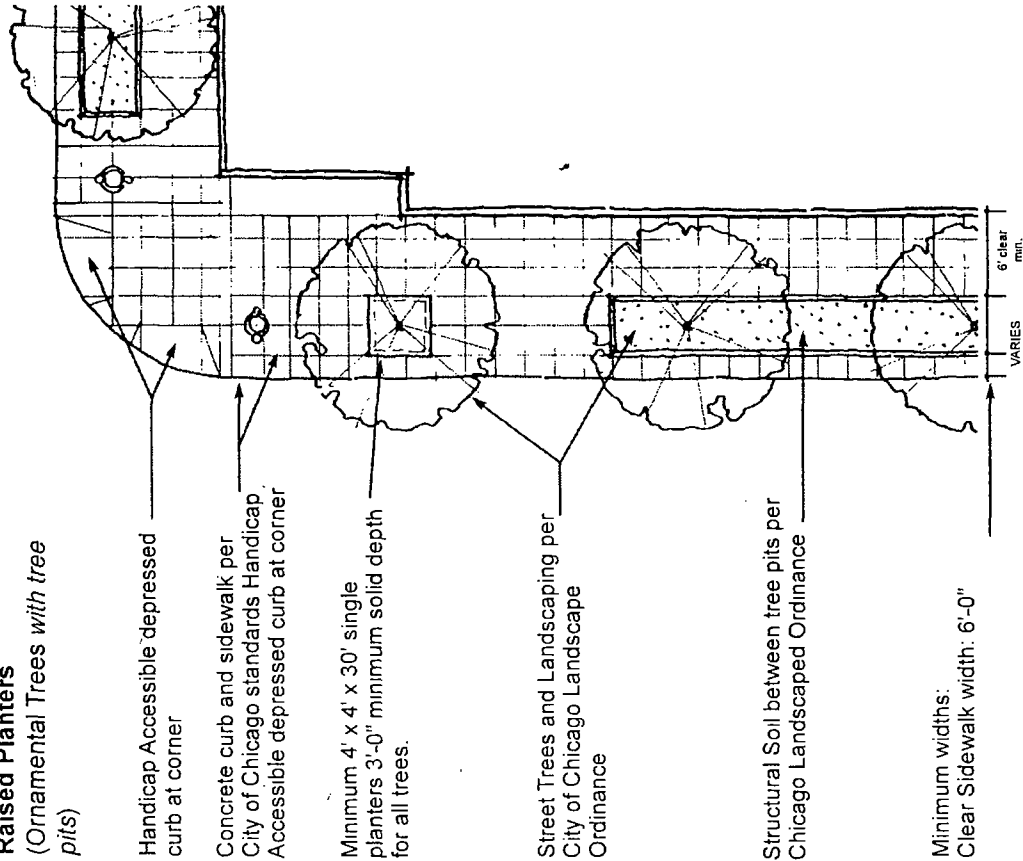
Sidewalk Along Park Road (High Impact Development)



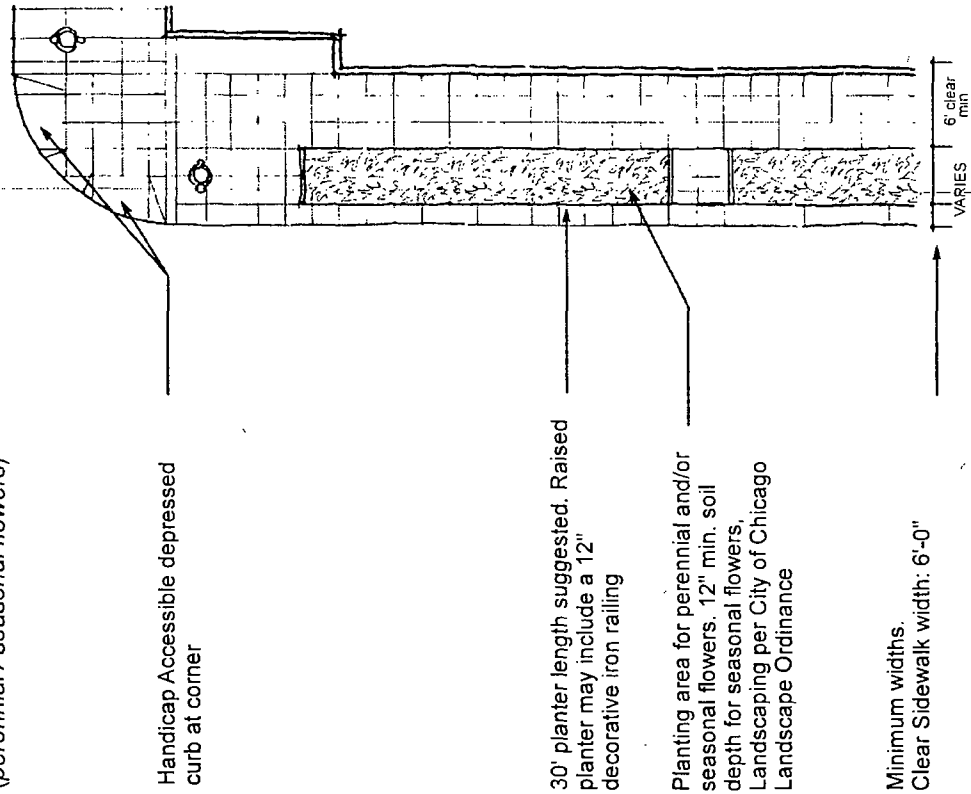
Sidewalk Along Park Road (Moderate Impact Development)



Sidewalk on Structure with Raised Planters
(Ornamental Trees with tree pits)



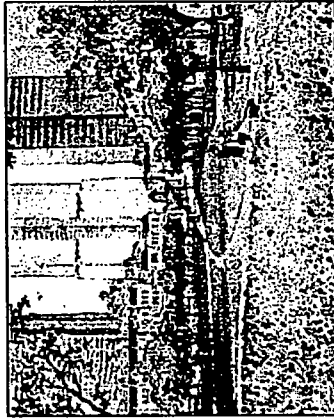
Sidewalk on Structure with Low Planters
(perennial / seasonal flowers)

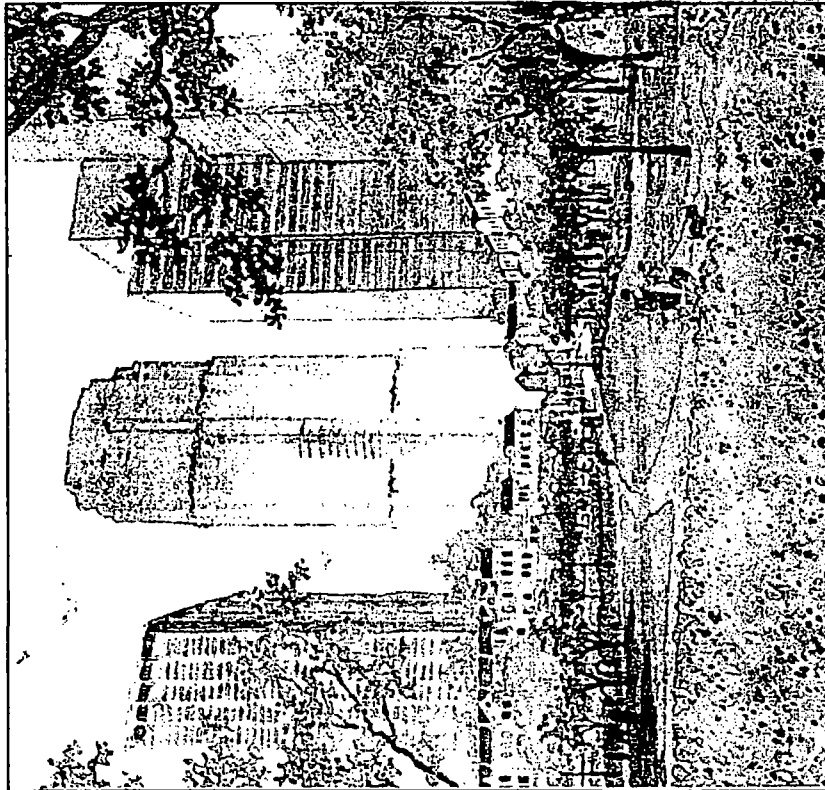


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5.0 Open Space

Introduction
Public Park Zone
The Open Space Framework
The Neighborhood Park Preliminary Conceptual Design
The Neighborhood Park Park Programming Zones
Neighborhood Park Preliminary Landscape Guidelines
Parks and Open Space Parcel C
Parks and Open Space Other Parcels





Introduction

The open space of Lakeshore East is defined by a series of green spaces including a large urban neighborhood park. The neighborhood park area will be approximately 6 acres in its size.

Overall the Lakeshore East site will contain approximately 12 acres of publicly accessible open space which includes courtyards, pedestrian promenades, and landscaped setbacks.

Publicly accessible open space in Parcel C will be open during hours consistent with the Chicago Park District hours for Lakeshore East Park. Additionally, wayfinding and other appropriate informational signage may be established throughout Lakeshore East, subject to approval by DPD.

Connections to the river and the lakefront will be provided as links to the great open space of the city. A strong connection to Grant Park via Field Blvd. will also be provided.

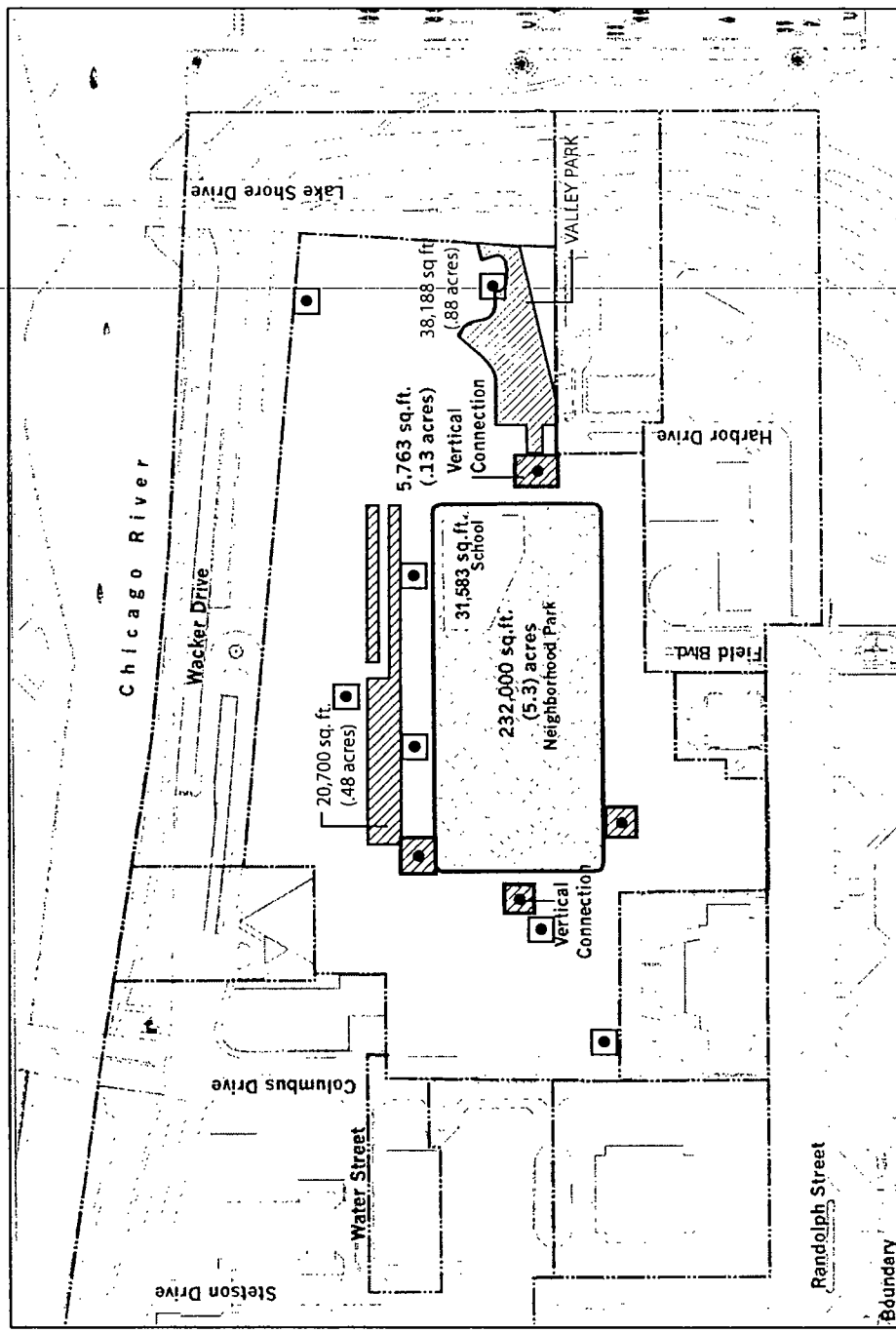
The neighborhood character of the park will be an amenity for both future and existing residents. It will essentially be located at the lower level on grade creating a neighborhood open space where trees and landscaping can flourish.

In conjunction with the neighborhood park there will be courtyards, and open space buffers which will enhance and link the surrounding open space amenities. Interior green spaces or courtyards will serve as open space amenities for both new and existing residents.

Roof top gardens are strongly encouraged to be created on various buildings. These gardens will provide a landscaped aesthetic while also enhancing the environmental aspects of the buildings.

Linear pedestrian promenades will be landscaped with trees, flowering planters and paved sidewalks. These pedestrian streets will act as promenades that connect the east and west edges of the site, but will also provide emergency vehicular access to future developments.

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Total Public Park:	232,000 sq. ft.
Land Area:	5.3 acres
Total School:	31,583 sq. ft.
Land Area:	.73 acres

NOTE: These plans are diagrammatic in nature and they indicate the approximate location and intent of the school location, the spaces to be dedicated as public parks and the vertical connections, but not the precise locations.

The amount of space dedicated as public park is subject to acceptance by the Chicago Park District.

The Open Space Framework for Lake Shore East consists of a variety of parks, promenades and courtyards.

THE NEIGHBORHOOD PARK

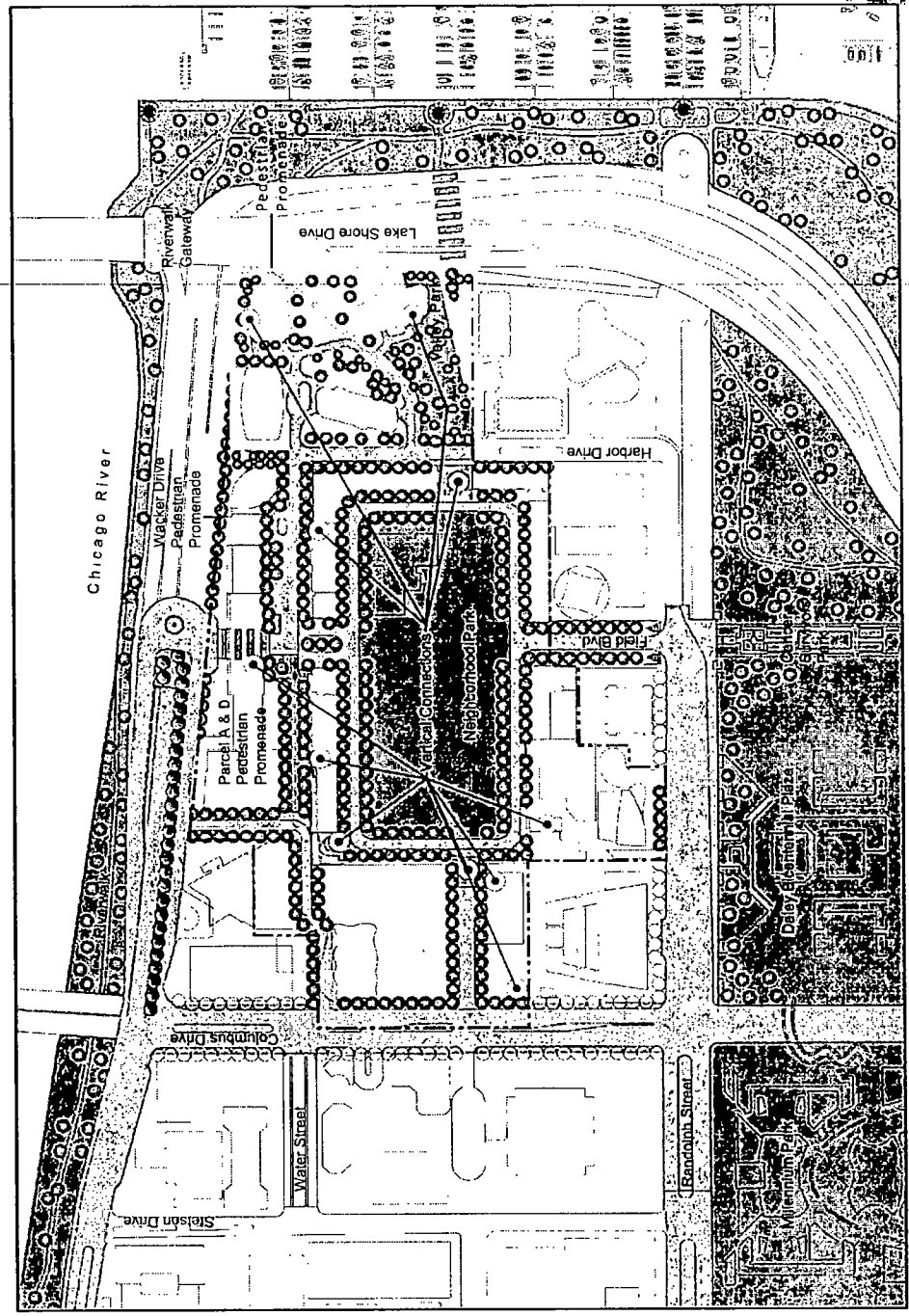
PARCEL C VALLEY PARK

PARCEL C LAKESHORE DRIVE
PROMENADE

PARCEL A&D
PEDESTRIAN PROMENADE

PARCEL B&C WACKER DRIVE
PROMENADE

VERTICAL CONNECTIONS



The Open Space Framework

NOTE: These plans are diagrammatic in nature and they indicate the approximate location and the intent of the vertical connection, not the precise locations.

NEIGHBORHOOD PARK Preliminary Conceptual Design

A large public park ("the Neighborhood Park") shall be designed and constructed to encourage public use. The Neighborhood Park is intended to function as a public space for outdoor events and provide a gathering space for the surrounding office, hotel and residential neighborhood.

Neighborhood Park Scale & Character

The park should generally function as a passive neighborhood open space. Large permanent active athletic uses, with the exception of the multi purpose open space in the center of the park, should be avoided. The park should be designed and scaled appropriately for neighborhood use as opposed to a regional attraction.

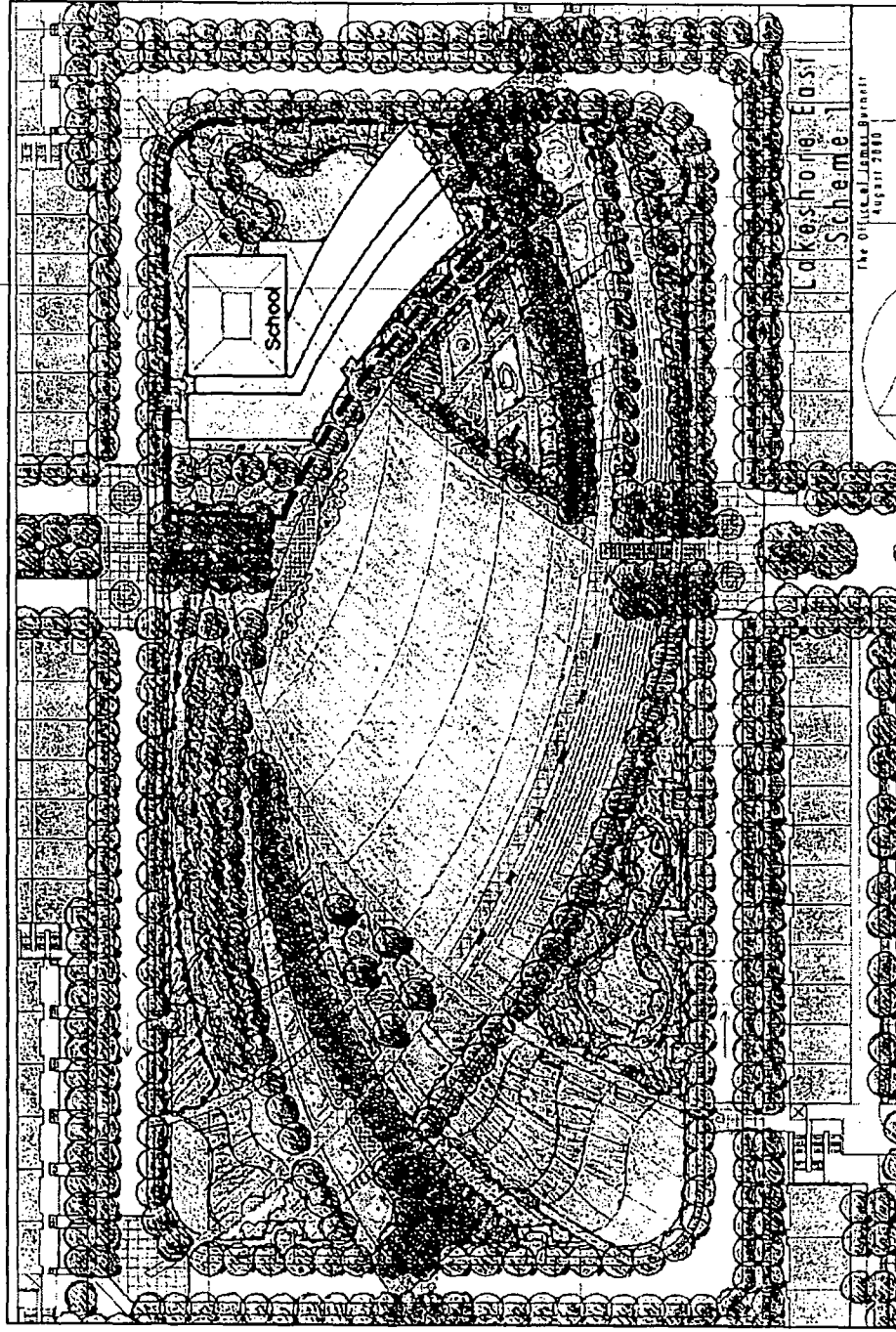
Park Perimeter / Entries

The perimeter of the Neighborhood Park shall be designed to define the park as a public space and will offer sidewalks, seat furniture, lighting and terraces for gathering. A wide landscaped parkway planted with large canopy trees and a continuous 6' wide pedestrian sidewalk should complete the Park Drive streetscape and define the edge of the park.

Access to the park shall be directed to and controlled at key locations through the use of identifiable entry areas. An entry overlook terrace shall be created at the south end of the park near the Field Blvd. intersection to provide a panoramic view of the park. A public stairway and handicap accessible ramp shall be provided at this location.

The eastern and western entries to the park shall be aligned with the vertical connections in parcels E and G.

Pedestrian crosswalks along the Park Drive should be coordinated with these entries and the flow of traffic.



Preliminary Neighborhood Park Concept
(For Illustrative Purposes Only)

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The Office of James Burnett

The Official James Burnett
August 2018

Park Programming Zones

The park shall include a centrally located, large, gently sloping open green space to be used for both active and passive uses.

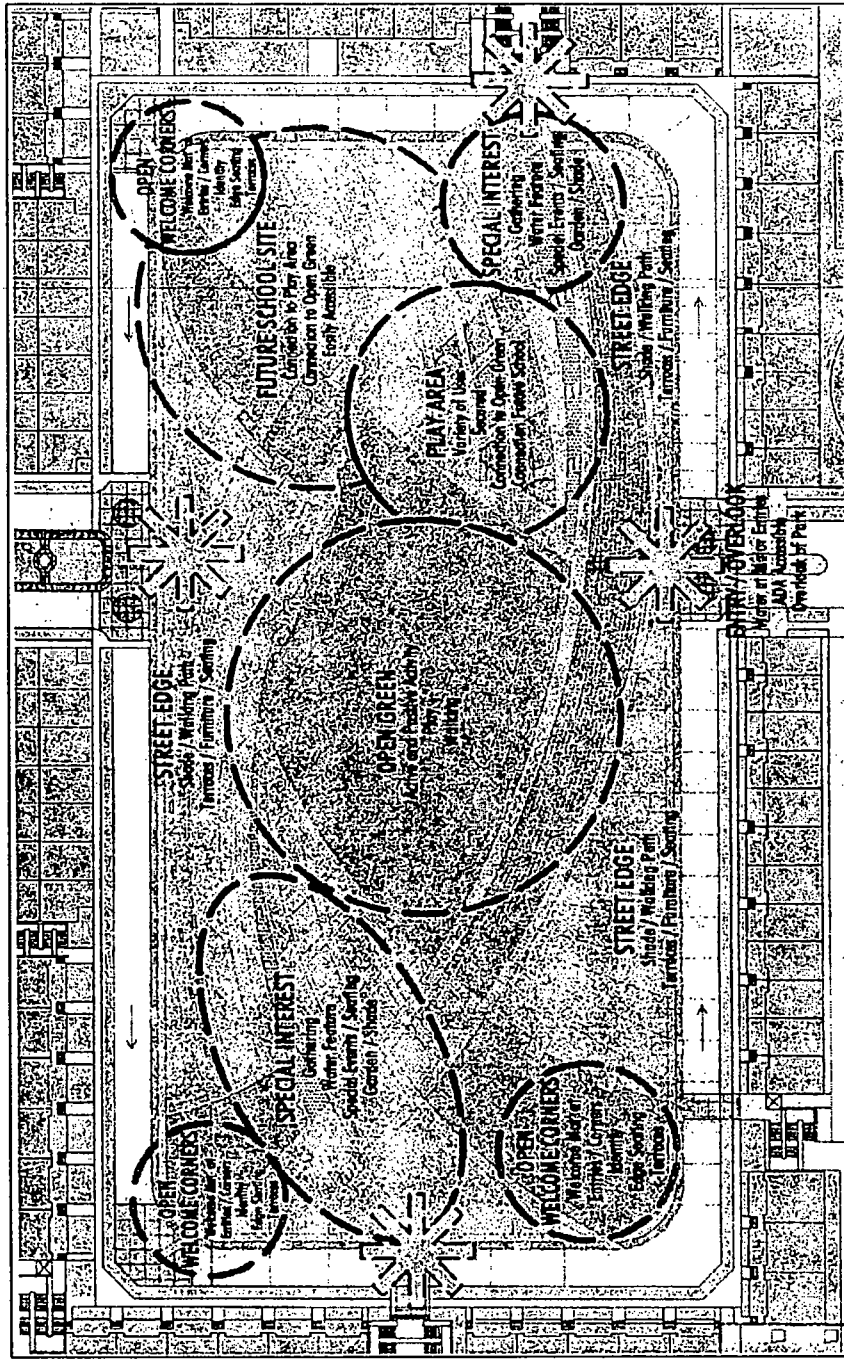
A secured play area shall be provided adjacent to the future school, and offer a variety of play opportunities for several age groups. The play area should also be located adjacent to the open green. The approved park design shall be completed within the first phase of development. The playground shall be placed within the park in a location where it will not be impacted by future school construction.

A centrally located dog park may be provided. The inclusion of a dog park will alleviate the requirement to install individual dog runs/facilities in the individual residential buildings.

A future school zone at the northeast corner of the park shall be designated. Prior to construction of the school, this area shall be designed and built to similar open space standards as the rest of the park for interim open space use. The future school will share space for Chicago Park District facilities.

Topography

The park shall generally slope from front el. +6.00 CCD on the north up to el. +30.00 CCD to the south. The park topography should transition smoothly to the various sloped roadways at the perimeter of the park. Large retaining walls at the perimeter of the park more than 3' high should be avoided.



Park Programming Zones

The Office of James Burnett



PRELIMINARY LANDSCAPE GUIDELINES

The design of the park will be coordinated with the City of Chicago, The Chicago Park District, and the community. The following preliminary landscape guidelines indicate minimum standards for landscape materials.

The design and quality of Landscape Materials within all publicly accessible neighborhood open space areas must be executed with these same minimum standards.

Paving

The location of sidewalks and their particular treatment will be determined by the intended use and the overall nature of pedestrian spaces. Park pathways shall be ADA accessible. Typical sidewalks shall use more basic materials such as concrete. Areas such as the linear promenades, entry ways for residential townhomes/towers, and other special pedestrian walkways could utilize higher quality materials.

Paving materials for pedestrian paths may include, yet are not limited to, the following: concrete, colored concrete, concrete unit pavers, brick, stone pavers, decomposed granite.

Landscape

The landscape is intended to enhance the site by providing flourishing species of diverse vegetation, while also promoting the use of indigenous plant materials. Ornamental planting should be used for local points, small courts or special areas within the park.

The planting specifications for new landscaping should be consistent with the City of Chicago Landscape Ordinance. The following is a brief summary of some additional guidelines for plantings:

Lawn Areas

Active and passive lawn areas will incorporate sod. Ornamental prairie grass or meadows may be used as ornamental beds.

Existing Trees

To the greatest extent possible, the existing trees designated as desirable species shall be protected, maintained and/or relocated into the neighborhood park or other future open space areas.

New Trees

The following minimum guidelines for tree sizes (at time of installation) shall be followed:

- 1) Minimum root ball sizes for trees shall conform to ANLA Standards
- 2) Specimen tree size:
 - a) Minimum caliper: 6 inches
 - b) Height range: 20' to 30' feet
- 3) Shade tree size:
 - a) Minimum caliper: 3 1/2" - 4 inches
 - b) Height range: 18' to 25' feet
- 4) Small upright/ornamental tree size:
 - a) Minimum caliper: 2 7/2 inches
 - b) Height range: 6' to 12' feet

Shrubs and Other Plantings

These minimum guidelines for the following miscellaneous plant types shall be followed.

- 1) Shrubs:
 - a) Minimum container size: 5 gallon
 - b) Height range: 1 1/2 to 3 feet minimum

2) Vines, trailing ground cover or trailing perennials:

- a) Minimum container size: 1 gallon.
- b) Minimum number of runners: 4.
- c) 8 inches minimum length of runners.

3) Clump ground covers or clump perennials:

- a) Minimum container size: 1/2 gallon
- b) crown and roots shall be well developed within container

4) Annuals

- a) Minimum container size: 2-1/2 inch pots
- b) Height range: varies with annual type
- c) Full crown with healthy, vigorous flowers

Landscape Irrigation System

A landscape irrigation system will be provided in all necessary areas for the general maintenance, and up keep of vegetation

Water Features

If water features are to be created in the park, they should be compatible with their setting. For example, smaller, more intimate fountains may help to create a serene space. Larger, more active and participatory fountains can help to draw large gatherings of people and provide a focal point for a larger setting.

The technical design of a potential water feature should incorporate the necessary equipment (i.e. filter, pumps, nozzles, etc.) required for efficient use and desired aesthetic effect. Public safety, wintertime use, and maintenance should also be carefully considered in the design of water features in the park.

Fences

If utilized, fencing in the park shall provide for safety only. Fencing should not hinder public access through the site.

Fencing should be ornamental in its nature in order to enhance the overall aesthetic setting of the park. High quality materials such as ornamental metal, black in color, should be used for fencing. Fencing shall be limited to a maximum height of 48".

Playground Equipment

Playground equipment will be furnished with the latest materials and safety surface to ensure both durability and safety. The equipment should be designed to accommodate a variety of users and age ranges. Playground equipment shall meet ADA requirements.

Park Furnishings

Site furnishings will be supplied where applicable. Furnishings may utilize a number of materials which could include steel, stone, wood, etc.

The design and selection of the Neighborhood Park furnishings including, but not limited to, lighting, benches, bike racks, and trash receptacles shall be coordinated and approved by the City of Chicago and The Chicago Park District.

Structures

Structures within the park setting should be simple, small in scale and in character with the park setting. Small structures such as arbors, pergolas, gazebos, may help to enhance the pedestrian realm and provide shade, a place to rest and create a sense of place.

Park Lighting

Park lighting shall be provided and should incorporate pedestrian friendly ornamental fixtures placed along the park paths. The fixtures shall be spaced at a comfortable distance in order to provide adequate lighting for safety at night.

PARCEL C PARK

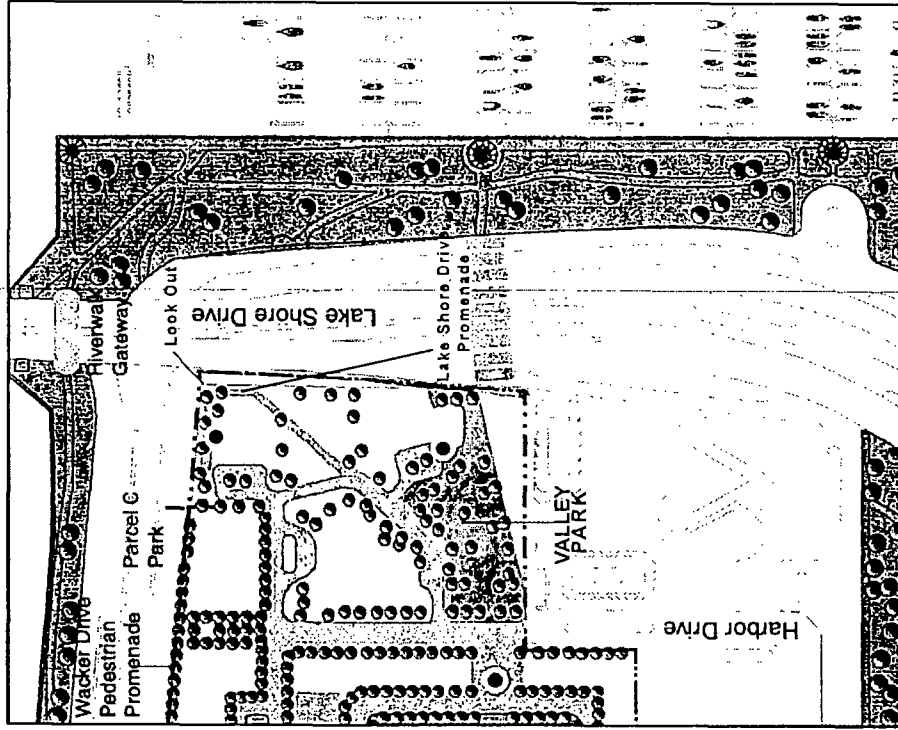
The Parcel C Park shall be an open space located within the future residential area on the south side of the development. A safe and convenient pedestrian access shall be provided front all Parcel C buildings to the internal park.

The park should feature a common green to be used for both active and passive uses. The park should provide walking paths, special paving, seating, and lighting in addition to special ornamental planting. A small children's playground could also be provided in this open space.

PARCEL C LAKESHORE DRIVE PROMENADE

This linear green space shall be designed to provide a publicly accessible pedestrian promenade along the northeastern edge of the development. This 20' wide (minimum) promenade shall be designed with trees, special plantings, lighting, and special paving surfaces. From the northeastern edge of the development the promenade then wraps back into the neighborhood with a pedestrian path leading users to the Parcel C Park.

Special consideration must be given to its relationship to Lake Shore Drive. Its location may be at or above the elevation of Lake Shore Drive. Parking podiums must be screened from view through the use of architectural or landscaped elements.

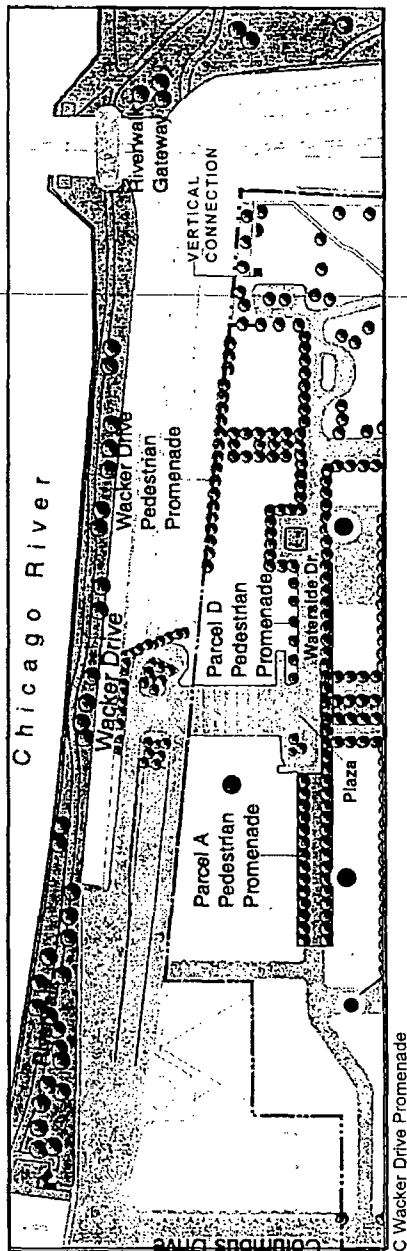


PARCEL B & C WACKER DRIVE PROMENADE

This linear green space shall be designed to provide a publicly accessible pedestrian promenade along the northern edge of the development at the upper level. This 20' wide (minimum) walkway shall be designed with trees, special plantings, lighting, and special paving surfaces.

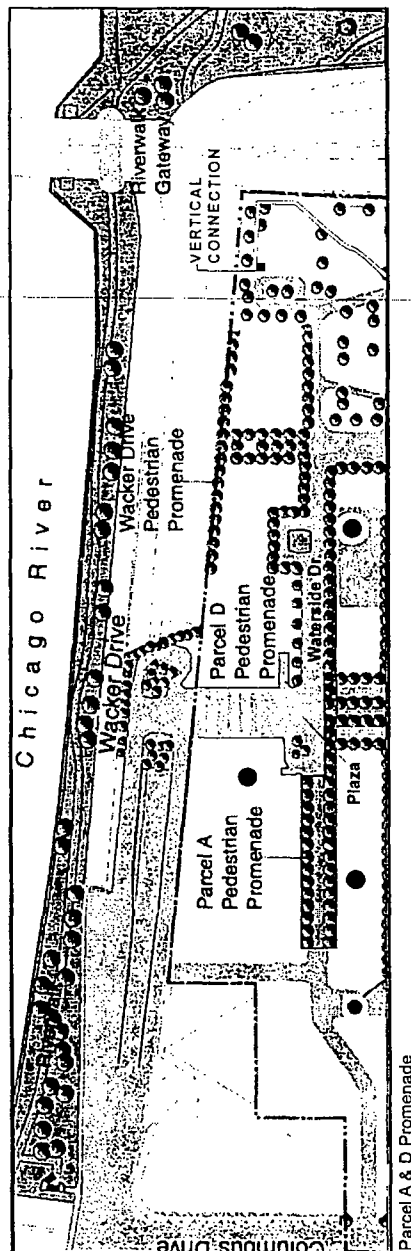
The promenade will also provide vertical access to the Chicago riverwalk and retail space overlooking Wacker drive.

Opportunities for sidewalk cafes and terrace overlooks should be encouraged and integrated into the promenade design.



PARCEL A & D PROMENADE

A linear green space shall be designed to provide a publicly accessible pedestrian promenade between the buildings fronting Upper Wacker Drive & the Park Drive. This promenade shall lead to a plaza spanning Field Blvd. North. Both the promenade & the plaza shall provide an important secondary east west connection at the upper level through the future development. The pedestrian walk shall be designed to accommodate emergency vehicular access.

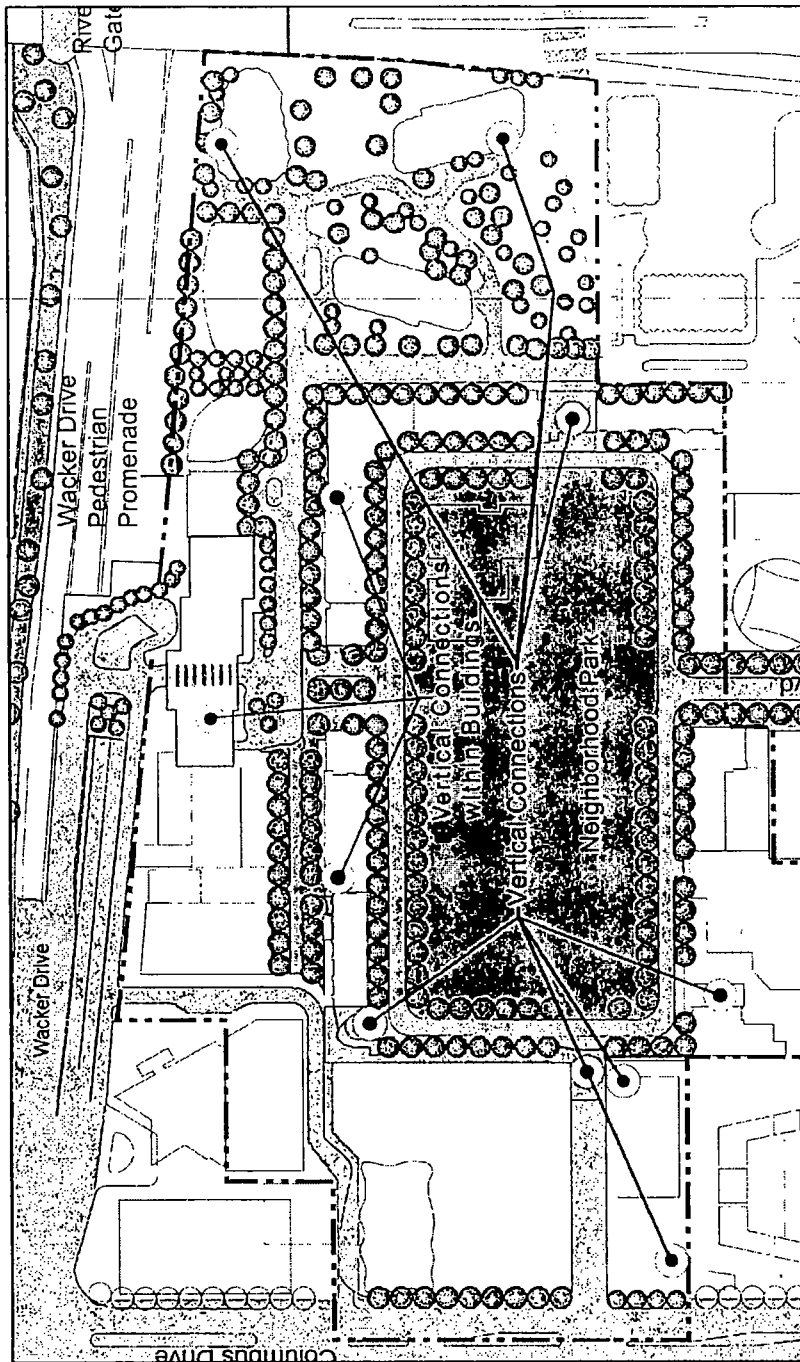


NOTE: These plans are diagrammatic in nature and are only intended to show approximate dimensions and locations.

VERTICAL CONNECTIONS

A minimum of 4 vertical connections shall be created to connect upper level development with the park ring roads. As previously mentioned, one additional connection must be included in Parcel C along Lake Shore Drive. These connections must include stairways and elevators as is feasible.

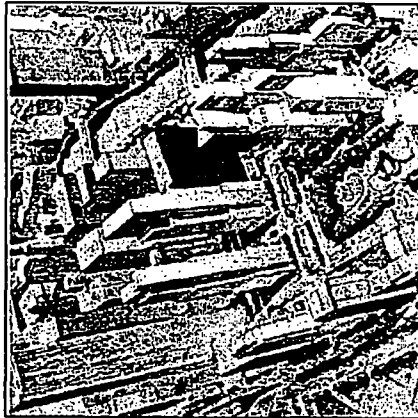
Placed at each edge of the Neighborhood Park, these connections will ensure views into the park from the surrounding areas. These connections must provide attractive and convenient pedestrian friendly access between the upper and lower levels of the district. The design of the stairways shall complement the Neighborhood Park and streetscape design through the use of special paving, lighting, hand rails and planters. These connections shall be treated as landscape features incorporating plantings to help soften this connection between levels.

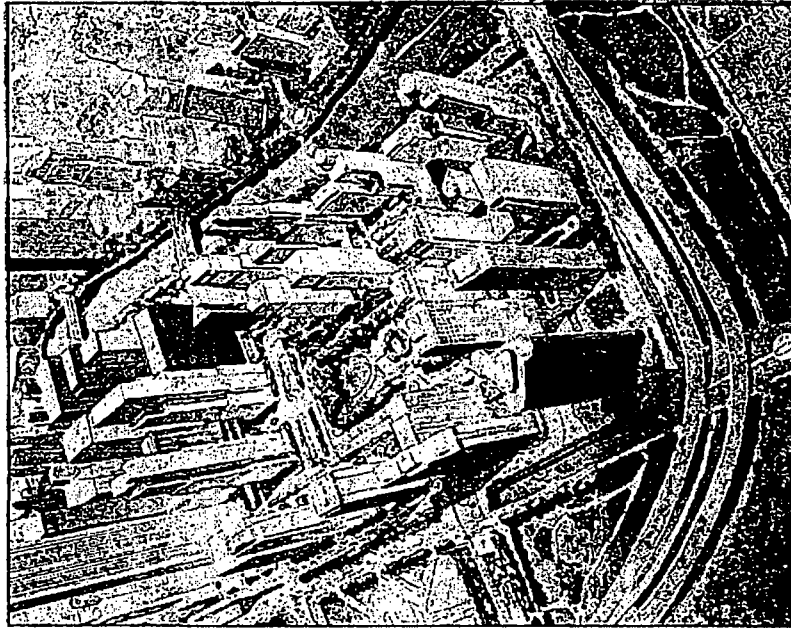


NOTE: These plans are diagrammatic in nature and they indicate the approximate location and the intent of the vertical connection, not the precise locations.

6.0 Design Standards

Introduction
Development Review Process
Controls for Development
Parcel Design Criteria
Building Design Criteria
Building Materials
Environmental Considerations





Introduction

The Design Standards included as part of the Lakeshore East Planned Development submittal are intended to ensure that the basic Lakeshore East Master Plan concepts are realized. These Master Plan Concepts include:

- Create urban street walls that support a diverse pedestrian experience.
- Create an enhanced pedestrian environment that physically and visually connects activities within existing and future buildings to adjacent public areas.
- Preserve views from existing development through new development to the greatest extent possible. Establish view corridors for new development.
- Create a distinctive skyline.

The following standards describe specific design intentions which should be used by the designers, developers and city agencies involved in the building of Lakeshore East.

Development Review Process

The Design Standards are intended to establish a self-policing structure for evaluation of projects which shall be submitted to the Department of Planning and Development for approval, prior to submission to the City for building permit

The developer shall establish a Lakeshore East Design Review Committee for internal review of all development proposals for conformance with the Master Plan. All design submissions should include detailed site plans, building plans, elevations, material samples, and information on lighting and signage. The Design Review Committee may also request additional project information including models or three dimensional renderings.

Any modifications to the Master Plan or Design Standards will require discussion with the City of Chicago Department of Planning and Development and a consensus that the changes do not negatively impact the intent of the Master Plan. Any modifications deemed significant will require an update to the Master Plan & Partial PD Amendment.

Final for Publication

Controls for Development

The Lakeshore East Master Plan establishes seven development parcels organized around a neighborhood park. The parcels have been defined to accommodate the projected long term build-out. The parcels have also been divided into sub-parcels to permit flexibility in responding to specific development proposals. The sub-parcel divisions may adjust slightly in order to accommodate building design refinements over time.

The Master Plan envisions a mix of highrise, midrise and lowrise development. The development of each parcel within the Master Plan is governed by a set of site design criteria and building envelope controls. Site design criteria include pedestrian and vehicle access. The building envelope controls mandate the placement of buildings within the parcels, setting a pattern of development that creates a strong sense of place at the pedestrian level. Two distinct sets of controls, building mass criteria and streetwall criteria, are defined for each parcel. The building envelope controls have been created in a way to allow some design flexibility within each sub-parcel. Also, the Department of Planning and Development may administratively allow minor modifications or adjustments to the building envelope controls.

Maximum Developable F. A. R.

Within Subarea E, a maximum developable area has been established. This maximum developable area will not fill the entire allowable building envelopes, but may be shifted from one parcel to another parcel as long as the maximum building envelopes are maintained.

For the purpose of calculating maximum developable F.A.R. area, only the inhabitable area at or above the building entry shall be included

Parcel Design Criteria

Pedestrian Access

A well-connected pedestrian network is critical to successfully creating an urban neighborhood in Chicago. The maximum allowable distance between two pedestrian walkways along a single blockface should not exceed the length of a typical downtown Chicago block or approximately 320'. Midblock pedestrian walkways which pass through development parcels are also encouraged

Vehicular Access

The multi-level plan of Lakeshore East should accommodate the bulk of parking and service below grade. Curb cuts should be kept to a minimum and the use of shared curb cuts along shared property lines is encouraged. Curb cuts should not exceed 20 feet in width. Parking entries should be integrated with the building's architecture through the use of similar materials, patterns, textures and color. All parking above the street level must be screened with occupied space.

Service access is permitted at the lower level only.

Emergency Vehicle Access

Required emergency vehicle access easements are identified on the parcel diagrams.

Interim Uses

Undeveloped parcels may support interim uses such as recreational open space, dog friendly areas,

and surface parking. Interim use plans will be reviewed by the Department of Planning and Development. They should identify all temporary improvements and timeframe of interim use.

Utility Easements

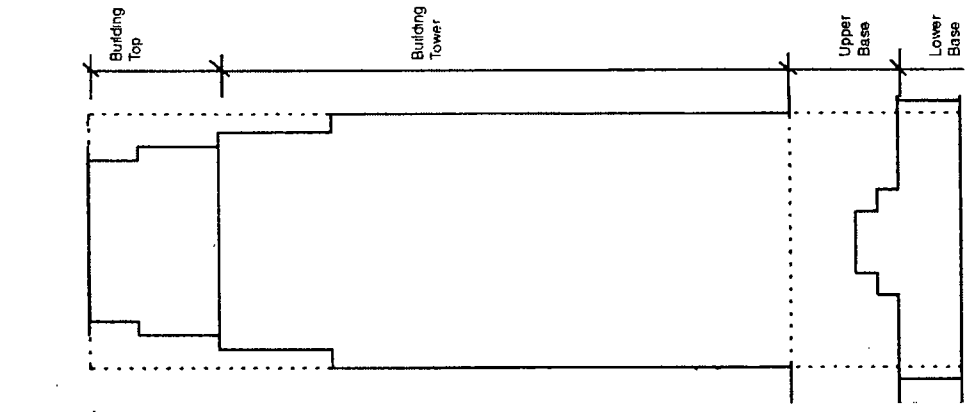
Utility easement corridors, varying in width and Dedicated and Conveyed to the City of Chicago below approximately +20.92 C.D. currently exist throughout the site. As future development fills in the site, the existing utility corridors will generally occur at the lowest level of the parking structure. A service roadway, generally at elevation +6 C.D., will typically occur along the easements and intermittent structural columns/caissons for the structures above may fall within the easement corridors. The placing of structural easements within existing easements must be coordinated with existing and future utilities

These utility easement corridors must be accessible to all utility entities, and will also have controlled accessibility to building service vehicles and to adjacent parking structures.

Existing utilities will either remain in place, or be relocated into the new, final utility corridors. New utilities will also occur within the new dedicated roadways on grade throughout the site.

Final for Publication

Maximum Building Envelope



Highrise Building Zones

Highrise Building Design Criteria

Highrises are characterized by their three elements: the building base, the tower and the top. Transition lines should be articulated at the two or three story level lower base, between the base and tower, and between the tower and top. The transition lines may be achieved by a change of material, a setback, a reveal or a cornice line.

Building Base

It is important that the base of the building extend to the sidewalk to define the urban street environment and support a close relationship between activities in each building. Special streetwall standards for the base include:

Setbacks and Building Lines

The building must extend to the defined edge of the sidewalk (specified by property line, easement or setback) for at least 80% of the frontage. The building may setback a maximum of 10 feet for up to 25% of the frontage from the defined edge of sidewalk if the additional setback is designed to include sidewalk cafes, seating, public art, landscaping or other amenities.

Ground Floor Active Building Uses and Transparency

Active uses that are visually connected to the pedestrian environment are encouraged along the ground floor of the building. These active uses may include retail, lobbies or office space. The ground level facade of the building should be transparent to allow viewing into the interior of the building. Blank walls are not encouraged.

Building Envelope

Building mechanical floors may extend beyond the maximum building envelope. Maximum building envelopes are identified for each sub parcel.

Building Tower

Generally, the preferred location for highrise towers should be toward the primary street frontage.

Building Top

A varied and distinctive skyline is desired. The building top should provide special architectural treatment to express building identity. Vertical design elements, such as spires, should be integrated with the design of the building tower and top. The height of a vertical design element may extend above the maximum building height, and is subject to administrative approval by the Department of Planning and Development.

All rooftop mechanical systems including communication devices should be concealed from view within an architectural enclosure

Building Separation

To preserve views, a minimum separation of 40' between non-party walls is required. Building projections, such as balconies or bay windows, may not extend into the minimum building separation.

Building Height

Building height is defined as the vertical distance from the curb level opposite the front of the building to the top of the highest occupied floor of the building. For purposes of identifying occupied spaces, the following areas shall be excluded: mechanical rooms, elevator machine rooms & overruns, rooftop deck access rooms, habitable areas comprising less than 20% of the total room area. Maximum heights, identified in total feet, for the building base, tower and top are identified for each parcel.

Townhouses

It is important that the townhouses create a continuous streetwall that frames the park and special pedestrian ways. It is also important that the townhomes along the Park Drive screen all existing and future podiums from view. Special townhouse standards include:

Setbacks and Building Lines

The design of the townhouse podiums shall include a minimum landscape setback of 5' from the sidewalk (specified by property line, easement or setback) East of Field Blvd. South, along the East / West segment of Park Drive, the minimum podium setback may be 3'. Entry stairs may extend into the landscape setback. Townhouses within a particular grouping along the same street frontage should maintain a similar setback from the side walk & from their podium.

Orientation

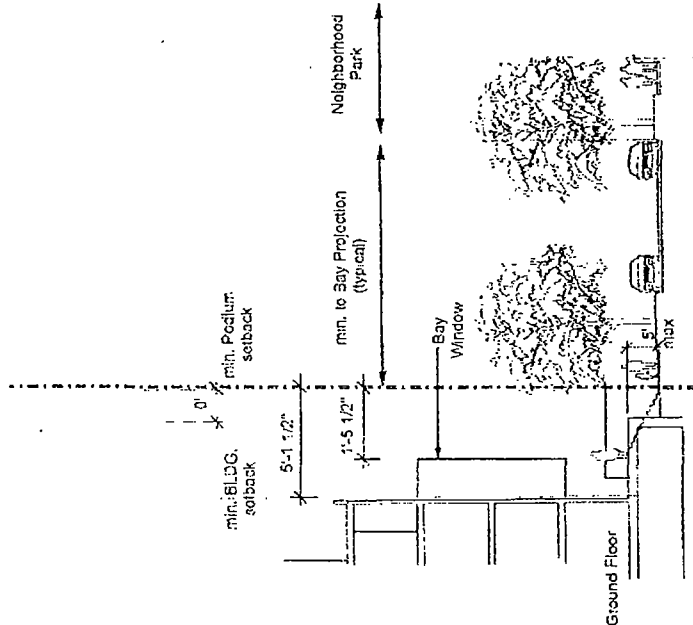
Townhouse entries should be oriented to the street. The townhouses should be designed to include living space at the ground floor to ensure "eyes on the street". Stacked townhouses which have two street frontages should address both streets.

Variety

Variety in the use of materials and articulation is encouraged within groupings of townhouses, in order to create an aesthetically diverse neighborhood setting.

Building Height

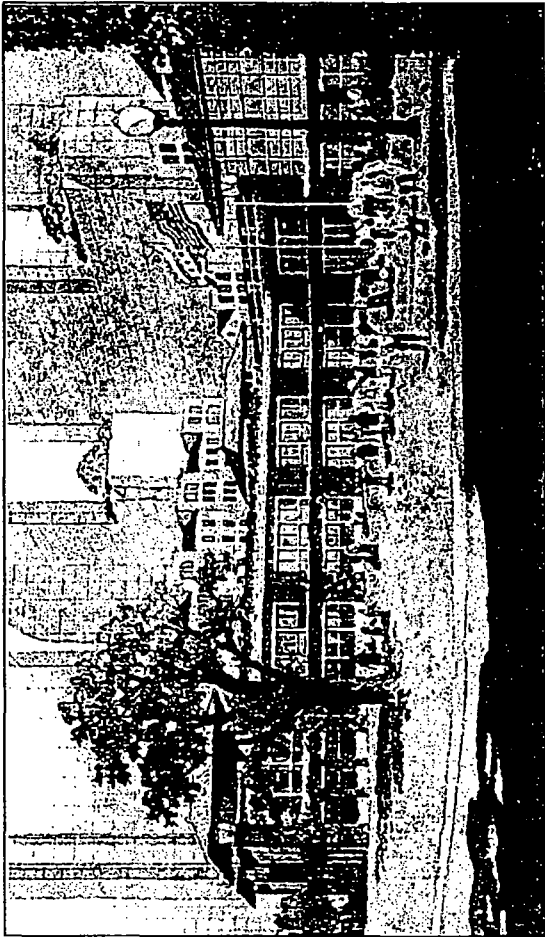
Building height is defined as the vertical distance from the curb level opposite the front of the building to the highest point of the townhouse roof, if extending beyond the maximum building height, roof top enclosures may not exceed 10% of the rooftop area and should be architecturally integrated with the design of the townhouse roof. Maximum heights for the townhouses are identified for each parcel.



PARCEL G1

----- Property Line

NOTE: These plans are diagrammatic in nature and are only intended to show approximate locations and dimensions.



School Building

The scope of the work for the 45,000 sq. ft. elementary school will include the site work required to prepare the site for construction of the proposed building. The developer shall also provide the "core and shell". It will include all cast in place concrete structural steel, steel joists, and roofing. Additionally, it will include complete exterior enclosures with windows, storefront, glass and glazing, exterior masonry, and exterior doors. Finally, it will include all utilities to a location within the building such as a pump room, or mechanical space. The "core and shell" of the building can be further defined by the following specifications sections from the Chicago Public Schools Project Manual dated 1999 / 2000 (a copy of which is on file at the Department of Planning and Development)

Demolition	02060
Site Clearing	02212
Excavation and Back filling	02221,02222
Water Distribution	02510
Sewers and Site Drainage	02700
Water Service	02707
Exterior Structure including	
Cast in Place Concrete	03300
Concrete Finishes	03312
Unit Masonry - Exterior Only	04200
Cast Stone - Exterior Only	04720
Structural Steel	05120
Steel Joists and Girders	05220
Steel Deck	05310
Metal Fabrications - Exterior Only	05500
Expansion Joint Assemblies - Exterior Only	05810
Bituminous Dampproofing	07160
Spray on Fireproofing	07250
Modified Bituminous Sheet Roofing	07257
Firestopping - Exterior Only	07270
Manufactured Roof Panels	07410
Flashing and Sheet Metal - Exterior Only	07500
Roof Accessories	07700
Joint Sealers - Exterior Only	07900
Steel Doors and Frames - Exterior Only	08110
Overhead Colling Doors	08330
Aluminum Entrances and Storefronts	08410
Aluminum Windows	08520
Exterior Metal Window Guards	08661
Glass and Glazing - Exterior Only	08800

Building Elements

Special consideration will be given to the following building elements during the development review process.

Landscape Roofs

Green roofs, or roofs that are covered with planting, can provide great benefits to the building as well as the urban environment and are encouraged. Green roofs may be used to reduce the building's energy costs and also reduce stormwater runoff

A majority of the defined green roof area should be covered by vegetation. Public access is not mandatory but maintenance access should be maintained. Future installations, such as mechanical equipment, should not have a negative impact on the landscaped area. Drought resistant indigenous plants that require minimal maintenance are recommended.

Arcades

Arcades are allowed, but must be continuous along the entire length of the building frontage from one street right of way to the next street. Minimum width is 8 feet clear, maximum width is 20 feet and minimum clear interior height is 14 feet. Arcades shall have a minimum decorative pavement treatment, and landscaping, seating or other public amenities as appropriate. Arcades shall not contain parking spaces, passenger drop-offs, ramps, loading berths or any other vehicular use, or trash storage facilities.

Through Block Pedestrian Connections

Indoor and outdoor through block pedestrian connections are encouraged.

Enclosed Through Block Connections

The connection should provide a continuous unobstructed corridor of at least 20 feet in width and the unobstructed height must be a minimum of 15 feet. Material finishes and lighting should be selected to ensure a pleasant and safe pedestrian connection.

Exterior Through Block Connections

The connection should be a continuous unobstructed space at least 20 feet in width. A minimum clear height of 20 feet is required where the connection is not open to the sky. Building elevations that border the connection should include active building uses where possible or be treated with plantings, murals and other architectural details.

Balconies

Balconies are permitted but must be integrated within the design of the building facade.

Bay Windows

Bay windows are allowed and may extend above a ground level landscape setback but may not extend into the minimum building separation.

Canopies and Awnings

Canopies and awnings are permitted for retail development and building entries. Canopies and Awnings should be integrated with the design of the facade. Materials should be high quality and may be either fixed or retractable. They must meet all

applicable City Codes and regulations.

Exhausts

Exhaust vents should not be located on a building's primary street frontage

Signage

Signage is to be kept to an absolute minimum. The design of signs should be compatible with the building and street frontage. High quality and durable materials are encouraged. Signs should be mounted in locations that respect the design of the building, including arrangements of bays and window openings. Fixtures for externally illuminated signs should be simple and unobtrusive.

Signage at building tops is limited to hotel uses and is subject to administrative review and approval of the Department of Planning and Development. Signage must be integrated with the design of the building.

Building Lighting

Lighting may vary within the Lakeshore East Development. Public spaces should be adequately lit to ensure a safe pedestrian environment.

Lighting used to highlight architectural features such as articulated building tops or building entrances is encouraged. Such architectural lighting should be planned to not visually impact adjacent buildings or public spaces.

Radio Antennae

Radio antennae may be allowed on Parcels G and D but must be integrated with the design of the building.

Parking

Parking Entrances

Parking garage entrances should be designed as an integrated element of the building's facade through the use of similar materials and detailing. Driveways and curb cuts should be compatible in material and detailing with adjacent sidewalks

Perimeter Walls

All Parking at or above the upper street level must be screened by occupied space. Parking along the lower and intermediate levels may extend to the parcel line, however the parking must be architecturally screened when it is adjacent to a public accessway, such as intermediate Wacker Drive and Lake Shore Drive. Along Field Blvd. North parking must be screened by occupied space at the lower level, and architecturally screened above the lower level. Along the Park Drive, the townhomes and other residential buildings will screen all existing and future parking podiums from view.

Ventilation

Parking garage ventilation should be well designed, with openings which should incorporate architectural screening. Ventilation structures should be designed as integral components of adjacent buildings or open space.

Building Materials

The selection of appropriate building materials should be made with a concern for the existing context.

All Glass curtain-wall buildings are discouraged and highly reflective glass will not be permitted. If all glass-curtain-walls are used, high quality, low-e or clear glass should be used.

Two grades of building material quality, Grade 1 and Grade 2, have been identified in the Master Plan.

- Grade 1
 - Natural Stone
 - Brick
 - Architectural Precast
 - Glass and Metal Curtain Walls

- Grade 2
 - Grade 1 Materials preferred
 - Exposed Structural Concrete allowed with architectural detailing and clean, high quality construction
 - formwork (such as steel forms or plastic faced plywood)

Generally, Grade 1 materials should be used for signature buildings at the perimeter of the site and Grade 2 materials may be used for buildings on the interior of the site. Unique materials and colors are encouraged along the ground level, at building entrances and on the rooftops.

In general, whether Grade 1 or Grade 2 materials are used, a high quality level of finishes and details is important.

Building Bases

Grade 1 materials should be used on the building bases and the townhouses. Clear or lightly tinted glass is preferred. Retail storefronts may include ornamental metals.

Building Towers

The portion of the building above the base should use materials that are compatible with the material used for the base and also be complementary with neighboring buildings. Towers identified as signature buildings should use Grade 1 materials, however, the level of articulation and detailing may be simplified. Towers identified as Grade 2 may use exposed structural concrete clear or slightly tinted glass is preferred. Ornamental metals may be used for building accents.

Building Top

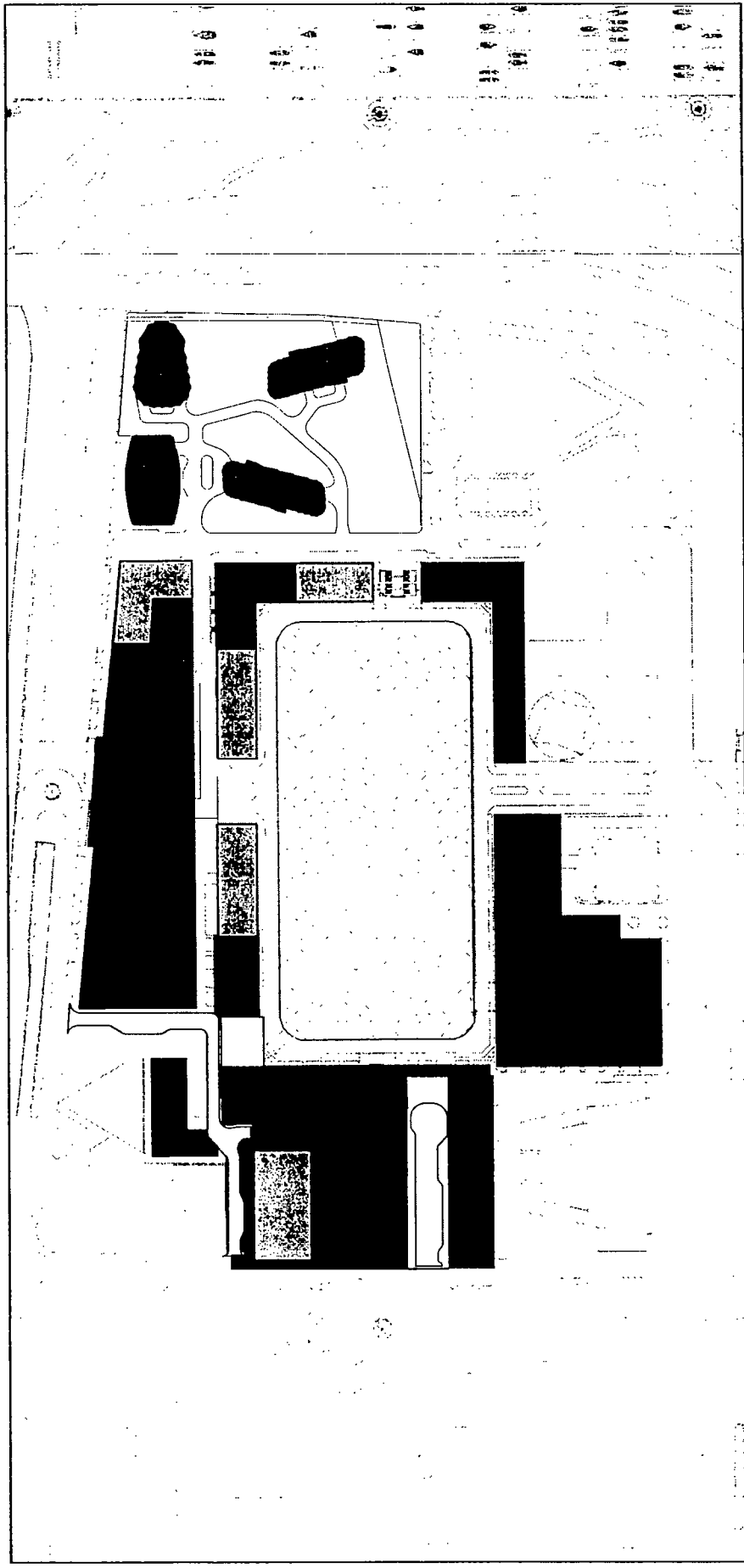
The materials used for the building tops should be of high quality and present a distinctive image for the building.

Material Color

Material colors should minimize differences between adjacent parcels. Predominantly dark buildings are discouraged

Stone, brick and concrete colors should be light to medium warm neutral colors.

Glass colors may include clear panes or slightly tinted gray, silver, green or blue.



Base* - Grade 1 Materials
Tower - Grade 2 Materials (where applicable)
Base* - Grade 1 Materials
Tower - Grade 1 Materials (where applicable)
*Refer to parcel diagrams for height of base material

NOTE: These plans are diagrammatic in nature and are only intended to show approximate locations and dimensions.

Environmentally Conscious Design

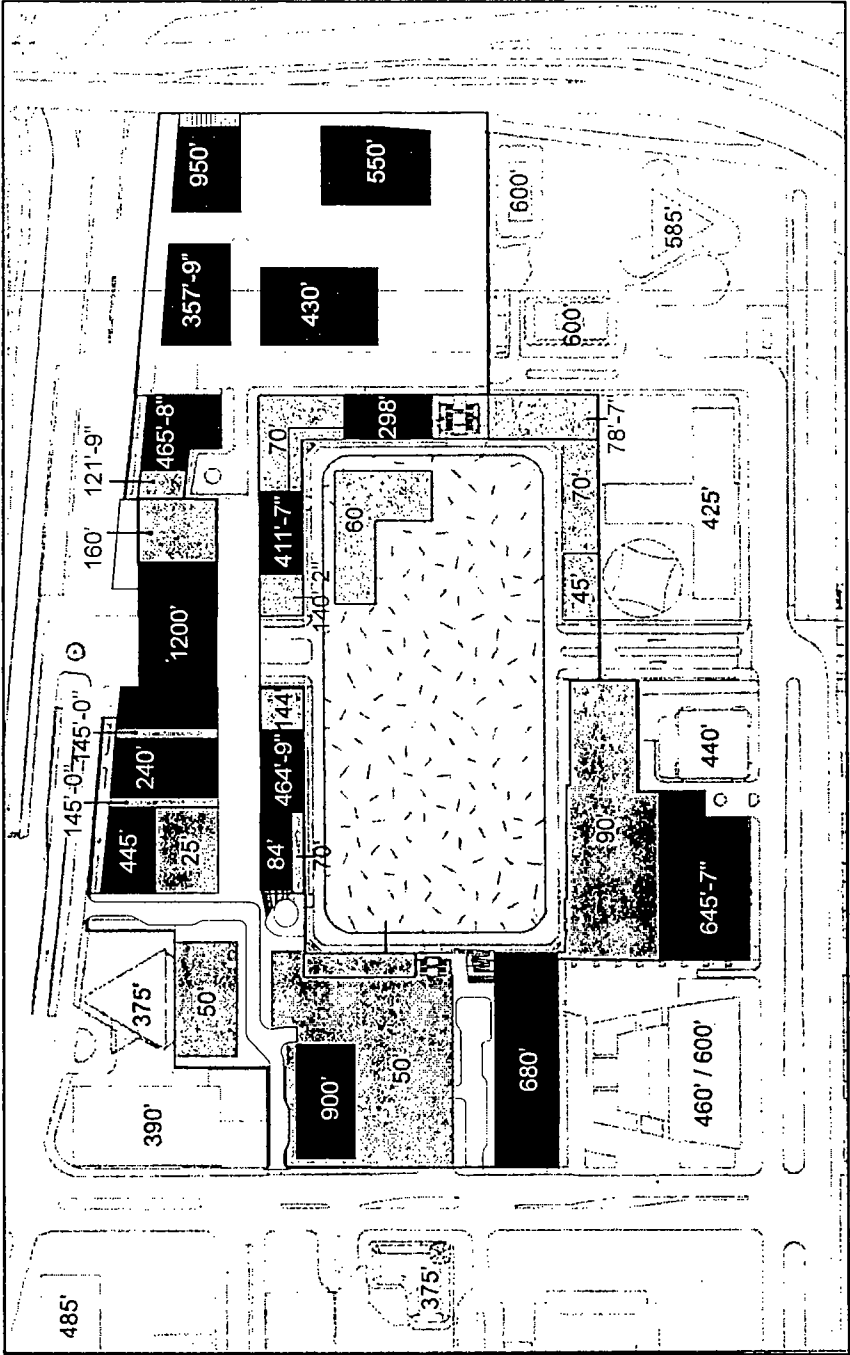
The Lake Shore East Development is a significant opportunity to showcase environmentally sensitive design. The following recommendations can be used as a checklist to evaluate and ensure an environmentally sensitive approach to building a neighborhood.

Site Design Recommendations

- Orient buildings to optimize solar exposure.
- Buildings near the Lakefront should reduce the amount of curtain wall in an effort to reduce the injury to migratory birds due to the transparent nature of the facade
- Utilize indigenous landscape materials, with an appropriate diversity in plant species.
- Properly manage stormwater runoff
- Relocate and reuse existing trees.

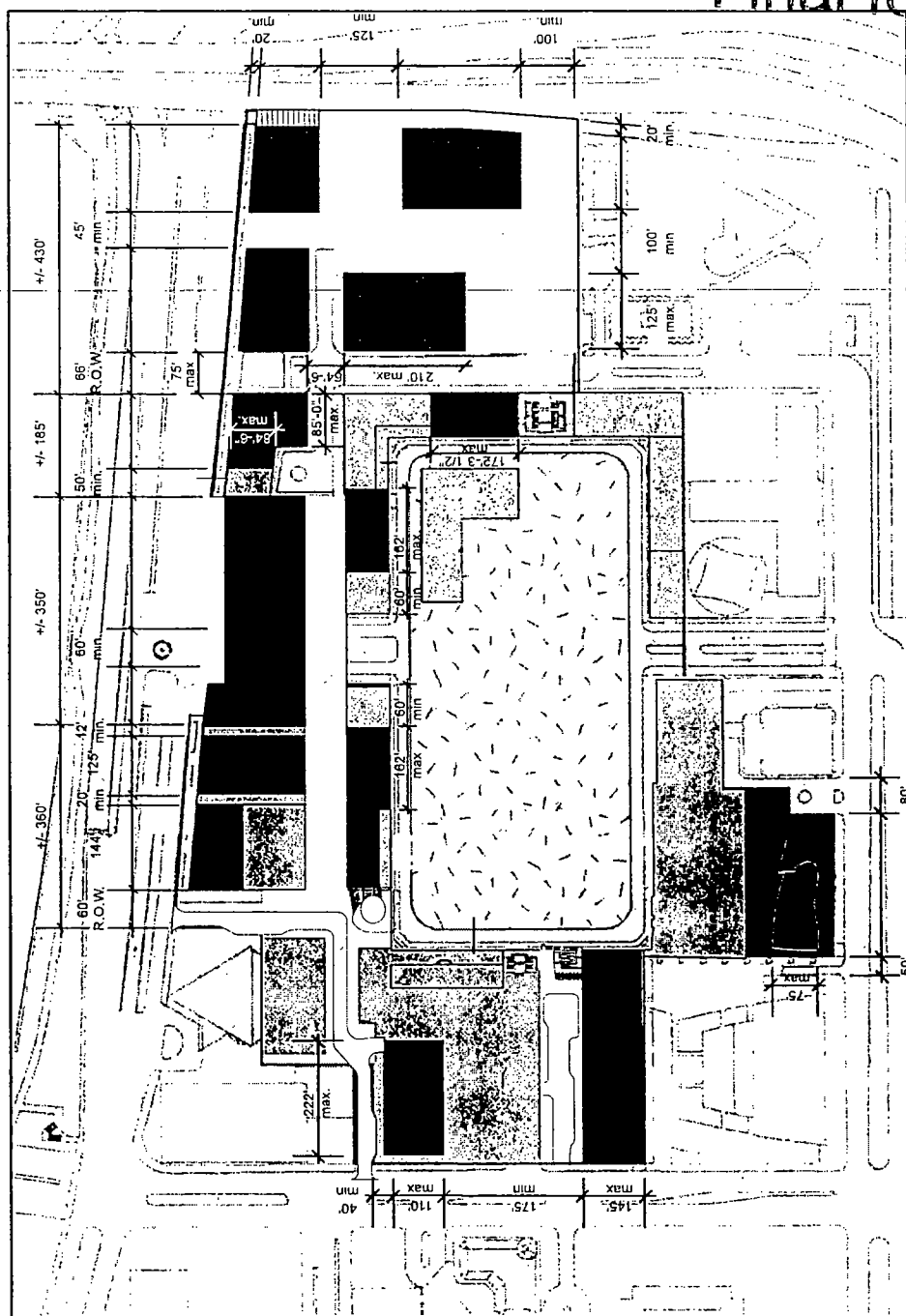
Building Design Recommendations

- Coordinate programmed areas that will benefit from sun exposures in appropriate zones within the building.
- Manipulate building envelopes that will respond to climate and orientation.
- Consider the integration of photovoltaic panels and/or fuel cells for electric generation.
- Encourage operable windows that provide fresh air to interior workspaces and living spaces
- Utilize energy efficient building systems
- Include "landscape roofs."
- Develop exterior and interior shading devices that minimize heat gain.
- Utilize exterior and interior "light shelves" that bring daylight into the interior zones and workspaces of the buildings.
- Minimize the use of hazardous or "off gassing" materials, specifically materials with volatile organic compounds (VOC's).
- Employ wood products harvested from certified forests.
- When possible, select materials based on life-cycling costs.
- Develop lighting controls that manage energy consumption including task lighting, daylighting, an energy efficient artificial lighting
- Coordinate a tenant recycling program with the City of Chicago's recycling program.



NOTE: Elevations reflect height of buildings above average grade, unless specified as CCD.
NOTE: These plans are diagrammatic in nature and are only intended to show approximate locations.

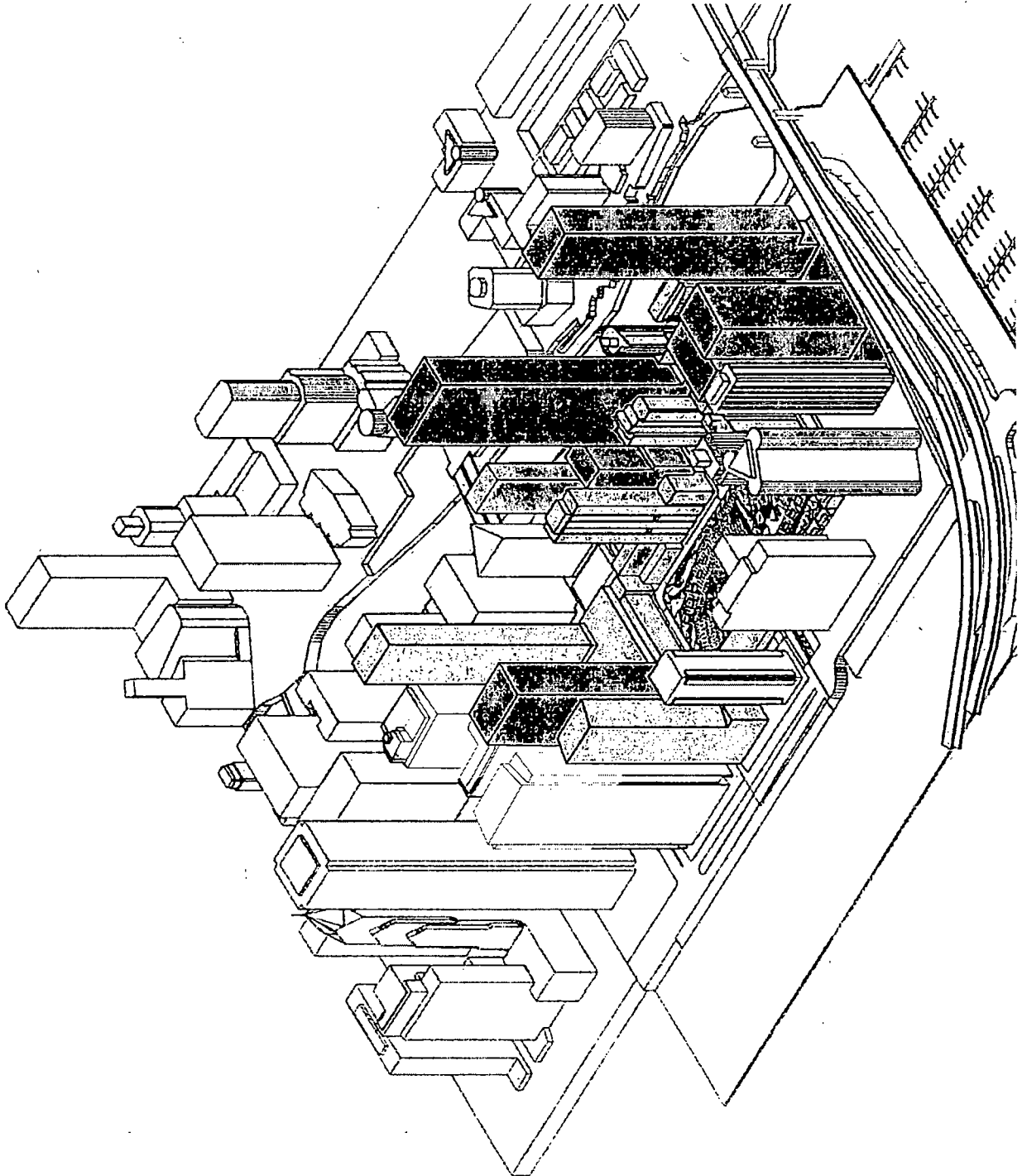
Highrise Buildings -
Maximum Building Envelopes above 20'



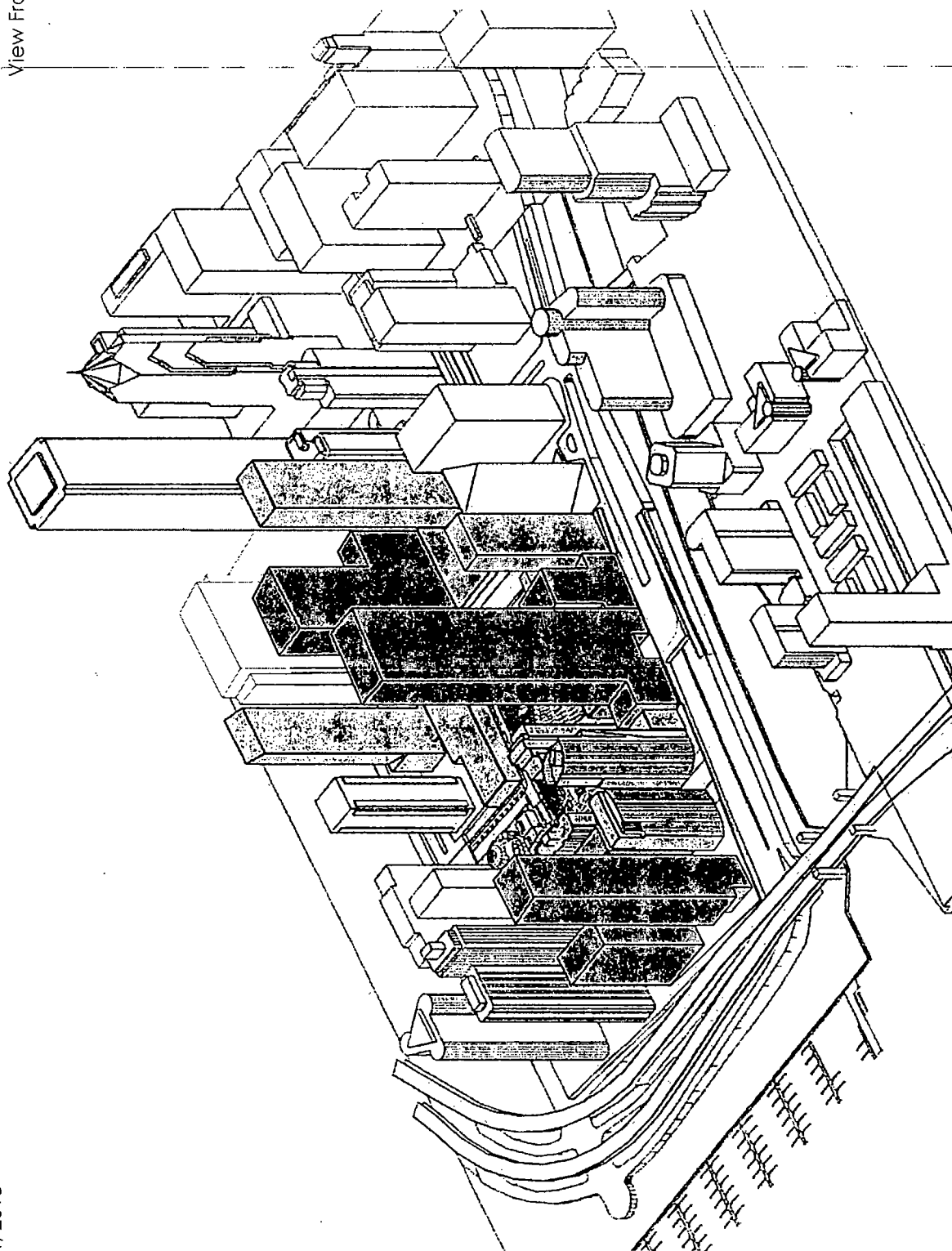
NOTE: All overall dimensions should be verified with land survey

NOTE: These plans are diagrammatic in nature and are only intended to show approximate locations.

Maximum Building Envelope
View From the Southeast

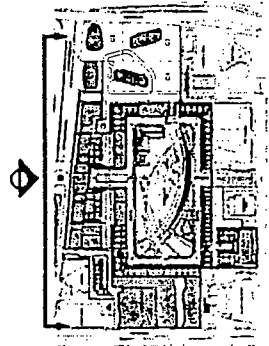
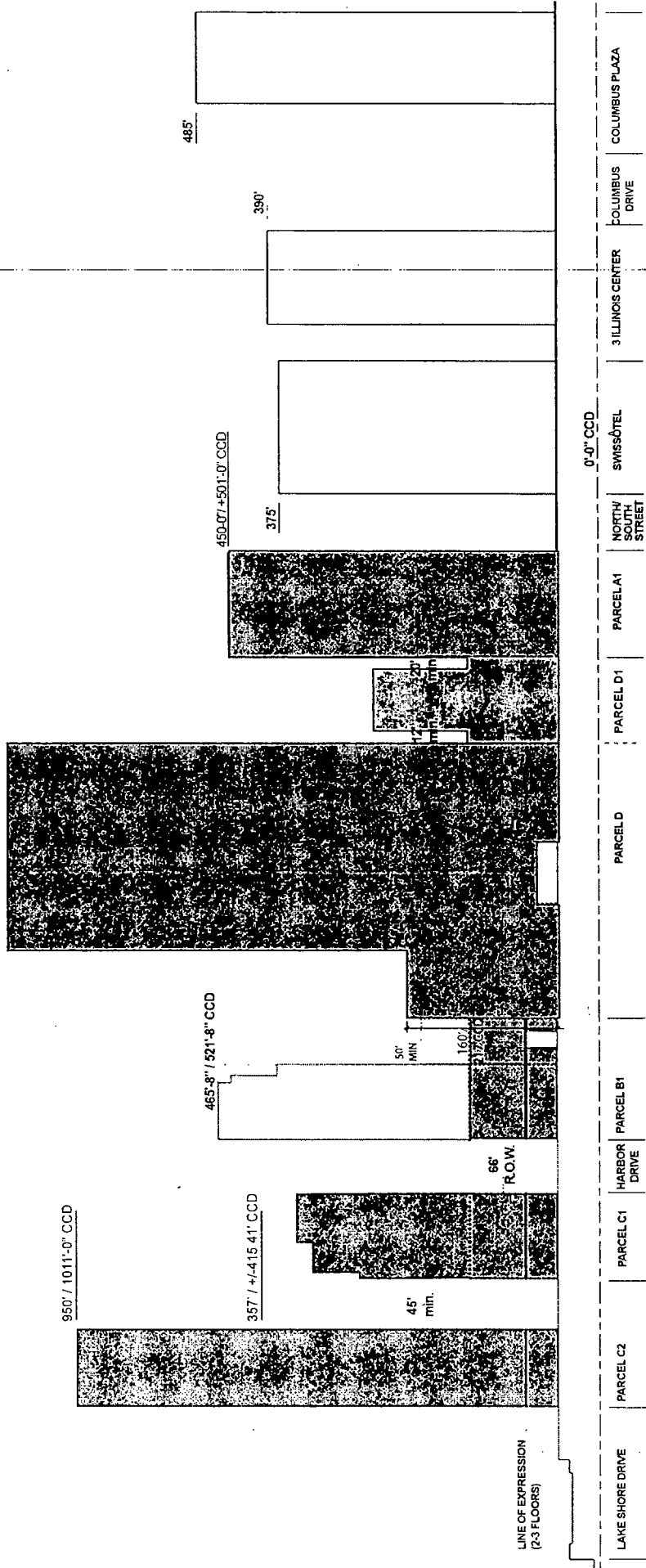


October, 2018



October, 2018

Illustrative Elevation
Wacker Drive



NOTE: These plans are diagrammatic in nature and are only intended to show approximate locations

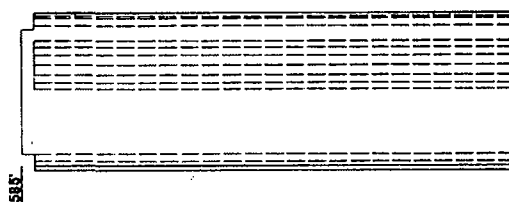
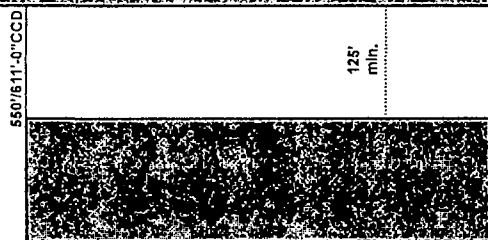
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October, 2018

Illustrative Elevation
Lakeshore Drive

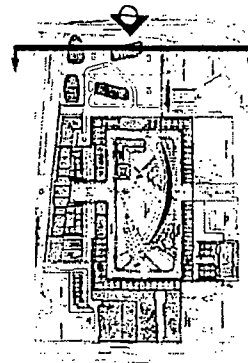
950/1011'-0" CCD

Maximum Building Envelope



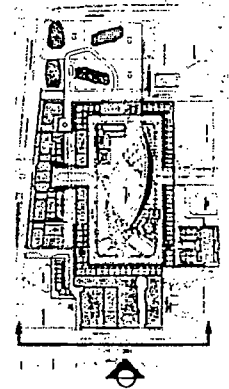
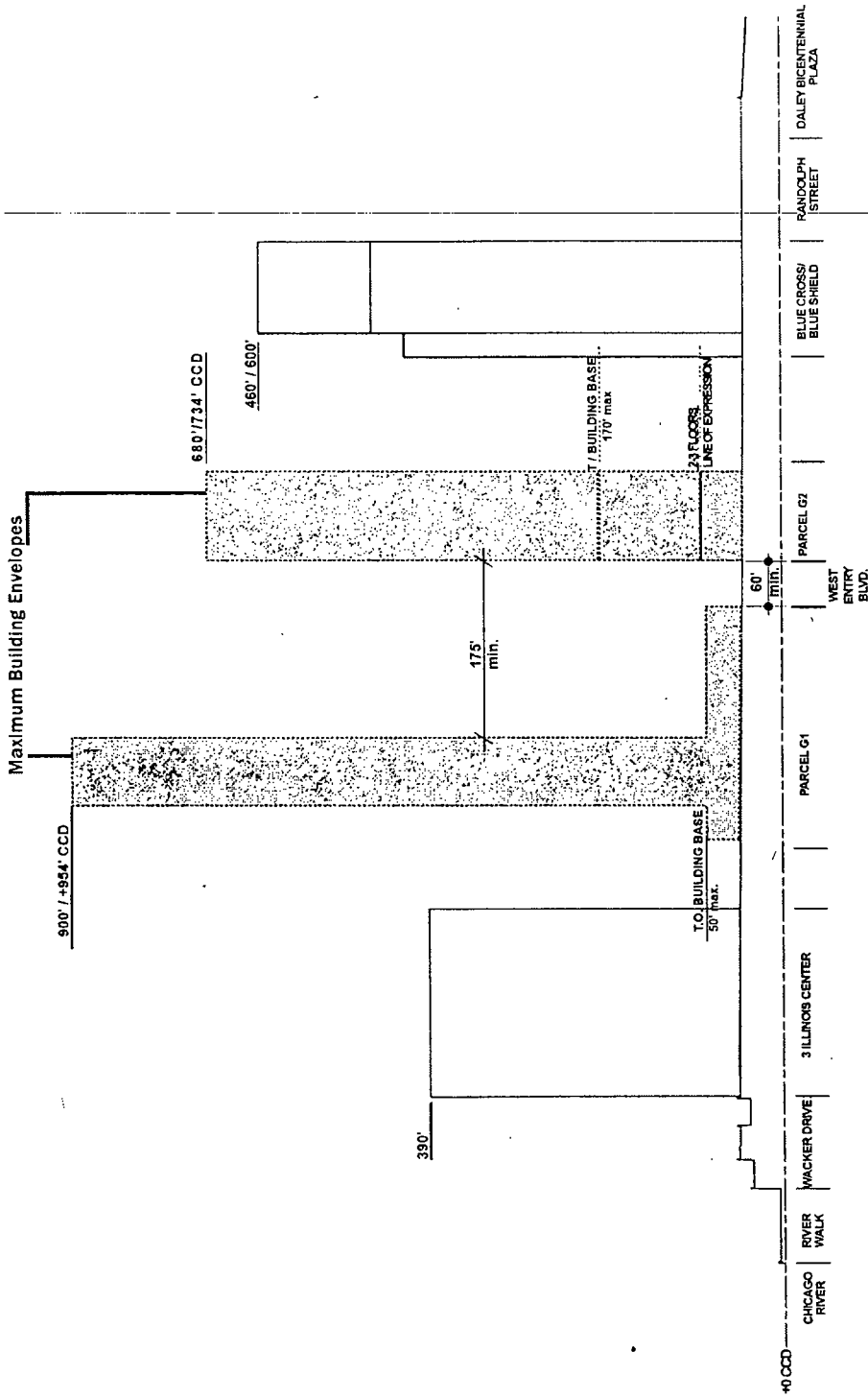
+0 CCD

DALEY BICENTENNIAL PLAZA | INTERMEDIATE RANDOLPH STREET | HARBOUR POINT | PARK SHORE TOWERS | PARCEL C3 | PARCEL C2 | WACKER DRIVE | RIVER WALK | CHICAGO RIVER

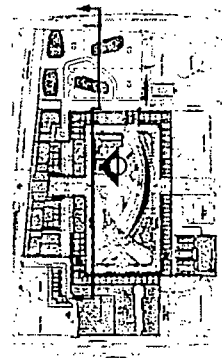
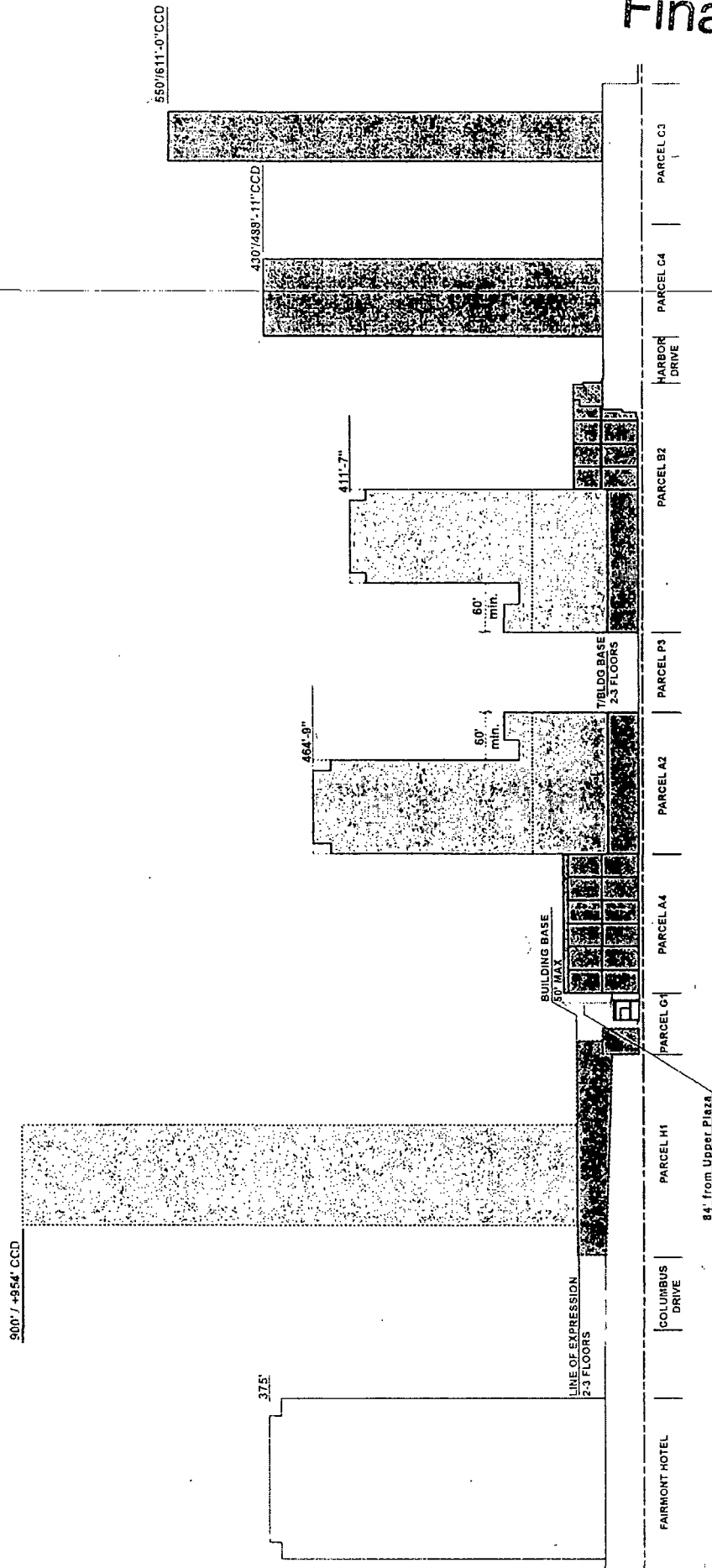


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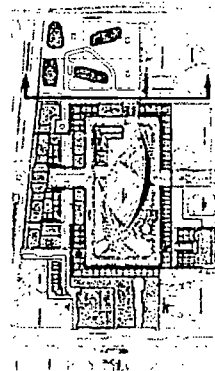
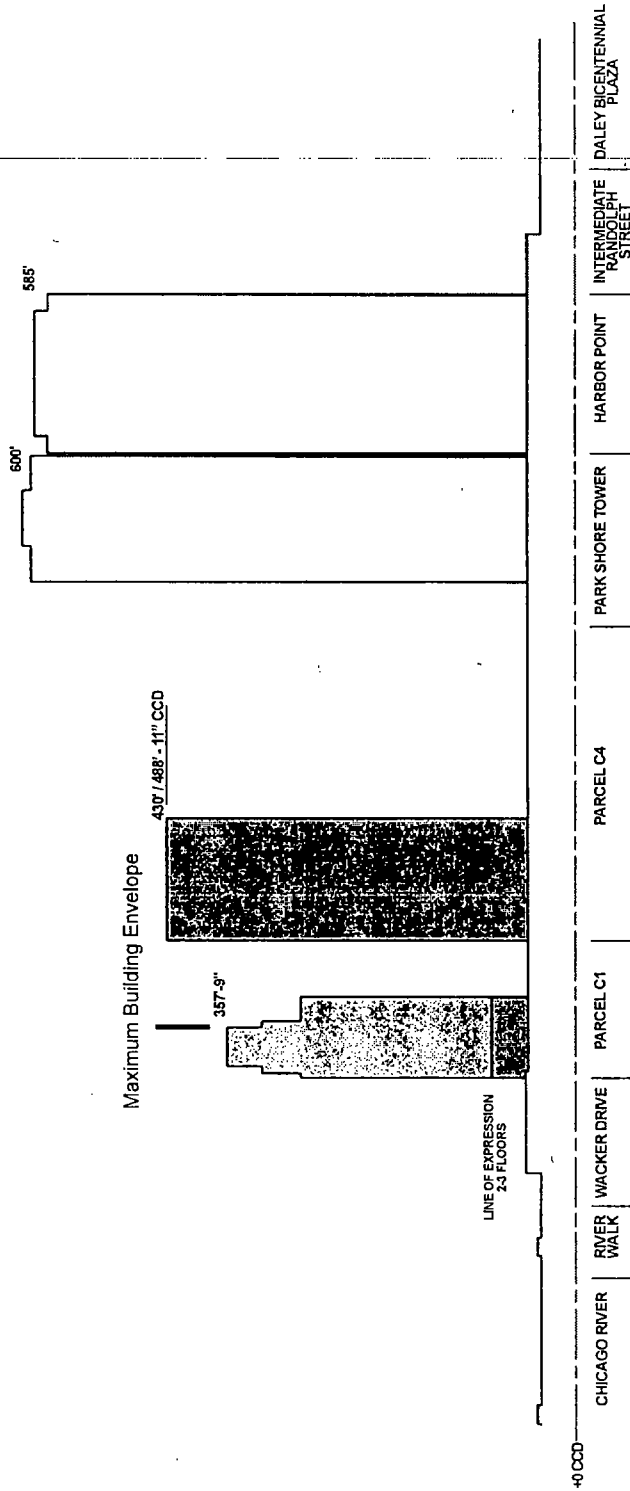
NOTE: These sections are diagrammatic in nature and are only intended to show approximate locations



NOTE These sections are diagrammatic in nature and are only intended to show approximate locations.

Illustrative Elevation
Harbor Drive:
View Looking East

October, 2018

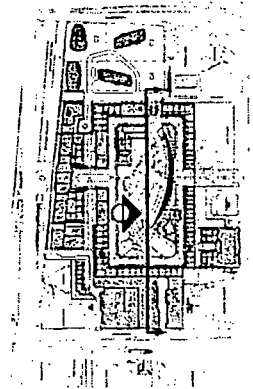
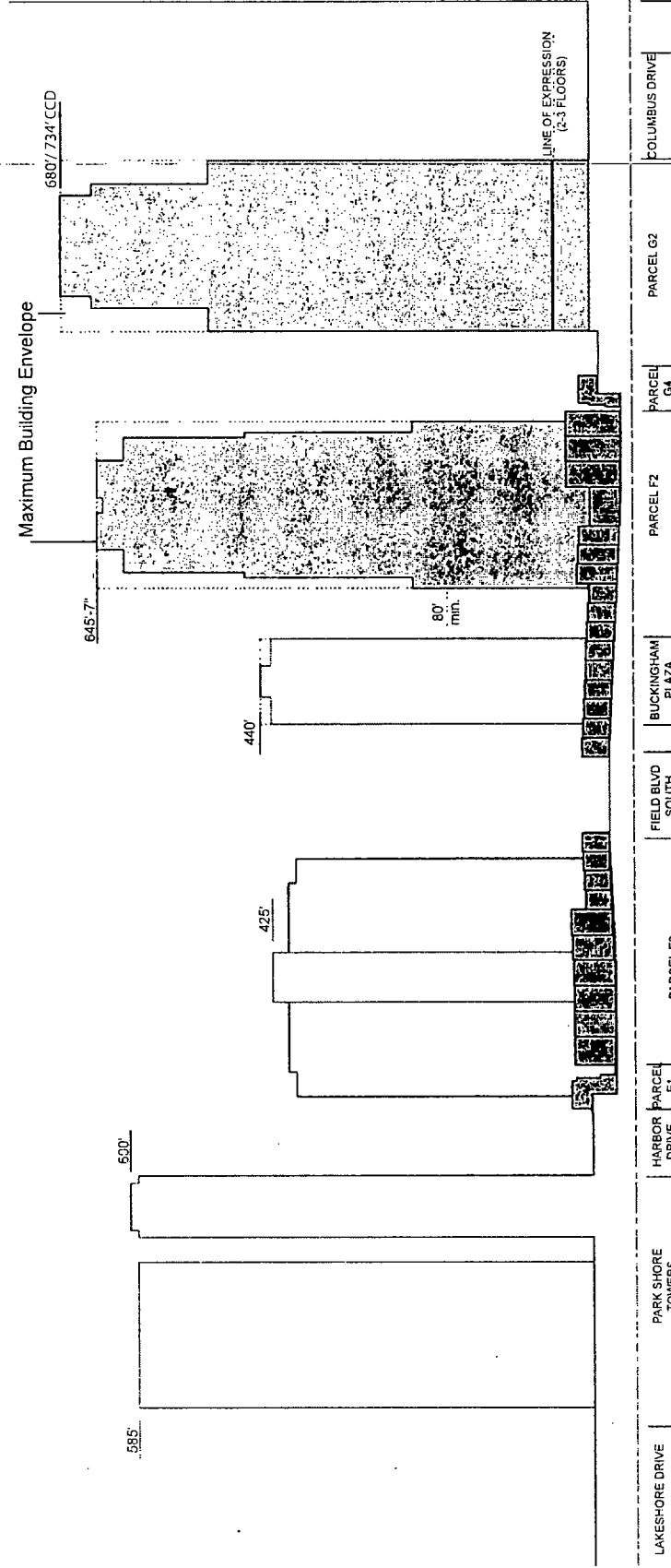


NOTE: These sections are diagrammatic in nature and are only intended to show approximate locations.

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Lakeshore East

Illustrative Elevation Neighborhood Park - View South

October, 2018

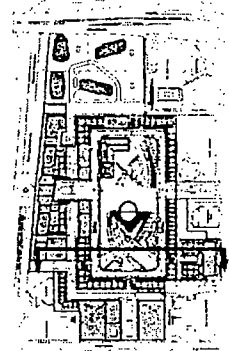
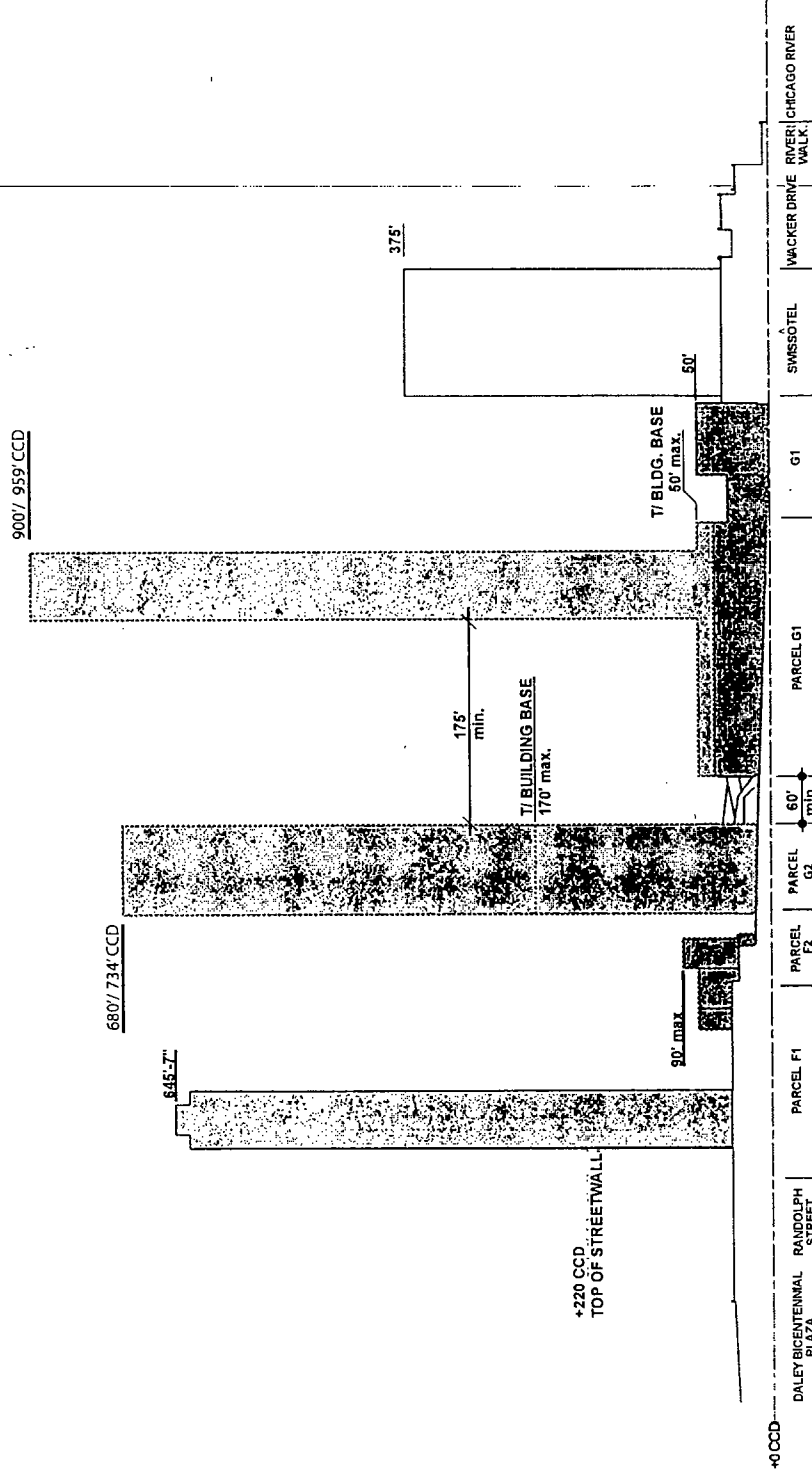


NOTE: These sections are diagrammatic in nature and are only intended to show approximate locations.

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Illustrative Elevation
North / South Section:
View Looking West

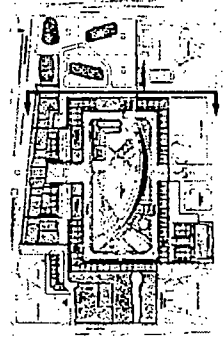
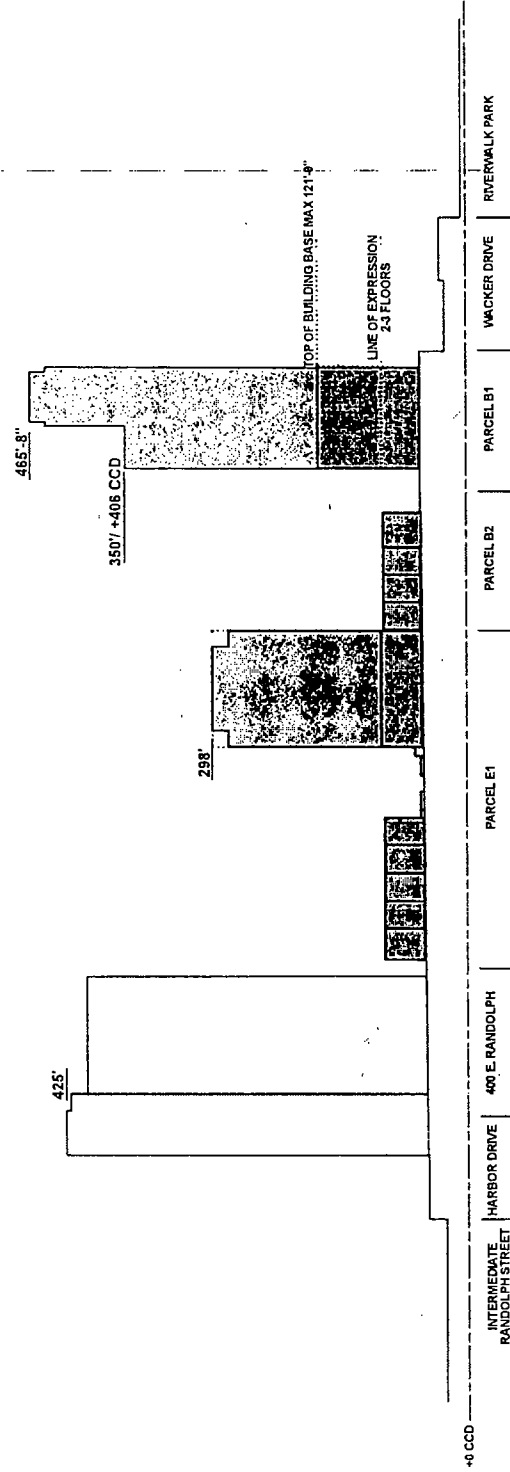


NOTE: These sections are diagrammatic in nature and are only intended to show approximate locations.

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Illustrative Elevation
Harbor Drive:
View Looking West

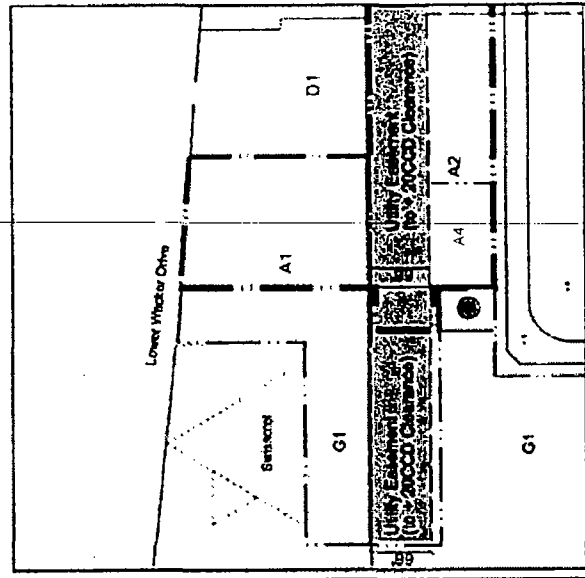


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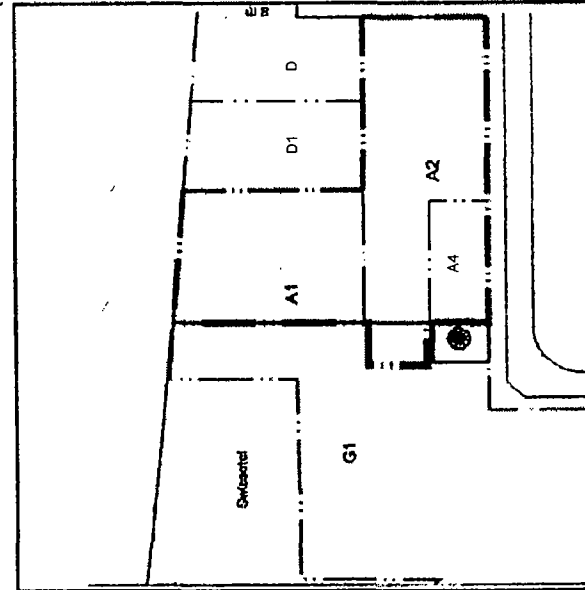
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Lakeshore East, LLC
6.21

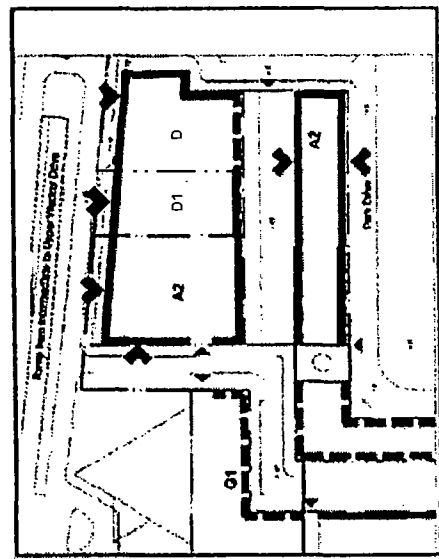
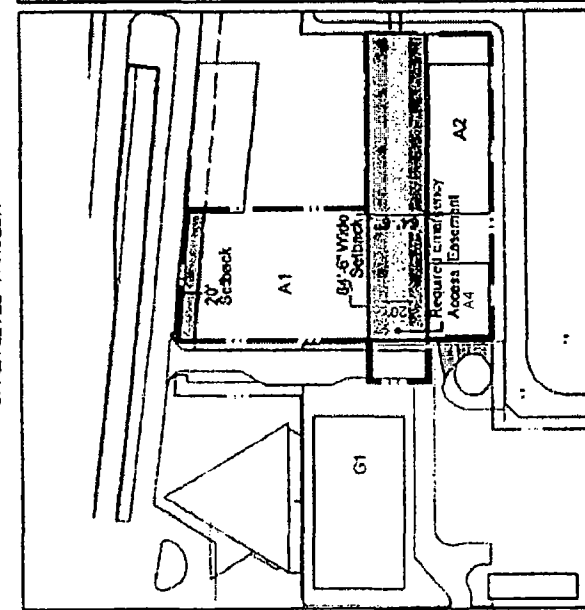
LOWER LEVEL - PARCELA



INTERMEDIATE LEVEL - PARCELA

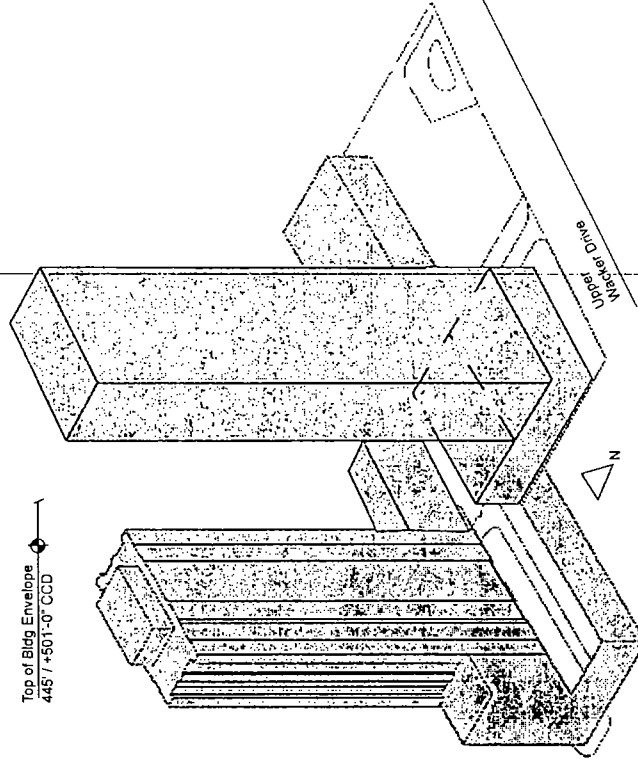
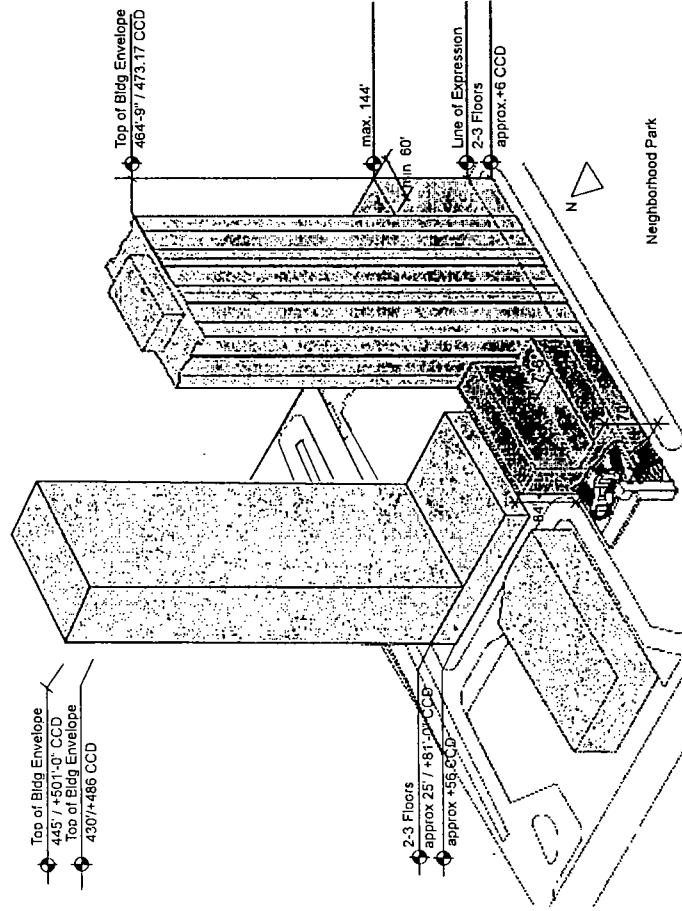


UPPER LEVEL - PARCELA

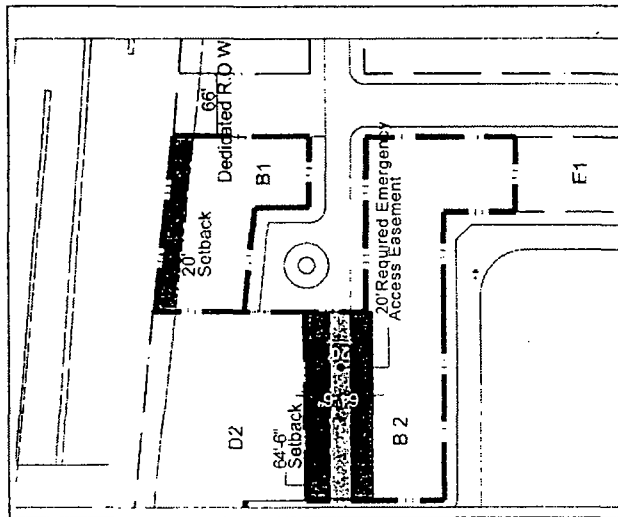


PARCELA - GRADE LEVEL BUILDING FOOTPRINT AND PREFERRED ACCESS PLAN

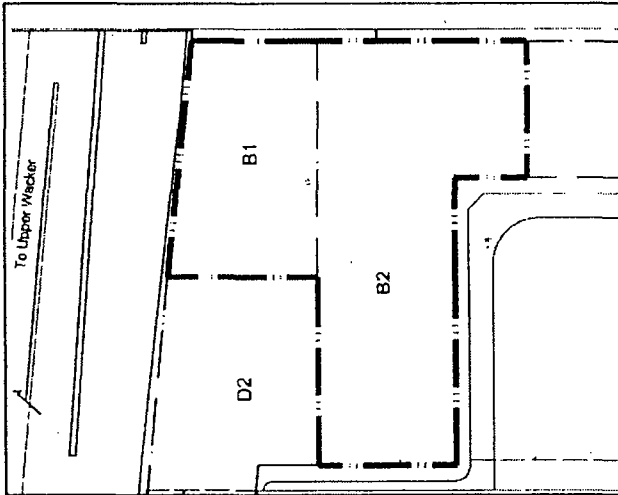
NOTE: These plans are diagrammatic in nature and are only intended to show suggested, not required locations for building frontage, lobby entries and parking access.



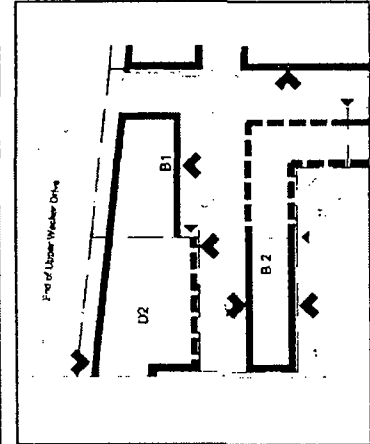
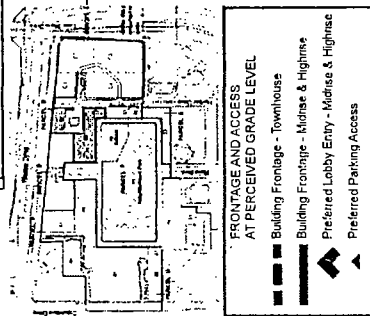
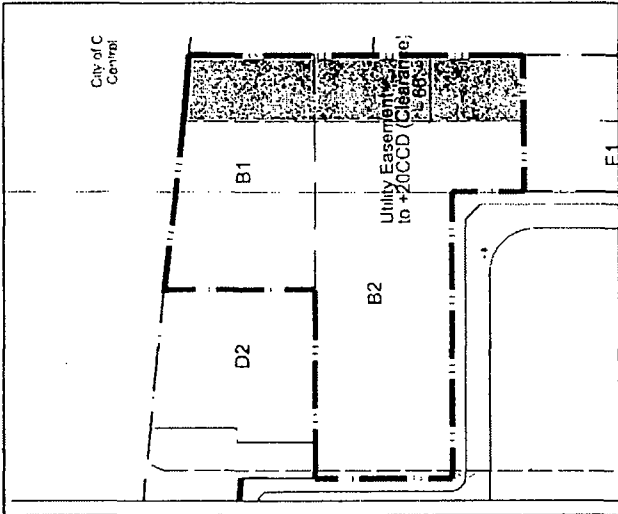
UPPER LEVEL - PARCEL B



INTERMEDIATE LEVEL - PARCEL B



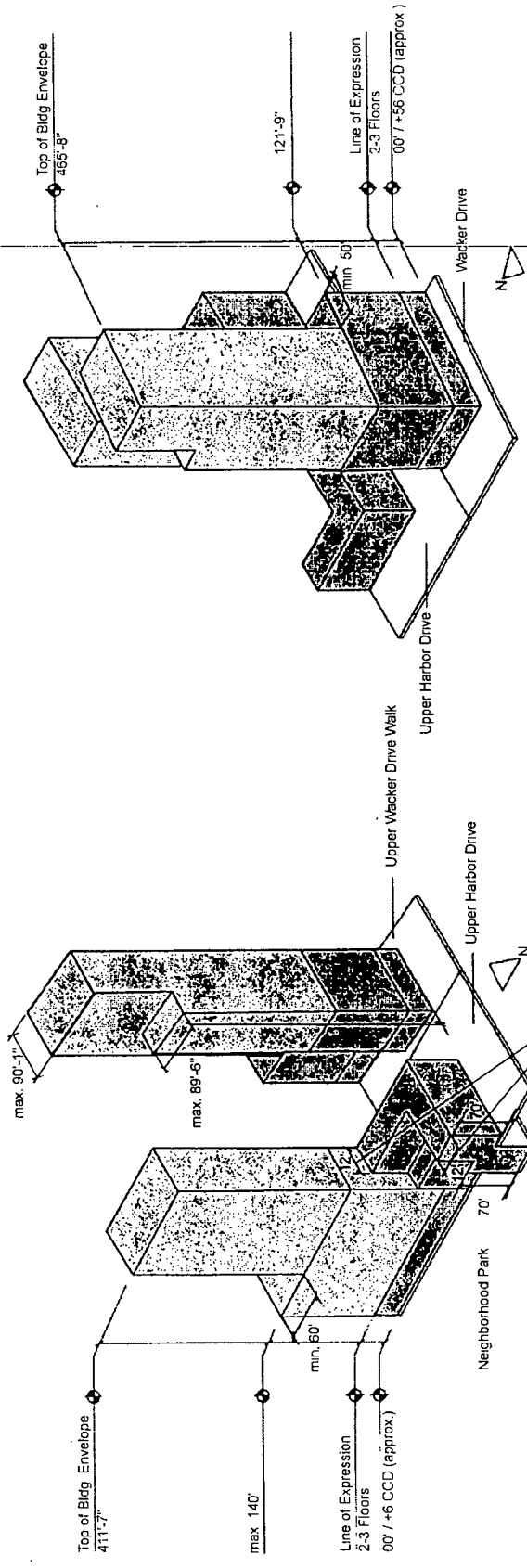
LOWER LEVEL - PARCEL B



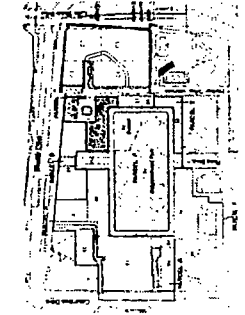
PARCEL B - GRADE LEVEL BUILDING FRONTAGE
AND PREFERRED ACCESS PLAN

Net Site Area (Approx.)	Upper	Intermediate	Lower
B1	17,493	33,177	22,246
B2	30,095	65,713	52,706

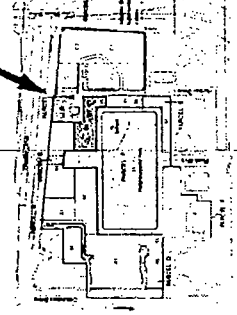
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12' NO BUILD SET BACK @ ROOF SUBJECT TO COMMITTEE ON BUILDING STANDARDS AND TESTS, DATED OCTOBER 2, 2002 (SEE ATTACHED)



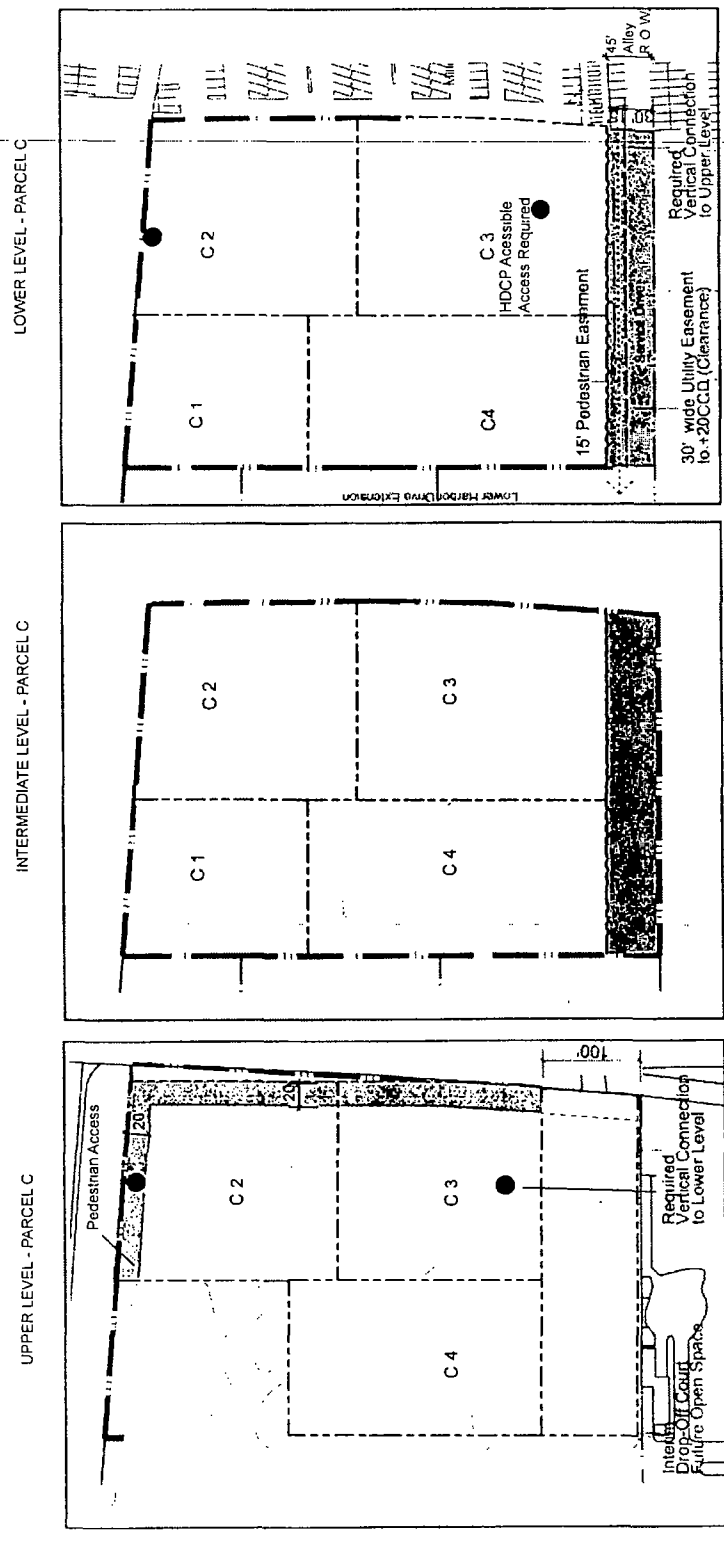
View Direction



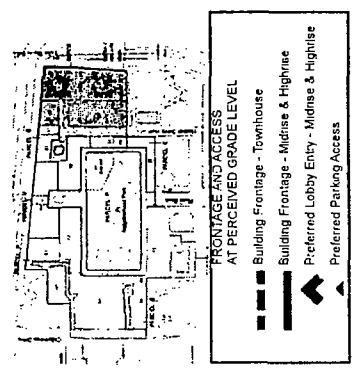
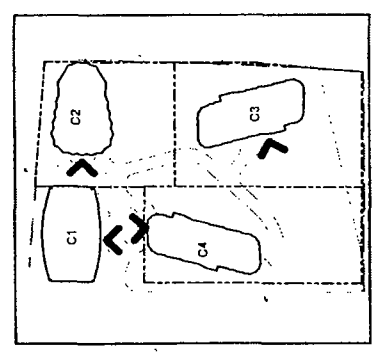
View Direction

Parcel Design Criteria - Parcel C

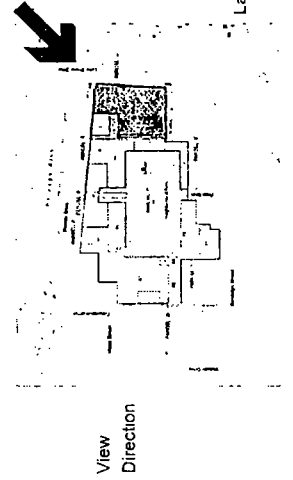
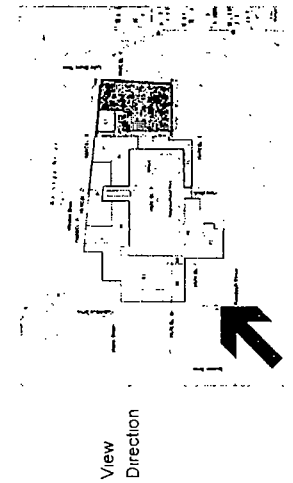
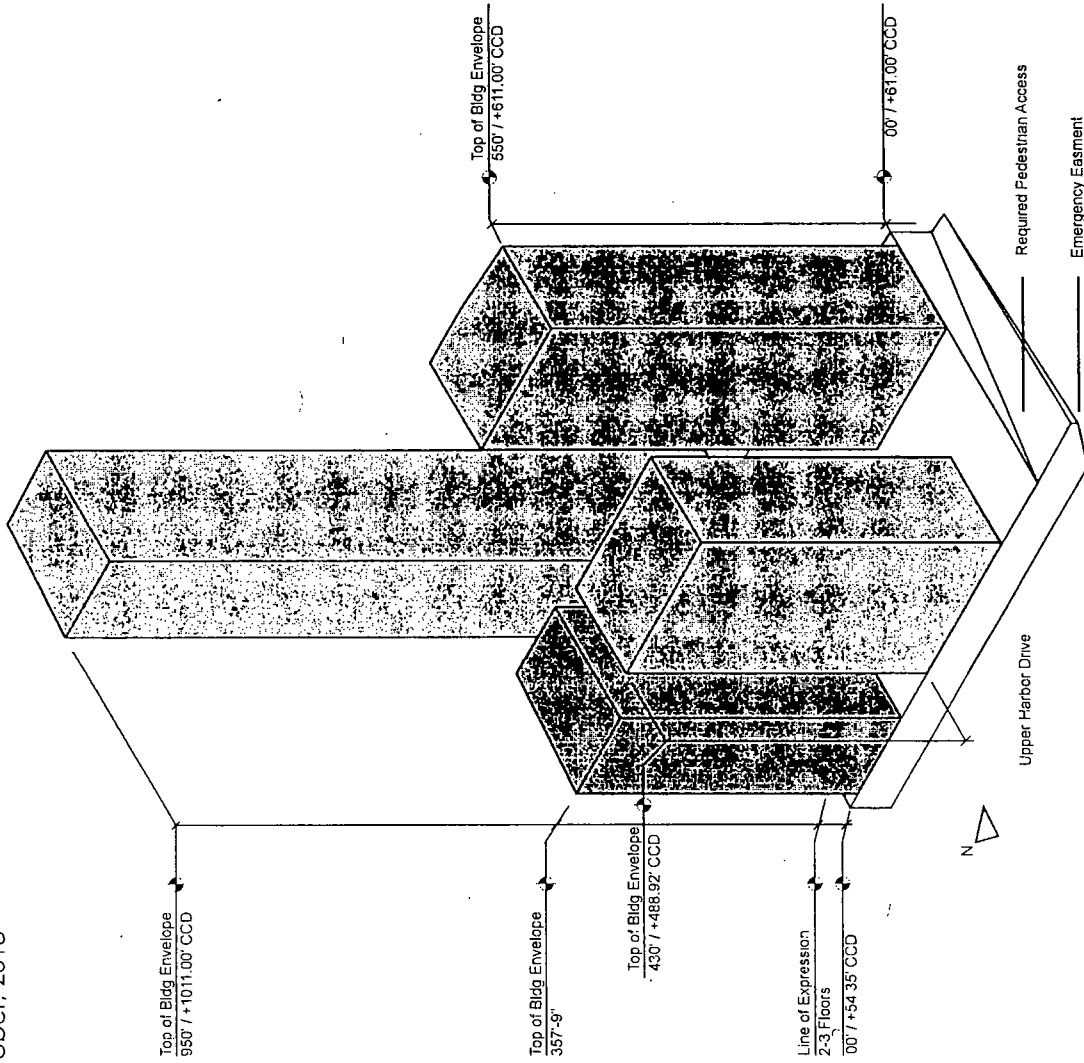
October, 2018



Net Site Area (Approx Sq Ft.)	Upper	Intermediate	Lower
C1	15,751	29,737	29,717
C2	38,927	47,076	47,076
C3	40,393	47,076	47,076
C4	52,421	52,421	52,421

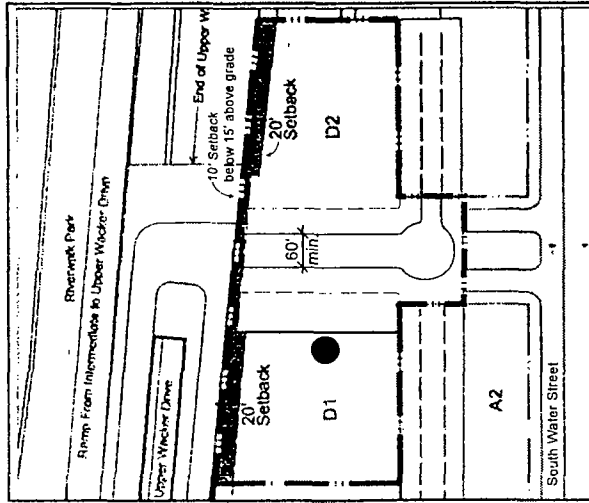


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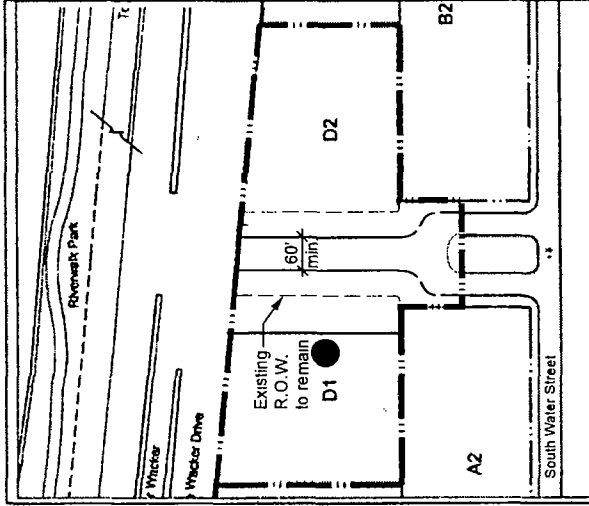


Parcel Design Criteria - Parcel D

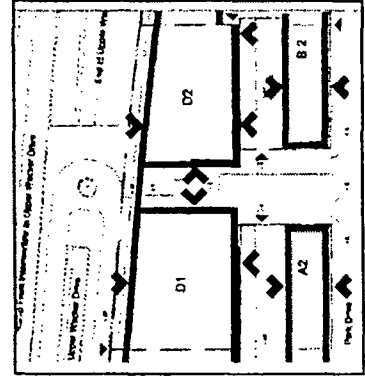
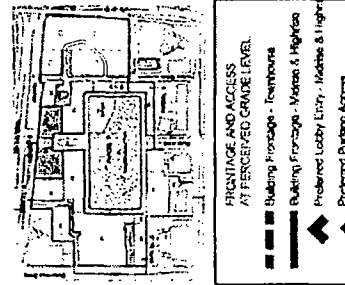
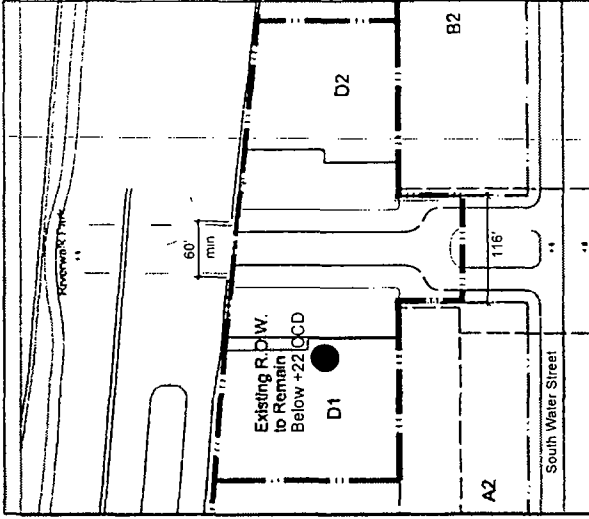
UPPER LEVEL - PARCEL D



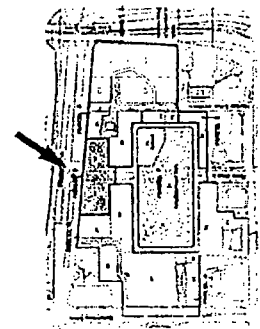
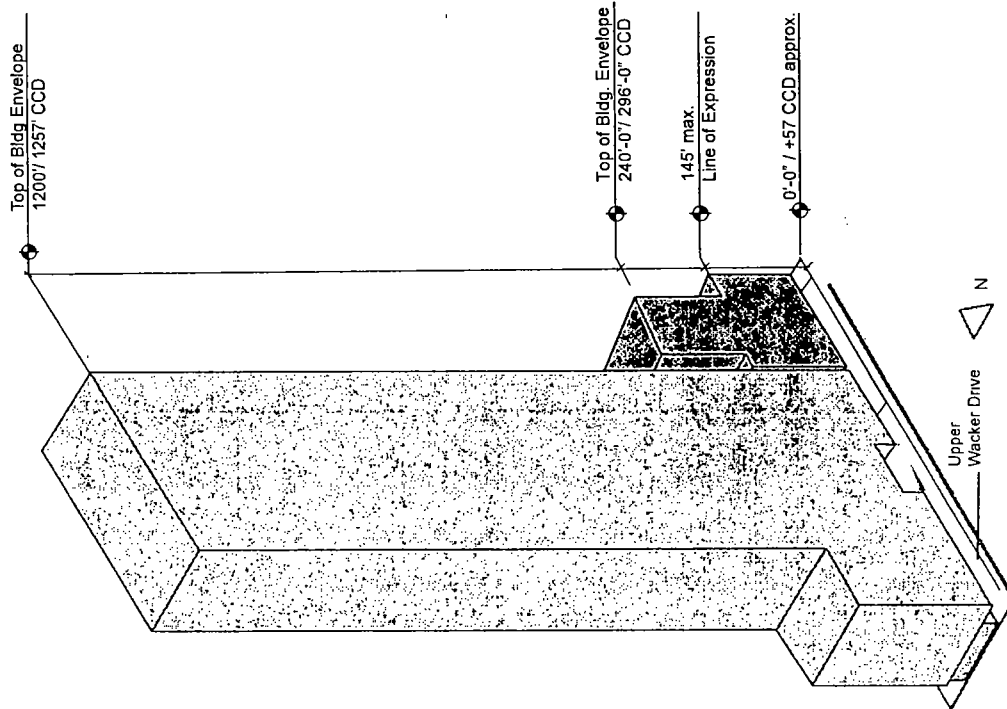
INTERMEDIATE LEVEL - PARCEL D



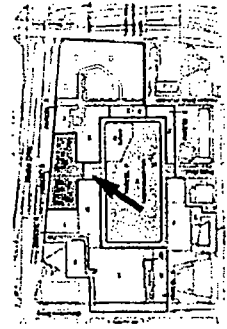
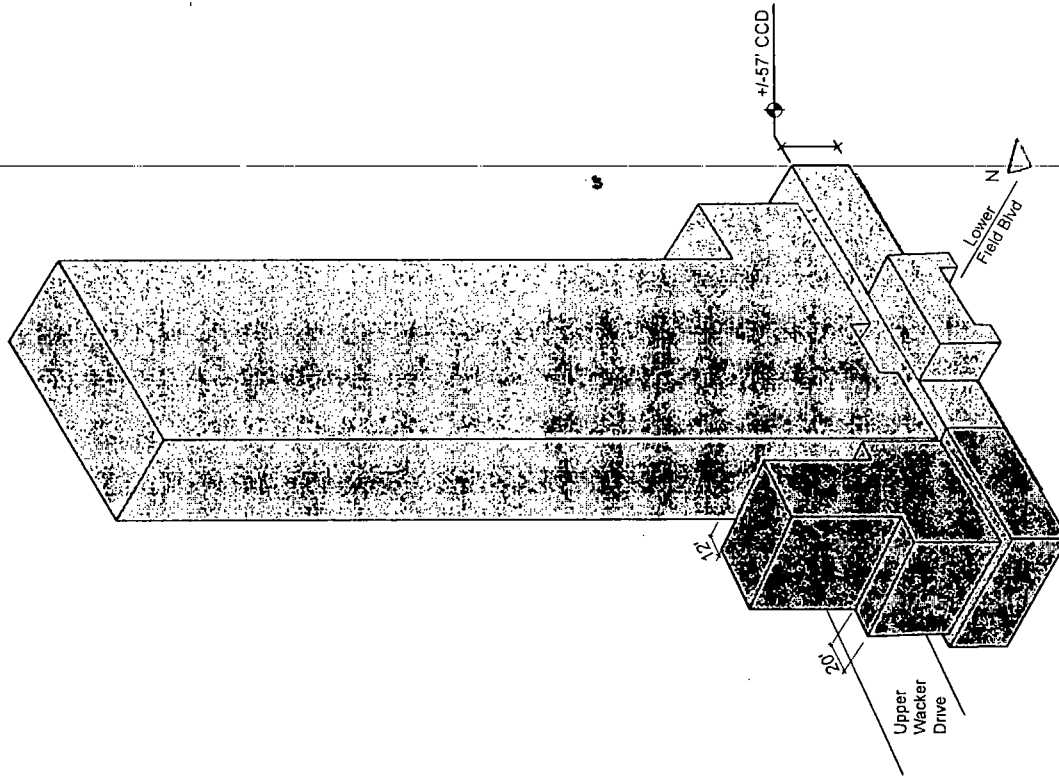
LOWER LEVEL - PARCEL D



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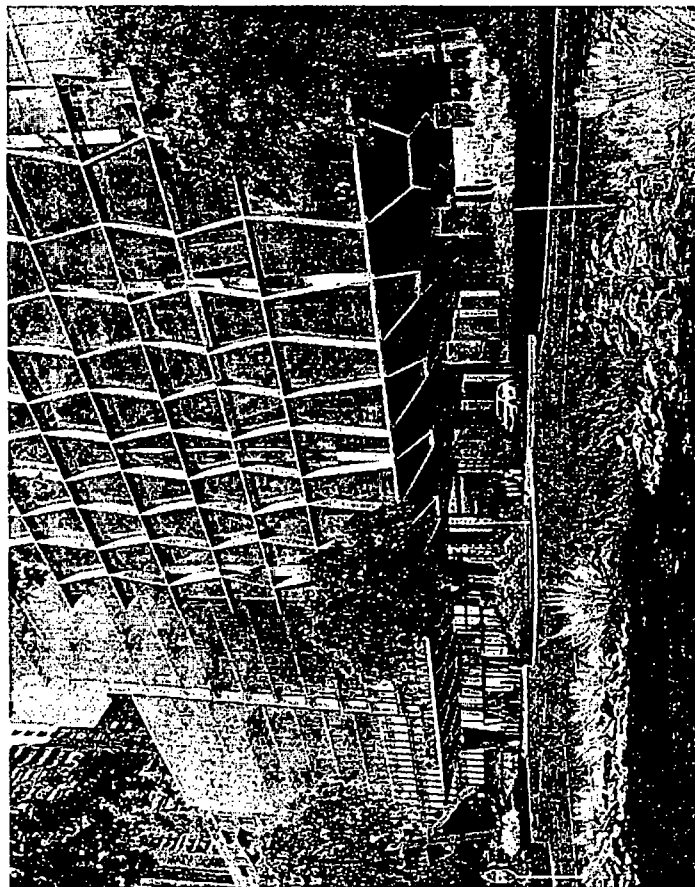
View
Direction



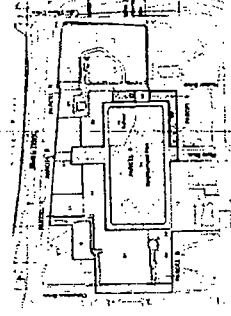
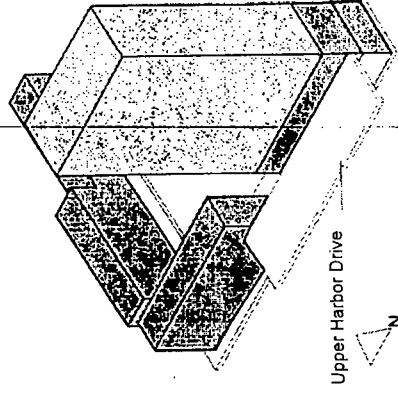
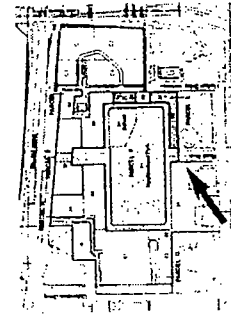
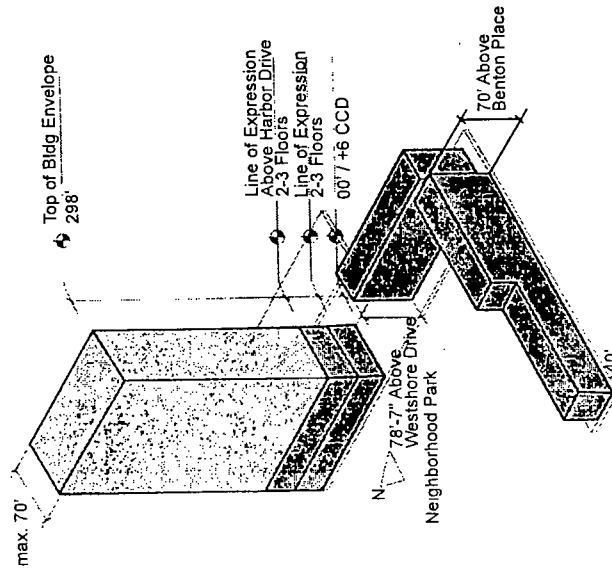
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Direction

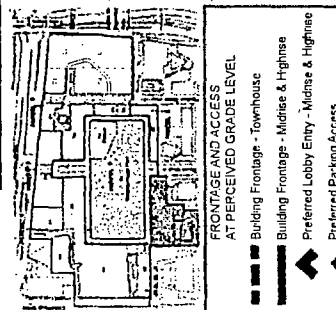
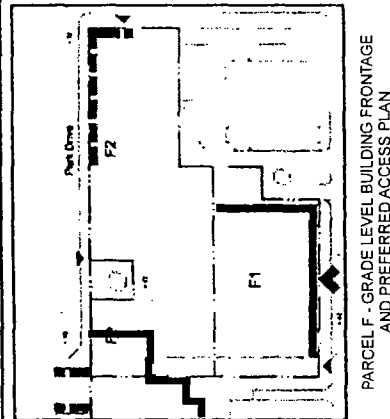
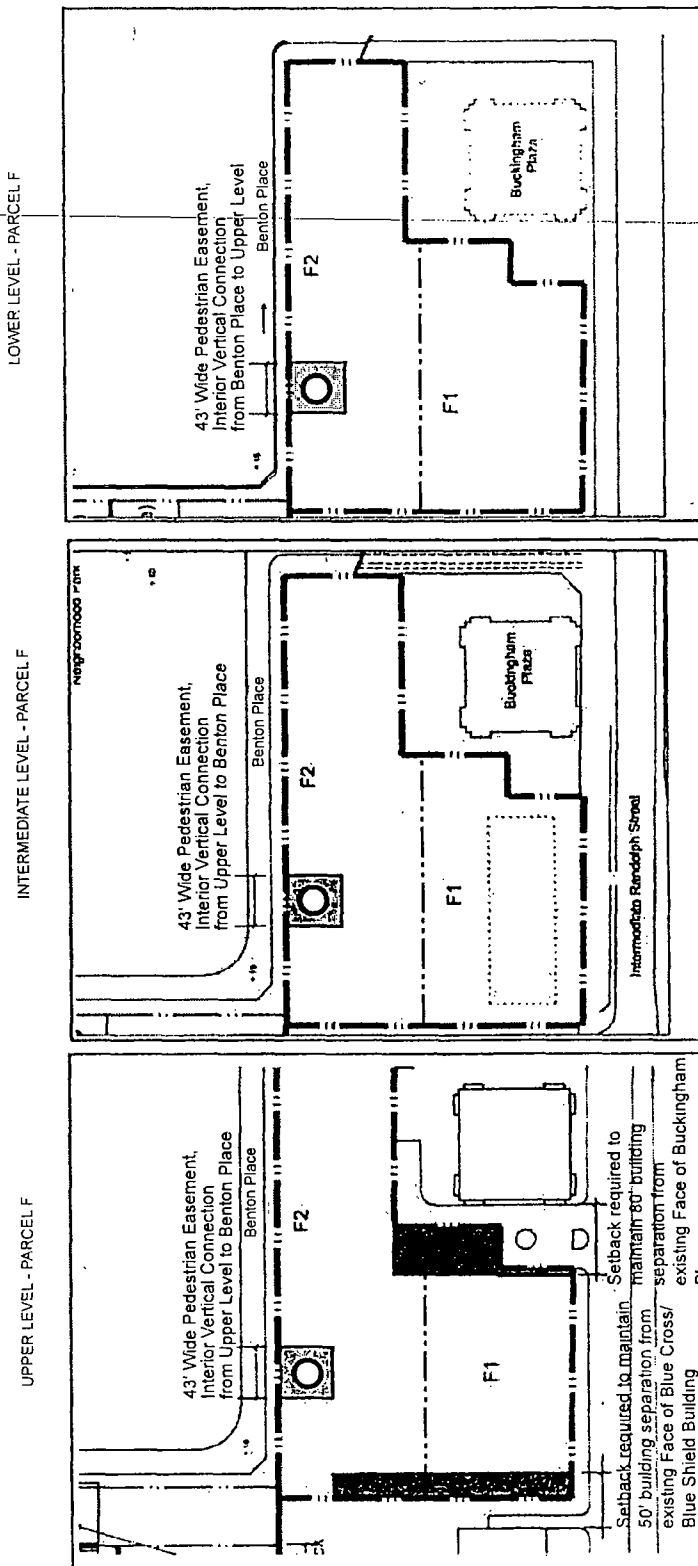
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Opening - Parcel D



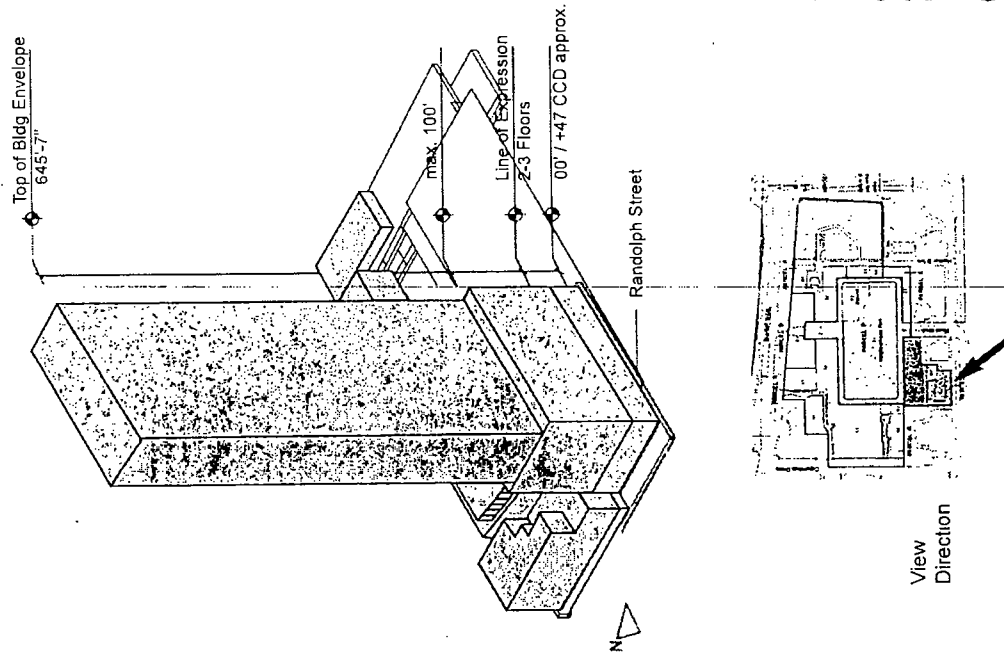
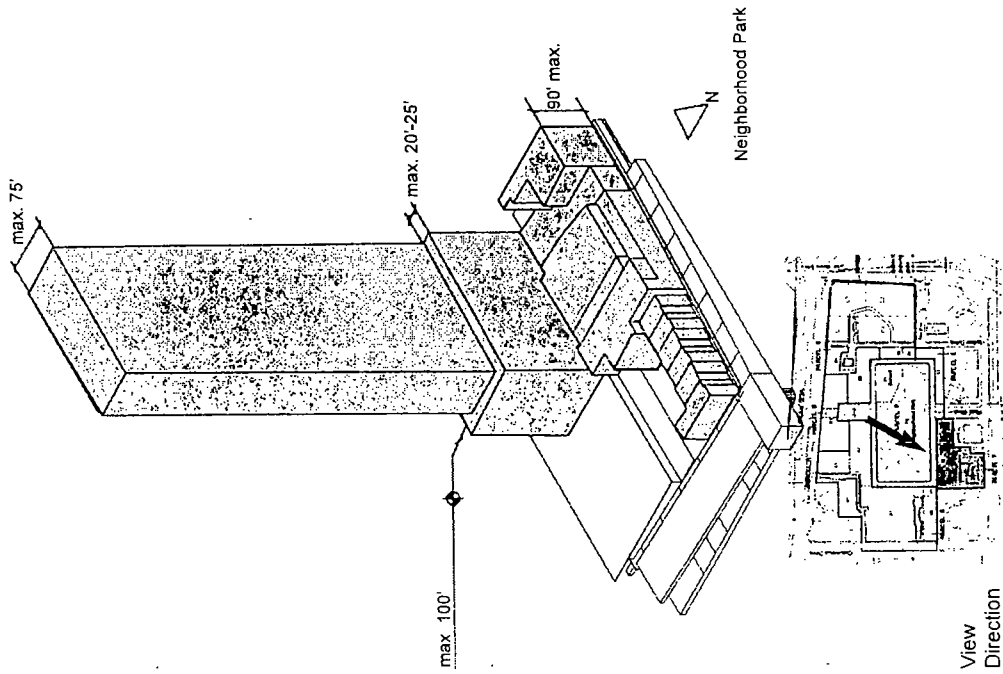
- Parcel D is intended to create a public "Gateway" into Lakeshore East. This also provides a visual connection to the Riverfront from the neighborhood park.
- The building spanning Field Blvd North is required to architecturally clad the existing structure of Upper and Intermediate level Wacker Drive within the opening.
- Proposed cladding solutions shall be reviewed and approved by the Department of Planning and Development, CDOT, and IDOT

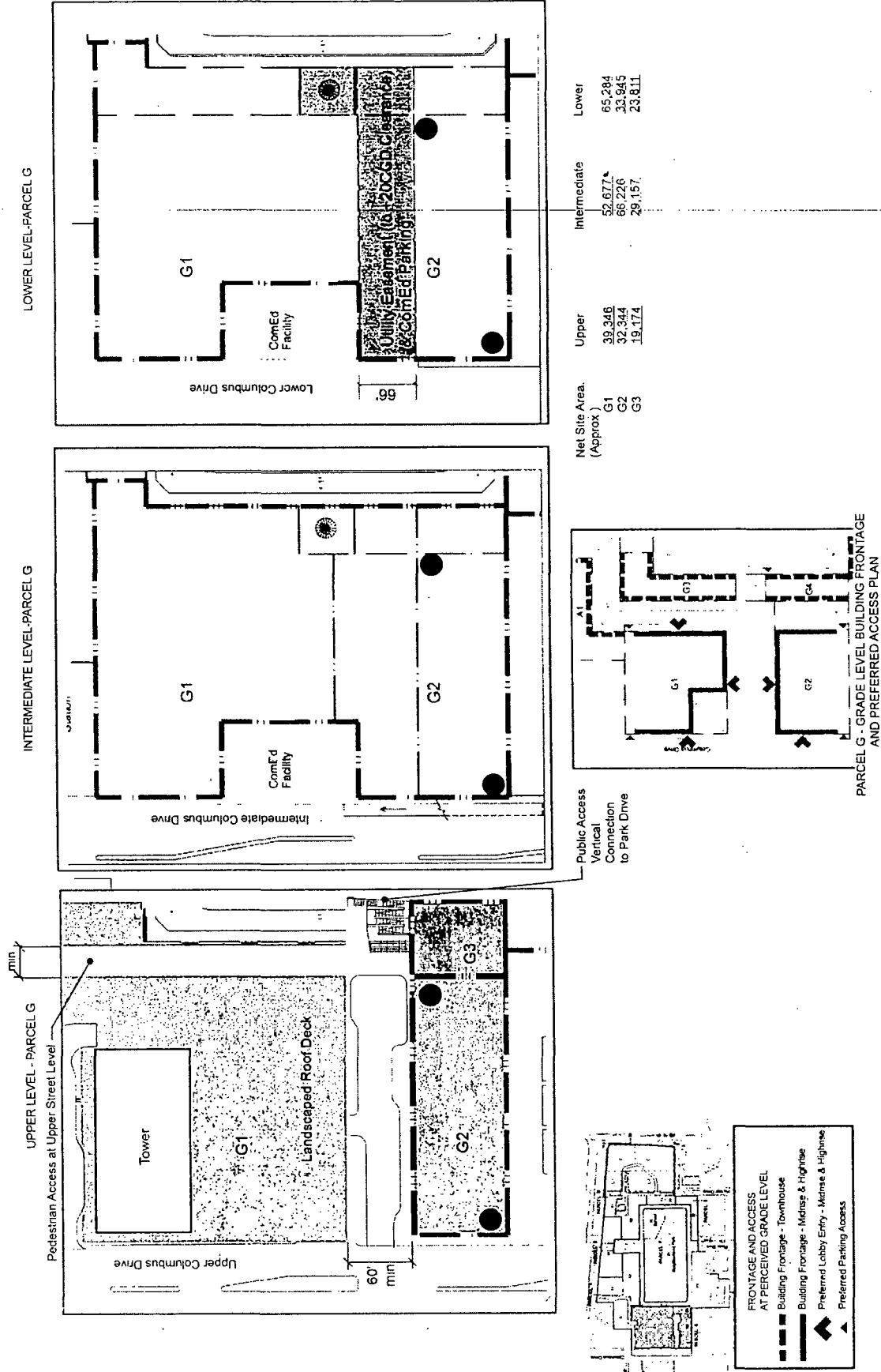




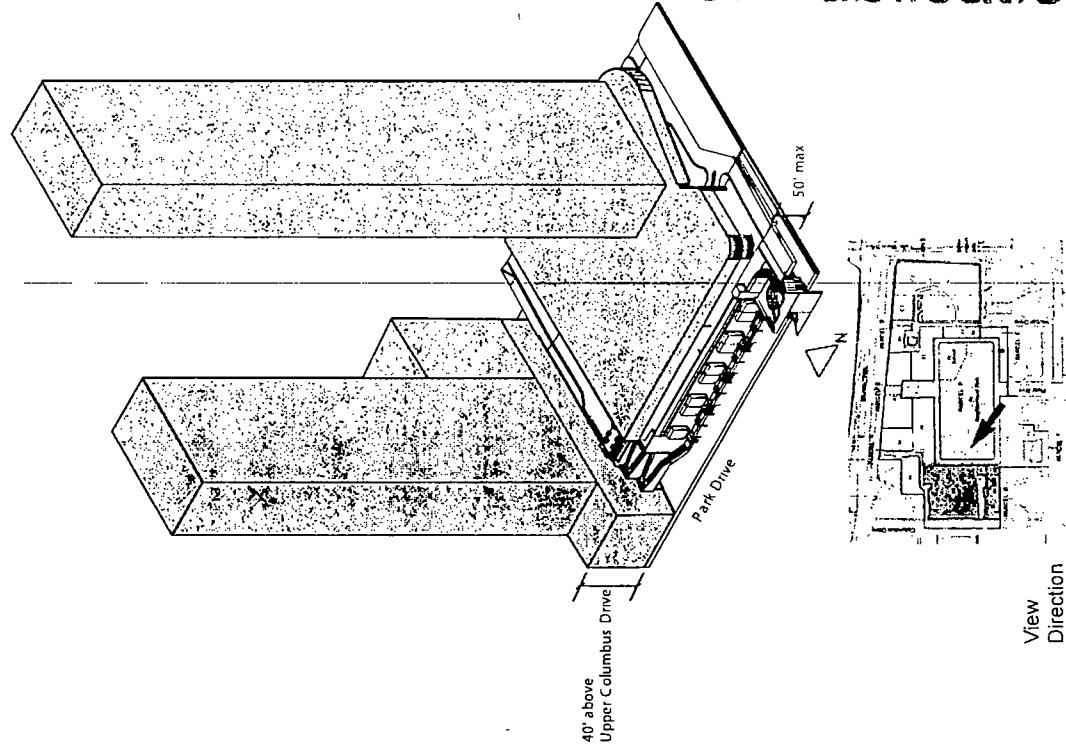
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Final for Publication

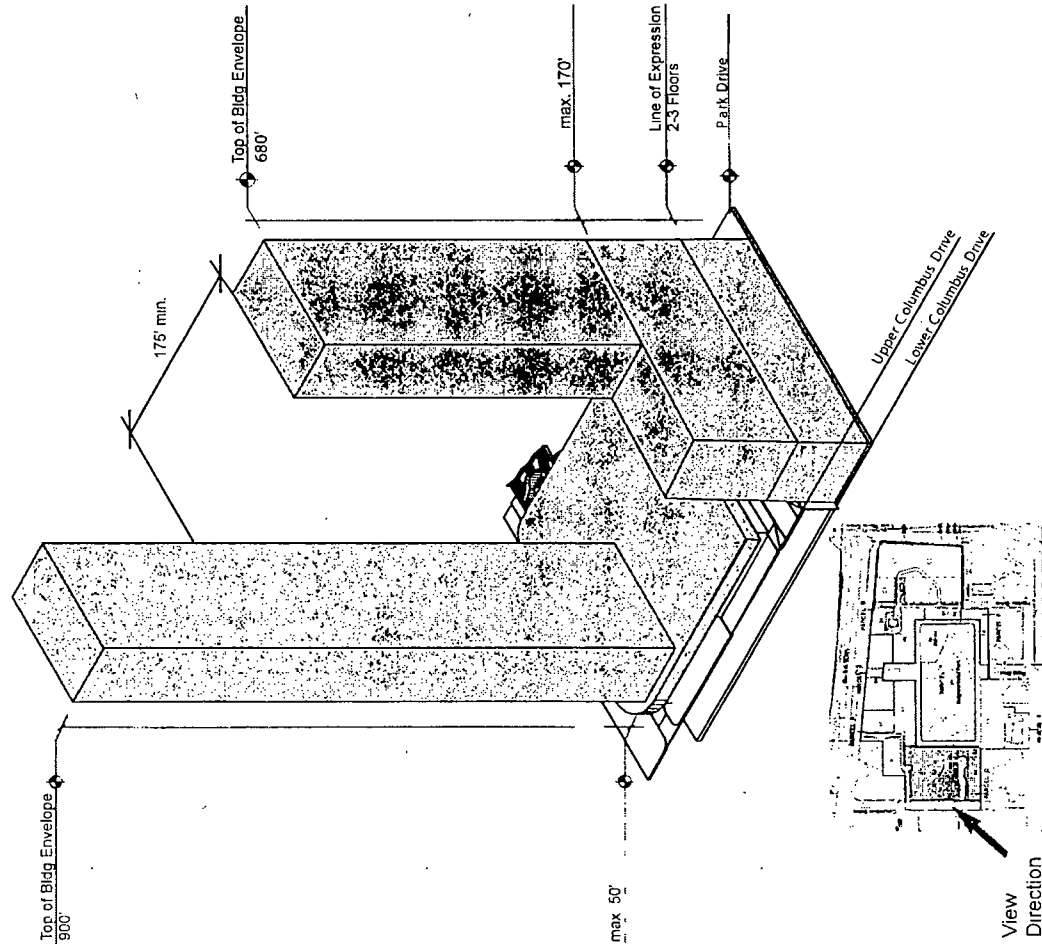




Final for Publication



View
Direction



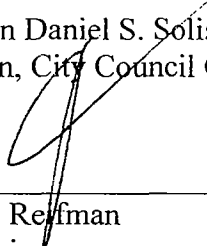
View
Direction



DEPARTMENT OF PLANNING AND DEVELOPMENT
CITY OF CHICAGO

MEMORANDUM

To: Alderman Daniel S. Solis
Chairman, City Council Committee on Zoning

From: 
David L. Reffman
Commissioner
Department of Planning and Development

Date: October 18, 2018

Re: Proposed Amendment to Planned Development #70 (2018) (generally located at 197-301 N. Harbor Drive and 452-500 E. Waterside Drive)

On October 18, 2018, the Chicago Plan Commission recommended approval of a proposed Amendment to Planned Development #70 (2018), submitted by the Co-Applicants, Lakeshore East, LLC, and IJKL, LLC, for the property generally located at 197-301 N. Harbor Drive and 452-500 E. Waterside Drive. The applicant is proposing to construct a three building residential/mixed use project on an interconnected base, not to exceed 950' in height with accessory parking and open space.

A copy of the proposed ordinance, planned development statements, exhibits, bulk table, staff report and Chicago Plan Commission resolution are attached. I would very much appreciate your assistance in having this matter heard at the next possible City Council Committee on Zoning. If you have any questions in this regard, please do not hesitate to contact me at 744-9476. Thank you.

Cc: Anna Robles, Dan Klaiber

PD Master File (Original PD, copy of memo)

City Clerk Copy App# 19827